

# WESTMINSTER MALL SPECIFIC PLAN

W

M

DECEMBER 2022

ADOPTED BY ORDINANCE NO. 2592

For the Community. By the Community.





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# Chapter 1

# Introduction



## Westminster Mall Specific Plan Draft Vision

The Westminster Mall is a flourishing center of community life, a vibrant destination and a thriving hub of shopping, living and working activity that is source of civic pride for our community.

Our Mall, located in one of the most highly-visible areas in our City, has an iconic design and is a place that others look to emulate. The character of this area is different from any other place in Westminster and has transformed the image of our community.

The Mall is a vibrant, social place where we feel comfortable meeting new people and old friends, enjoying family gatherings and public events. It's the "place to be" and the first place we want to take guests who are visiting from out of town.

As the needs of our community have evolved, the Mall has been able to attract substantial reinvestment to Westminster and provide our community with a mix of new retail, hospitality, housing, jobs, and public spaces. Collectively, all of these opportunities have contributed to our long-term economic health and fiscal stability.

The Mall provides new housing opportunities for first-time home buyers, professionals, families, and seniors in our community at a variety of affordability levels to accommodate the changing life stages and lifestyle needs of our residents.

Our mall features rich and inviting indoor and outdoor spaces, designed with open seating, recreational amenities, and attractive lighting and landscaping. Spaces are programmed with activities year-round to be enjoyed by all Westminster residents and is easily accessible to visitors driving, biking, or taking public transit. Its thoughtful design reflects its unique character internally and helps establish an experience that cannot be found anywhere else in Orange County.

**WESTMINSTER MALL:** Where opportunities to shop, work and live all come together to create community.

What would you add or change about the vision?

Handwritten sticky notes on the left board, including:

- More open space
- More greenery
- More seating
- More public art
- More events
- More parking
- More bike lanes
- More walkways
- More public transit
- More affordable housing
- More job opportunities
- More community spaces
- More outdoor seating
- More landscaping
- More lighting
- More recreation
- More amenities
- More accessibility
- More inclusivity
- More sustainability
- More safety
- More security
- More cleanliness
- More maintenance
- More customer service
- More staff
- More training
- More technology
- More innovation
- More collaboration
- More communication
- More transparency
- More accountability
- More integrity
- More honesty
- More respect
- More kindness
- More compassion
- More empathy
- More understanding
- More patience
- More tolerance
- More forgiveness
- More grace
- More mercy
- More love

## VISION

Handwritten notes on the right board, including:

- More greenery
- More seating
- More public art
- More events
- More parking
- More bike lanes
- More walkways
- More public transit
- More affordable housing
- More job opportunities
- More community spaces
- More outdoor seating
- More landscaping
- More lighting
- More recreation
- More amenities
- More accessibility
- More inclusivity
- More sustainability
- More safety
- More security
- More cleanliness
- More maintenance
- More customer service
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- More honesty
- More respect
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- More empathy
- More understanding
- More patience
- More tolerance
- More forgiveness
- More grace
- More mercy
- More love

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## 1.1 INTRODUCTION

The Westminster Mall Specific Plan (hereafter WMSP or Specific Plan) provides a comprehensive plan and vision to guide the coordinated development of the parcels that comprise the Westminster Mall site to create new benefits to the community and support economic growth.

The project site provides a unique opportunity to reposition the Mall into the thriving activity center that it once was and to accommodate the future growth of the City. Since the City of Westminster is built out, there are limited opportunities throughout the City to create new housing, retail, office, hospitality uses that are in high demand. The Westminster Mall site, at nearly 100 acres, offers an ideal area to accommodate a significant amount of the City's future growth and change.

Over the years, the community has expressed a strong desire to revitalize this important commercial center through the creation of a vibrant destination with experiential, unique, and convenient shopping experiences while strategically providing a space for an increased range of housing options. While the Westminster Mall site has the potential to address needs of the local community, it also provides an opportunity to create a broader regional draw of visitors, due in large part to its convenient access directly adjacent to the I-405 Freeway and dedicated off-ramp into the project site.

The WMSP was developed as a mechanism to implement the goals and policies of the City of Westminster's General Plan (2016). The General Plan promotes mixed-use and focuses the City's future growth and development along prominent corridors and at the Mall this allows other portions of the City to preserve the single-family character of other areas in town. The General Plan also mandated that a Master Plan be prepared for all parcels on the Mall site to integrate all future development into a singular cohesive vision. This is particularly important because the timing for new projects will vary by property owner; therefore the WMSP must provide comprehensive planning guidance so that property owners, City planners, and the community as a whole will know what is expected of future projects.

### A Brief History of Westminster Mall

#### 1920s

During the 1920s, the world's largest goldfish farm moved into the area where the Westminster Mall stands today.

#### 1974

August 7, 1974 the Westminster Mall opens to the public.

#### 2008

Westminster Mall undergoes a major renovation.

#### 2015

The City of Westminster General Plan calls a mix of uses at the Mall.

#### 2019-22

Westminster Mall Specific Plan outreach and adoption.

## 1.2 COMMUNITY OUTREACH

Discussions with the public and property owners served as the critical first step in defining (and refining) a vision, objectives, and an overall development concept that is the focus of the Specific Plan document.

As part of the initial planning efforts, the City conducted an extensive outreach program with property owners at the Mall, surrounding residents, and the community at large. The outreach efforts were facilitated to gain a better understanding of the community's vision and concerns for redevelopment of this area and to provide a forum for the exchange of ideas between the City and stakeholders in advance of the drafting of the Specific Plan.

**STEWARDING THE  
SUCCESSFUL FUTURE  
OF THE WESTMINSTER  
MALL MATTERS TO OUR  
ENTIRE COMMUNITY.**

The WMSP contains design and development standards that address topics such as permitted uses, streetscapes, open space requirements, traffic solutions, parking, building height limitations, and maximum development capacities that were all developed as a result of the feedback received during the initial outreach process.

Four community workshops were held prior to the creation of the WMSP to discuss a series of topics including:

1. A general overview of the future redevelopment of the Mall;
2. Traffic and mobility concerns;
3. Design considerations (landscaping, height, building scale and transitions from single-family residential neighborhoods, open space, etc.); and
4. A final workshop that integrated the community's ideas and feedback into a revised concept.

Combined, the meetings had an attendance of approximately 700 people. In addition, the last two meetings were broadcast live via FaceBook Live, allowing interested constituents to provide their comments virtually to be responded to in real-time during the question and answer portion of the presentation.

In August 2018, the City launched a community survey as an initial piece of public outreach and information gathering for the Specific Plan. The online survey requested community feedback about the types of activities, amenities, and mobility features they would like to see in the future as the mall redevelops. Many responses also identified ideas for the city and property owners to consider for the future, including examples from other cities that participants wanted the City to replicate in Westminster. The survey had a very high level of participation and completion by the community; by the time the survey closed (five weeks later), almost 2,000 people submitted a response and almost half had provided written comments and feedback for the city to consider.

Through the City's comprehensive engagement process detailed in this section, the Westminster Mall Vision and Objectives were created to articulate the community's aspirations for the Mall and were approved by City Council and are outlined in the following section (Section 1.3).



*To reach the greatest number of people possible, the City broadcast presentations about the Mall on FaceBook Live, allowing people to participate and ask questions from their home or business.*

On March 12, 2019, around 300 people participated in the first WMSP Community Workshop held at the Mall. During the workshop, participants were asked to provide input on various aspects of the future design of the Mall, and to tell the City what they liked and wanted to change about the proposed Westminster Mall Vision. Topics most frequently mentioned by attendees generally fell into four categories: traffic, building heights, housing, and placemaking.





*The future of the Westminster Mall is envisioned to create a new activity center with internal walkways and paseos separated from cars that connect different uses and districts internally and allow people to walk between areas and uses without having to drive from place to place as shown in this conceptual illustration.*



## 1.3 WESTMINSTER MALL VISION AND OBJECTIVES

### 1.3.1 VISION

The WMSP Vision is an aspirational statement of the description of what the Westminster Mall will look like in the future and the values the community identifies as important. It includes a description of how it will be used, mix of uses desired, what it will look like visually once it is transformed, and what features it should integrate into future redevelopment of the site.

Our Mall, located in the most **high-profile and visible area** of our City, has an **iconic design** and is a place that others look to emulate. The character of this area is different from any other place in Westminster and has transformed the image of our community.

The Mall is a **vibrant, social place** where we feel comfortable meeting new people and old friends, enjoying family gatherings and public events. It is the **“place to be”** and the first place we want to take guests who are visiting from out of town.

As the needs of our community have evolved, the Mall has been able to attract substantial reinvestment to Westminster and **provide our community with a mix of new retail, hospitality, housing, jobs, and public spaces**. Collectively, all of these opportunities have contributed to our **long-term economic health and fiscal stability**.

The Mall provides **new housing opportunities for first-time home buyers, professionals, families, and seniors in our community** at a variety of affordability levels to accommodate the changing life stages and lifestyle needs of our residents.

Our mall features **rich and inviting indoor and outdoor spaces**, designed with open seating, recreational amenities, and attractive lighting and landscaping. Spaces are programmed with **activities year-round** to be enjoyed by all Westminster residents and are **easily accessible** to visitors driving, biking, walking, or taking public transit. Its thoughtful design connects its unique spaces internally and helps establish **an experience that cannot be found anywhere else in Orange County**.



“ The long-term vision for Westminster Mall *calls for mixing traditional retail uses with new housing, public spaces, and entertainment uses to create an experience-oriented destination* at one of Westminster’s most important sales-tax-generating centers. New commercial pad sites are encouraged to infill the existing surface parking area while other parking strategies, including shared parking and wrapped or screened parking structures, should be used to ensure sufficient parking is available on-site. A specific plan or other master planning mechanism for Westminster Mall is required to guide future development. ”

**Westminster General Plan**  
July 2016

### 1.3.2 OBJECTIVES

Through the Specific Plan outreach process, a list of objectives was compiled to contribute to the implementation of the vision for the Specific Plan. Objectives are more clearly defined than the WMSP Vision. They represent the specific paths that an organization will take to meet its vision.

**Gateway to Westminster.** Use signage, landscaping, and the design of new development to clearly delineate the entrance to Westminster and serve as a landmark in North Orange County along Interstate 405.

**Priority Location for New Growth.** Serve as one of the primary locations in the City (in addition to corridors) to accommodate future growth; consistent with the provisions of the General Plan.

**Balanced Planning.** Implement projects within the Specific Plan Area that give equal consideration to planning, environmental considerations, and economic feasibility.

**Greater Mix of Land Uses.** Create a land plan that encourages a greater mix of uses and appeals to a diverse population and accommodates future growth for the City. Support a range of development options that respond to changing market conditions and bolster the local economy.

**Retail, Office, and Hotel Development.** Encourage upscale shopping, dining, and entertainment as well as office and hospitality options to be developed in the area.

**Housing Diversity and Affordability.** Provide a diversity of housing types and a range of affordability when new residential uses are proposed in the Plan.

**Access to Open Space.** Pursue opportunities to create new gathering spaces and open space amenities. Provide options to increase public connectivity to open space, including the improvement of the Westminster Nature Activity Trail.



*The WMSP will introduce new shopping and dining options in the area.*



*The WMSP allows for a mix of land uses, including new housing choices, to accommodate future growth of the City.*



*New signage or monument features at the entrance can help identify the Mall as a distinctive location.*



*Careful building placement and thoughtful architectural design will create a sense of place at the Mall.*

**Balance New Development with Existing Roadway Capacity.** Any proposed development must be able to be served by the capacity of the existing roadway (no additional lanes).

**Bike and Pedestrian Transportation Options.** Improve pedestrian and bicycle connectivity by creating an active streetscape that promotes safe walking and cycling.

**Building Form and Architectural Design.** Provide clear standards and guidelines to encourage future development that respects the surrounding residential neighborhoods, enhances views, and creates a sense of place through thoughtful building placement, form, and architectural design.

**View Enhancement and Protection.** Minimize the effects of new buildings on existing views from neighboring residential uses and generate view opportunities adjacent to the freeway through control of building placement and/or height.



*A focal point of the WMSP will be new gathering spaces and open space amenities.*



*The WMSP will facilitate increased pedestrian and bicycle connectivity inside the Mall.*



*Enhanced and well designed streetscapes promote a safe pedestrian environment and create spaces for activity, which are strongly desired by the community.*



# Chapter 2

## **Purpose and Authority**





KAY  
JEWELERS

KAY  
JEWELERS

AUSTIN'S

COTTON ON

CAFE

**WE BUY GOLD FOR CASH!**

**FAST-FIX**  
JEWELRY AND  
WATCH REPAIR  
714.896.6673

LOWER LEVEL BY MACY'S

THE WALL

## 2.3 PURPOSE AND AUTHORITY

The Westminster Mall Specific Plan (WMSP or the Specific Plan) provides customized regulatory guidance to facilitate the orderly development of land uses and densities and intensities that would not otherwise be allowed by the City's development standards.

The Specific Plan is established through the authority granted to the City of Westminster by the California Government Code, Title 7, Division 1, Chapter 3, Article 8, Sections 65450 through 65457. The Government Code authorizes cities to adopt specific plans either by resolution or by ordinance as regulation. A Planning Commission hearing and City Council hearing are required to adopt the Specific Plan. This Specific Plan is both a policy and a regulatory plan that must be adopted by ordinance. It establishes policy direction, including a conceptual land use plan, which guides the development of the properties within the project area, as well as permitted uses, development standards, and design standards and guidelines. Development or site plans for this area must be consistent with this Specific Plan.

In response to government requirements, this Specific Plan has been prepared to provide an essential link between City of Westminster policies and future development in the Specific Plan Area. Functioning as a regulatory document, the Westminster Mall Specific Plan implements the City of Westminster's General Plan for a mixed-use district. All future development plans, tentative parcels, and/or tract maps, or other similar entitlements shall be consistent with regulations set forth in this document and with all applicable City regulations. Westminster Mall Specific Plan amendment procedures are described in Chapter 8, *Administration and Implementation*.

## 2.4 RELATIONSHIP TO ADOPTED PLANS AND POLICIES

### 2.4.1 REGIONAL PLANS

#### THE MASTER PLAN OF ARTERIAL HIGHWAYS

The Orange County Transportation Authority (OCTA) coordinates transportation planning in Orange County by administering and updating the Master Plan of Arterial Highways (MPAH), last updated in 2018. Coordination throughout the region is essential to the integrity of a functional regional highway network, therefore funding, including Measure M2 and Orange County Transportation Funding Program (CTFP), is contingent upon compliance with the MPAH. The MPAH classifies Bolsa Avenue as a major arterial highway, Goldenwest Street as a primary arterial highway, and Edwards Street as a secondary arterial highway. Requirements prompted by the MPAH for the Westminster Mall area are discussed further in Chapter 6, *Mobility*.

### 2.4.2 LOCAL PLANS

#### WESTMINSTER GENERAL PLAN

The California Government Code states that a "Specific Plan shall include a statement of the relationship of the Specific Plan to the General Plan, and further, that it may not be adopted or amended unless found to be consistent with the General Plan." The City adopted its city-wide General Plan in 2016, which set forth the goals, policies, and directions the City will take in managing its future. It is the blueprint for development and a guide to achieving the long-term city-wide vision. In the General Plan, Westminster Mall is a cornerstone of the City's land use plan and the focus of the City's future growth and development. The General Plan identifies the Specific Plan Area as Mixed Use - Westminster Mall, and the Specific Plan is consistent with the population density and intensity maximums permitted by the General Plan.

## WESTMINSTER MUNICIPAL CODE

The Zoning Regulations, Title 17 of the Westminster Municipal Code (WMC) regulates land use development in the City of Westminster. In each zoning designation, the regulations specify the permitted and prohibited uses and the development standards. This specific plan was adopted by ordinance, and becomes an independent set of zoning regulations that provide specific direction to properties in the project area. In cases where this Specific Plan is silent, the Community Development Director or their designee will make an interpretation based on the intent of the provisions of the Specific Plan (except under *Permitted Uses*, Chapter 4, where it identifies uses not listed in the Specific Plan are explicitly prohibited).

## WESTMINSTER PARKS MASTER PLAN

The City of Westminster Parks and Recreation Facilities Master Plan was adopted in Spring 2020. The Parks Master Plan is a tool that inventoried the City's park assets and is intended to serve as an implementing tool to achieve the parks and recreation goals and objectives identified in the WMSP.

The City of Westminster is underserved for parkland, and specifically the area near the Mall. The Parks Master Plan identifies the Mall as one of the City's primary opportunities to add much needed park land and provide a new urban open space experience for nearby residents, business employees and patrons, commuters, and visitors. The Specific Plan allows for new residential development, which previously was not a permitted use. As new residents are introduced into the area, the demand for parks and recreation opportunities will increase significantly and as a result, are a critical requirement of any new project's future placemaking features.

The Parks and Recreation Facilities Master Plan reinforces the Specific Plan's requirement for the development of two large open spaces at the Mall (ranging from 2-3 acres in size) and improvements to (and

connections with) existing and future trails. Information regarding the open space requirements for the WMSP can be found in Chapter 5, *Development and Design Standards*.

## CALIFORNIA ENVIRONMENTAL QUALITY ACT COMPLIANCE

The Westminster Mall Specific Plan EIR is a Program EIR. As provided in Section 15168 of the CEQA Guidelines, a Program EIR may be prepared on a series of actions that may be characterized as one large project. The Specific Plan establishes an overall development program that can be characterized as one large project, but its implementation will require a series of future discretionary actions (approvals of specific projects) by the City of Westminster. The Specific Plan Program EIR is intended to serve as the primary environmental document for all future entitlements (later activities) associated with implementation of the Specific Plan, including all discretionary approvals requested or required to implement the project.

Pursuant to Section 15168 of the CEQA Guidelines, a later activity under the Specific Plan development program must be examined in the light of the Specific Plan Program EIR to determine whether additional environmental documentation must be prepared. For each later activity the City must complete an environmental analysis to determine if the environmental impacts of the activity were evaluated within the Specific Plan Program EIR. Because these later activities are not new projects as defined by CEQA, compliance for each impact category is narrowed to a determination as to whether the activity would result in: (1) substantial change from the previous analysis; (2) result in a more severe impact; or (3) create a new significant impact. Based on the results of the analysis, the City will determine which of the following actions is applicable to the later activity:



1. The later activity is a component of and consistent with the Specific Plan and has been previously analyzed as a part of the Specific Plan Program EIR and findings certified pursuant to the CEQA Guidelines. No additional CEQA documentation is required (CEQA Guidelines Section 15168).
2. The later activity is a component of the Specific Plan and has been previously analyzed as a part of the Specific Plan Program EIR and findings certified pursuant to the State CEQA Guidelines; however, minor technical changes or additions are needed to make the previous documentation adequate to cover the project. An Addendum to the Specific Plan Program EIR is required (CEQA Guidelines Section 15164).
3. The later activity is either not a component of the Specific Plan or has not been previously analyzed as part of the Specific Plan Program EIR, in which case an initial study and additional environmental review under CEQA will be required unless the later activity is exempt under CEQA.

In addition, future development projects within the Specific Plan Area are eligible for streamlining under CEQA Guidelines Section 15183.3, effective January 1, 2013. To be eligible, a project must:

4. Be located in an urban area on a previously developed site or surrounded by urban uses (75 percent of perimeter);
5. Satisfy performance standards in CEQA Guidelines Appendix M; and
6. Be consistent with the general use designation, density, building intensity, and applicable policies in the Southern California Association of Governments Sustainable Communities Strategy.



## 2.5 ORGANIZATION OF THE SPECIFIC PLAN DOCUMENT



### CHAPTER 1. Introduction

Outlines the community's aspirations for the project area including an overview of the community engagement process and the WMSP Vision and Objectives.



### CHAPTER 2. Purpose and Authority

Covers the purpose of the Specific Plan, requirements for environmental review, and community outreach.



### CHAPTER 3. Community Structure and Land Use Plan

Identifies community features and considerations, the land use concept, and general development distribution.



### CHAPTER 4. Permitted Uses

Details the uses permitted, conditionally permitted, and precluded in the project area.



### CHAPTER 5. Development and Design Standards

Provides required standards such as building height, density, parking, and landscaping requirements.



### CHAPTER 6. Mobility

Provides context-sensitive streetscape and network design for motorized and non-motorized transportation.



### CHAPTER 7. Design Guidelines

Guides physical design related to site configuration, public spaces, and building design.



### CHAPTER 8. Public Services and Infrastructure

Focuses on phasing strategies and the major infrastructure systems including: storm drain, sewer, and water.



### CHAPTER 9. Administration and Implementation

Provides the process for project approvals, funding and financing mechanisms, phasing and a list of implementation actions.



### Appendices A and B

Provides a glossary of terms and an overview of the Specific Plan's consistency with the General Plan.

*Additional analyses were conducted in association with the Westminster Mall Specific Plan and are available under separate cover. They include the Initial Study and Environmental Impact Report, which incorporate the environmental documentation and traffic impact analyses prepared for the project.*



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# Chapter 3

# **Community Structure and Land Use Plan**





WESTMINSTER  
MALL

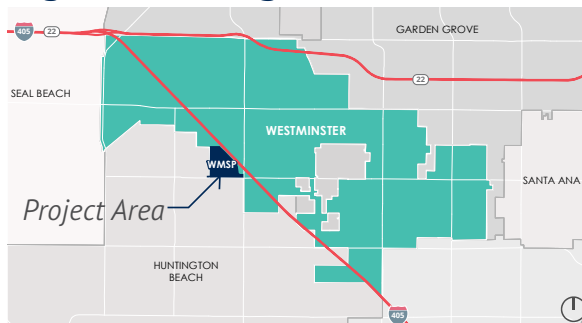
### 3.1 LOCATION

The Westminster Mall area is on the western side of the City of Westminster in Orange County. It is bordered by the City of Huntington Beach on the south and west as shown in Figure 3.1, *Regional Location*.

As shown in Figure 3.2, *Local Vicinity*, the Westminster Mall Specific Plan Area is approximately 100 acres in size and located on the south side of Interstate 405 (I-405) corridor bounded by Bolsa Avenue to the south, Edwards Street to the west, Goldenwest Street to the east, and the Navy Rail Easement to the north.

The WMSP Area is in close proximity to local educational and employment centers. WMSP is located one-half mile north of Golden West College, a community college with approximately 17,000 students, in addition to a number of elementary, middle, and high schools. Employment centers within a mile of WMSP, include the Boeing campus and Bella Terra, a redeveloped outdoor mall in Huntington Beach.

**Figure 3.1: Regional Location**



**Figure 3.2: Local Vicinity**



## 3.2 EXISTING CONDITIONS

Built in the 1970's, the Westminster Mall was considered "the place" to go for many residents in Orange County. Once the location of a working goldfish farm, the iconic Mall next to the I-405 Freeway served as a sign of progress and economic vitality. In recent years, the mall has experienced the effects of a shifting retail marketplace with the closures of several anchor and big box tenants such as Sears and Babies R Us, providing an opportunity to redesign and reposition the Mall into a modern, mixed-use community and thriving activity center. This shift provides an opportunity to integrate new uses onto the site that would best serve Westminster residents and have a draw from the greater region.

Following is a brief description of the land use, community structure features, including gateways, corridors, edges, and pathways, open space, and mobility that influence future development opportunities in the WMSP Area and shaped direction provided in the Land Use Plan, Development Standards, and Design Standards and Guidelines.

Figure 3.3: Existing Site and Uses







*The Mall site is surrounded by predominantly single family residential uses in Huntington Beach (shown in yellow).*



*Existing uses include anchor retail stores such as Macy's (top) and JCPenney (bottom).*



## SURROUNDING LAND USES

The WMSP Area is bordered by established single-family residential neighborhoods in Huntington Beach south of Bolsa Avenue and west of Edwards Street. Ada Clegg Elementary and Helen Stacy Middle School are located west of the project area in the adjacent neighborhood, and share access to the area at the Edwards Street and Mar Vista Drive and Royal Oak Drive intersections. The WMSP Area also includes an easement that the City holds over the northernmost section of the Specific Plan Area, which has been reserved for a future trail (see Chapter 6, *Mobility* for more information regarding future improvements planned). US Storage Centers and All American Asphalt are two industrial uses located just north of the project area along Edwards Street (north of the Navy Rail easement) and also share freeway frontage with the Mall site.

## PARCEL CONFIGURATION AND PROPERTY OWNERSHIP

Although it may change over time, at the time of the drafting of this specific plan, the 100-acre WMSP Area is owned by four different property owners. Additionally, two of the parcels include very long-term ground leases for Mall anchors. Moving forward, coordination among the property owners will be necessary to support the master planning efforts required to redevelop the site in a cohesive manner as required by this WMSP.

## EXISTING STRUCTURES

There are several existing structures on the site, the most prominent of which is located at the center of the property - the Westminster Mall building - which was built in the early 1970's. The main building is surrounded by several outlying retail pads on the perimeter of the site, some of which are big box retail pads (Edwards Street) and other areas have been developed with smaller building footprints to accommodate restaurant and banking uses (Bolsa Avenue). As new uses are proposed, they may locate in existing structures, adapt existing buildings to be reused for a new use, or existing buildings may be demolished and replaced with new structures to make more efficient use of the site. The existing retail and restaurant uses comprise a total of approximately 1,360,000 square feet of non-residential uses on the site.



*The drainage channel easement will be an opportunity to provide a unique frontage edge along Bolsa which may include open space and /or a frontage road*

## DRAINAGE EASEMENTS

An approximate 72-foot wide area owned by the Orange County Flood Control District (OCFCD), which includes a 25-foot wide drainage easement and 47-feet of OCFCD owned property, runs along the southerly edge of the project site (parallel to Bolsa Avenue). No structures are permitted to be constructed over the easement, however parking lots, open spaces, trails, and landscaping are allowed. Any new development along this edge must take the easement into consideration when proposing new buildings. More information regarding drainage on the site can be found in Chapter 8, *Public Services and Infrastructure*.

## MOBILITY

The WMSP project area can be directly accessed from the southbound I-405 Freeway via an offramp into the site which will accommodate the majority of vehicular access generated by uses there. The freeway right-of-way was recently expanded and is designated as a Caltrans Landscape Corridor in this area. Ongoing coordination with Caltrans will be required for the project over time regarding landscaping, the freestanding sign, and other issues affecting the interface along the freeway edge.

In addition to the freeway, the project area is also accessed by two local roadways: Bolsa Avenue, an arterial road, and Edwards Street, a connector road. Roadway sections for both streets can be found in Chapter 6, *Mobility*.

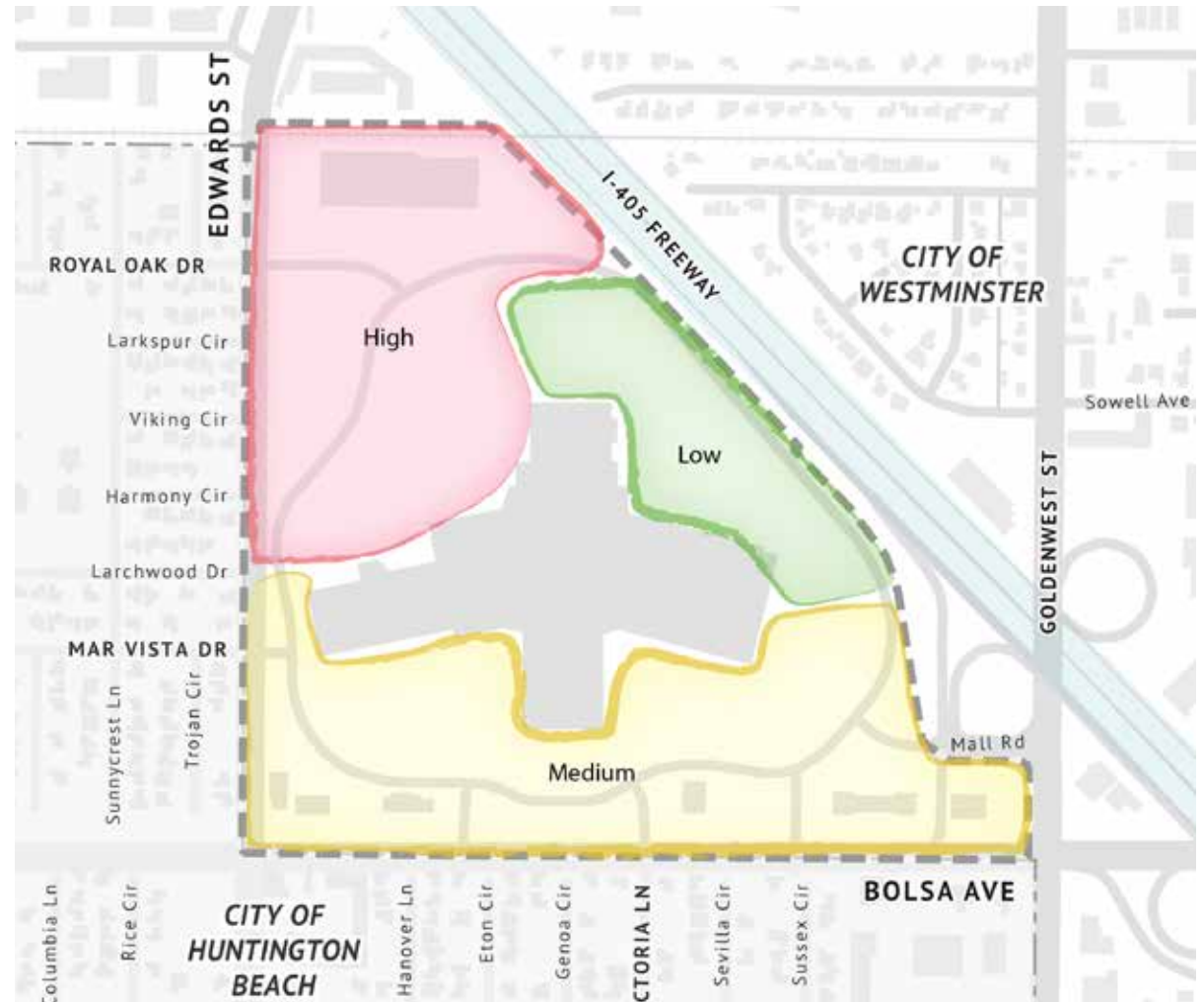
The Orange County Transit Authority (OCTA) currently operates Routes 64 and 64x, providing transit services from the WMSP site to the Tustin area along Bolsa Avenue (Bolsa-Victoria stop). Another bus stop is located on the Mall side of northbound Edwards Street (Edwards-Mar Vista stop). Any future redevelopment of the WMSP Area must take into consideration the current location of the stops and relocate them as appropriate if necessary.

The WMSP project site is also within a Southern California Association of Governments (SCAG) High Quality Transit Area (HQT) which qualifies future projects for a more limited environmental review process.

## TOPOGRAPHY

The site's topography changes from street level to the rear of the site near the I-405 freeway and can be grouped into three major areas. Given the large nature of the existing parking lots and the significant variation of surface elevation present in various areas of the site, there are three generally distinguishable topographic patterns on the site (high, medium and low) as depicted in Figure 3.4. Each of these areas is clearly defined by a slope embankment between them. The northeast area adjacent to the I-405 Freeway is the lower of the three areas identified. The three major areas are most easily identified and reflected in the existing parking lots that surround the mall. The area northeast of the mall adjacent to the I-405 Freeway is generally lower than the other two adjacent areas, with downward slopes from each to this area (Figure 3.4 identified as "low"). The southern area, adjacent to Bolsa Avenue, is elevated on the east and slopes downward toward the intersection of Bolsa Avenue and Edwards. The northernmost portion of the site along Edwards and the future Westminster Nature Activity Trail slopes downward to the east and south.

Figure 3.4: Site Topography



### 3.3 COMMUNITY STRUCTURE

A priority of the WMSP is to develop a comprehensive plan and vision for the area that avoids haphazard development of the parcels on site and to instead create a unified master plan.

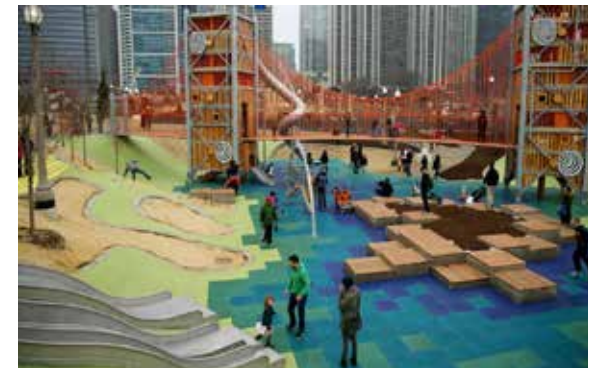
One of the challenges of redevelopment of the site is that it is owned by five different owners, each with their own priorities, timing, and/or site constraints, which may compete or differ with each other. Regardless, all future development will require coordination among neighboring owners to achieve the vision of the WMSP, particularly as it relates to open space and mobility.

Previously, under the existing development model, each parcel operated somewhat independently of each other, without the benefit of a clear understanding of the collective goals the individual actions were intended to achieve. This WMSP provides guidance for future growth from a holistic approach developing clear direction, strategies, standards, and guidelines that take into consideration the interrelationship between all uses and parcels in the area and how they could affect one another.

This section of the Specific Plan provides context for land use refinements incorporated into the Plan (i.e., what was changed to implement the WMSP Vision and what remained the same) and summarizes the features that comprise WMSP's community structure such as: gateways, edges and views, special uses, and proposed activity areas (two new parks). It also outlines the aspects of each feature that should be considered, enhanced, preserved, refined, or avoided. The land use plan, development standards, mobility plan, and design standards and guidelines included in subsequent chapters of the Specific Plan are also intended to implement the big ideas and goals related to community structure as discussed in this section.



*Public plazas with distinct artwork provide a unique sense of place and identity for WMSP.*



*Urban parks are a required element of the WMSP and will provide new recreational opportunities and activity spaces for residents living in the area.*



*Pedestrian corridors with activated frontages and uses promote walkability.*

**Figure 3.5: Gateway Locations**



### 3.3.1 GATEWAYS

The site has two primary gateways at the I-405 freeway off-ramp and along Bolsa Avenue, and secondary gateways along Bolsa Avenue and Edwards street. The intent of the long-range plan is to formalize primary gateways with signage and enhanced traffic flow. The intention at secondary gateways is to limit traffic flow, and reduce cut through traffic in adjacent neighborhoods. There is an opportunity to develop an exclusive pedestrian and bicycle gateway at the northern Westminster Nature Activity Trail. This gateway should interface with the site's internal pedestrian network.



*Wayfinding signage at key entryways helps visitors to navigate the site.*



*Vibrant colors and night time lighting make gateways and entries clearly visible.*



*Use of gateway signage to clearly identify entry into the WMSP from Bolsa Ave. and Edwards St. or to delineate transitions between uses or districts internally.*

### 3.3.2 EDGES AND VIEWS

#### EDWARDS

The scale of the Edwards Street edge should respond to the surrounding land uses and changing topography. An opportunity is to develop this as a residential edge, thereby creating a more cohesive residential corridor on both sides of the street. Priority should be placed on pedestrian and bicycle access, as outlined in the City’s General Plan and landscaping should be used as a means to enhance aesthetics, soften building massing and encourage pedestrian activity.



*Architectural treatments at corners (above) and building framed plazas (below) focus and capture views into WMSP.*



#### BOLSA

Along Bolsa Avenue there is a 72-foot area owned by OCFCD which includes a 25-foot drainage easement that prohibits the development of buildings. This provides the opportunity to develop this area as the front yard of the site with internal circulation, linear parks, and private open space. Views into the site should prioritize a variety of high-quality experiences that brand the mall as a unique development where people want to be. Prominent views and associated architectural features and open space should be aligned with the primary access at the Bolsa gateway.

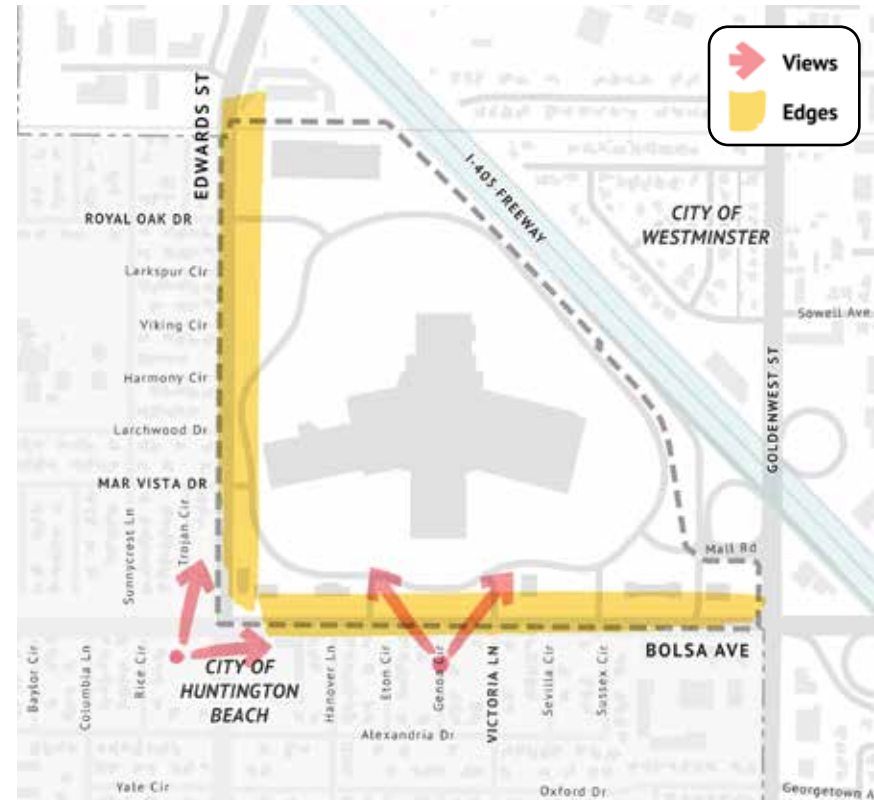


*Vertical mixed-use is one practice that will serve to create an active edge along certain corridors.*

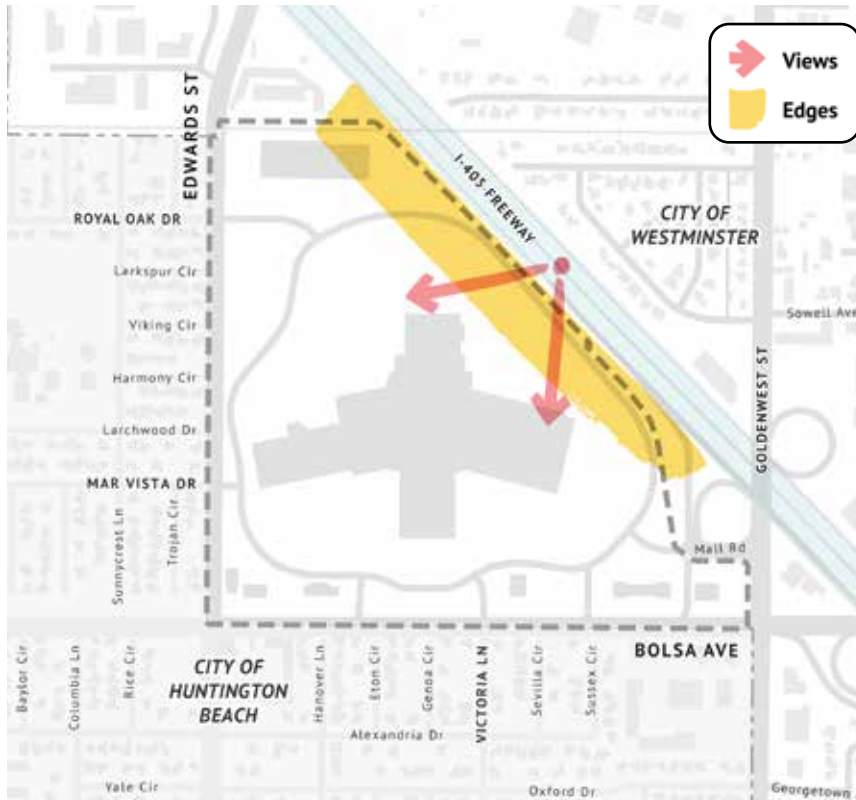


*View corridors should be framed to provide aesthetic interest and visibility.*

**Figure 3.6: Bolsa And Edwards Edge**



**Figure 3.7: Freeway Edge**



Existing view into the WMSP looking west from the I-405 Freeway.

**FREEWAY**

The I-405 Freeway runs adjacent to the site and due to existing topography, looks down into the existing surface parking lots at the east side of the site. There is an opportunity to reframe the views into the site to create excitement and interest in the commercial, open space, and cultural offerings of a redeveloped mall. Conceptual improvements include landmark icon signage, views into public open space, and artistic lighting.



*Light Patterns on Buildings  
Buildings incorporating light  
and color to enhance WMSP  
identification from the freeway.*



*Artwork should be viewable from  
the freeway.*

### 3.3.3 CIRCULATION

The internal circulation on the site will be multi-modal and prioritize pedestrian orientation where feasible. Primary circulation will be organized around a roadway network that provides access to all portions of the project while minimizing cut-through traffic and will be oriented to align with the primary gateways and proposed developments.

The objective of the circulation system is to accommodate planned traffic volumes, calm and slow traffic, minimize cut-through traffic, and encourage pedestrian and bicycle travel. It should also allow pedestrians, bikes and vehicles to travel through the site without having to leave the property to get to the other side.

Future developments will need to be coordinated and phased around the primary circulation to ensure that utilities, emergency access, pedestrian connectivity and other infrastructure are adequately provided, and undue expense is not incurred. Chapter 6, *Mobility*, should be referenced for more detailed requirements related to internal and external roadway alignments, street sections, site access and traffic calming measures.

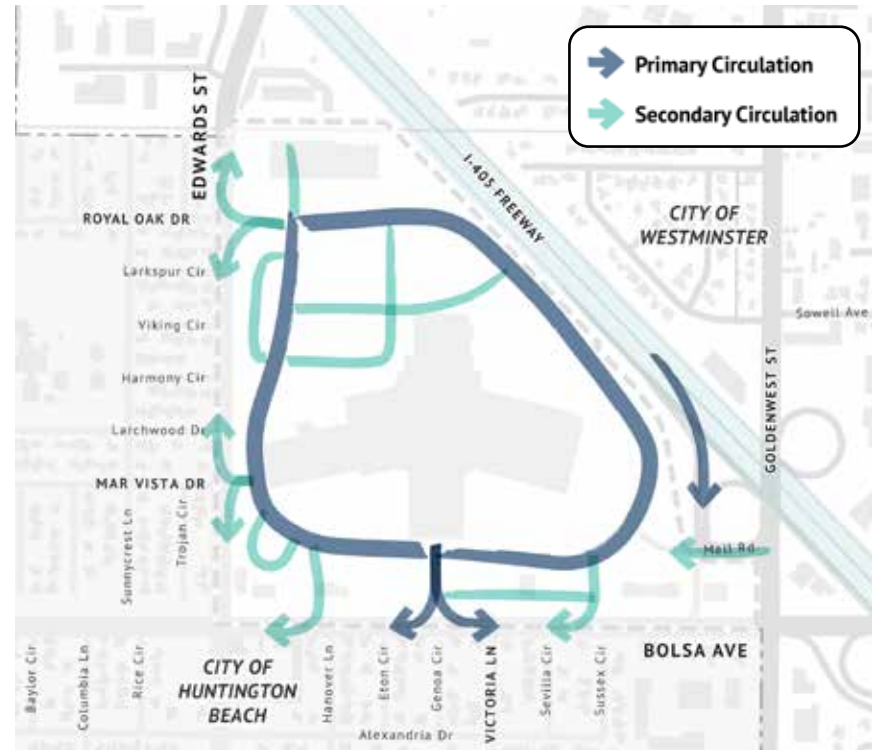


*A hierarchy of streets will provide parking and movement throughout WMSP (above and below).*



*Paving treatments improve visibility of pedestrian crossing areas.*

**Figure 3.8: Circulation**



*Note: Conceptual illustration of internal circulation system. Final configuration will be developed and reviewed upon project submittal.*



*Complete streets will offer visitors and residents ease of access throughout WMSP.*



**Figure 3.9: Open Space**



*Note: The precise locations of the 3-acre Urban Park Plaza and the 2.5-acre Neighborhood Park are shown above. The precise location and boundaries of these two parks may shift as the build-out process unfolds so long as alternative sites of equivalent size are identified and secured. In contrast, the locations of the Internal Community Paseo and Bolsa Promenade are conceptual and are not considered definitive.*



*Courtyards (Above) and forecourts (Below) act as transitions between spaces and provide open space.*



*Larger plaza spaces can function for day-to-day activities and as planned event space.*



*Rooftop decks can also function to satisfy some open space requirements.*



*Play equipment and seating should be included in some open spaces to allow for recreational uses.*

### 3.3.4 OPEN SPACE

There is a demand and need for open space in the City of Westminster. The approximately 100-acre site provides an opportunity to create usable open spaces ranging from small to large scale. Uses can include places to eat, rest and relax, view art, play, exercise, or hold public events. Nine-and-a-half acres of new parks are required in the WMSP and an additional seven-and-a-half acres of publicly accessible open space to address the park and open space deficit in this area of town, consistent with the goals of the City's General Plan and the City's adopted Park and Recreation Facilities Master Plan. Parks and open spaces should be designed and placed on the site to maximize usability by the largest number of users and should be generally distributed throughout the WMSP Area. Internal pedestrian connectivity is also required to facilitate access to each open space. Connections can include paseos, sidewalks and other means of pedestrian access. For more detail on the specific locations and sizes, see Figure 5.15, *Open Space Location and Sizing*.

## 3.4 LAND USE OVERVIEW

The Westminster Mall Specific Plan Area will be designated Specific Plan (SP-3) in the Westminster zoning map and will regulate the project area through the provisions of the WMSP which will allow a mix of uses. Within the mixed-use designation, several preferred uses are identified with specific development standards (Chapter 5) and land use patterns.

Since the WMSP is an entirely mixed-use site, the Specific Plan provides a great deal of flexibility regarding the placement and type of uses permitted throughout the WMSP. Because the Specific Plan generally allows for flexibility in placement of residential, retail, office, hospitality, and entertainment uses located on site, it relies heavily on the design and placement of uses (regulated by the development standards and design standards and guidelines in Chapters 4, 5 and 7) to create a quality development. Projects may propose a single use type (horizontal mixed-use) or may be vertically mixed (ground floor retail over residential, for example) so long as the required mix of uses identified for the site is maintained. The distribution and desired priority of uses of new development envisioned in the WMSP project area are illustrated in Figure 3.10, *Land Use Concept* and Figure 3.11, *Illustrative Concept*.

### 3.4.1 LAND USE APPROACH

The *Land Use Concept* (Figure 3.10) identifies the types of uses desired in certain areas of the WMSP based upon considerations such as the scale of development allowed (maximum heights), proximity to primary or secondary gateways into the project site, and proximity to the freeway and adjacent single family neighborhoods. The land use concept also identifies uses that are viewed as priority uses in various locations on the WMSP site (these uses should be considered first by the applicant). The uses labeled in Figure 3.10 are a representation of the preferred areas where land uses should be located on the site.

Table 4.1, *Permitted Land Uses*, identifies specifically where uses are allowed within the site and identifies the level of entitlement review required to establish each use.

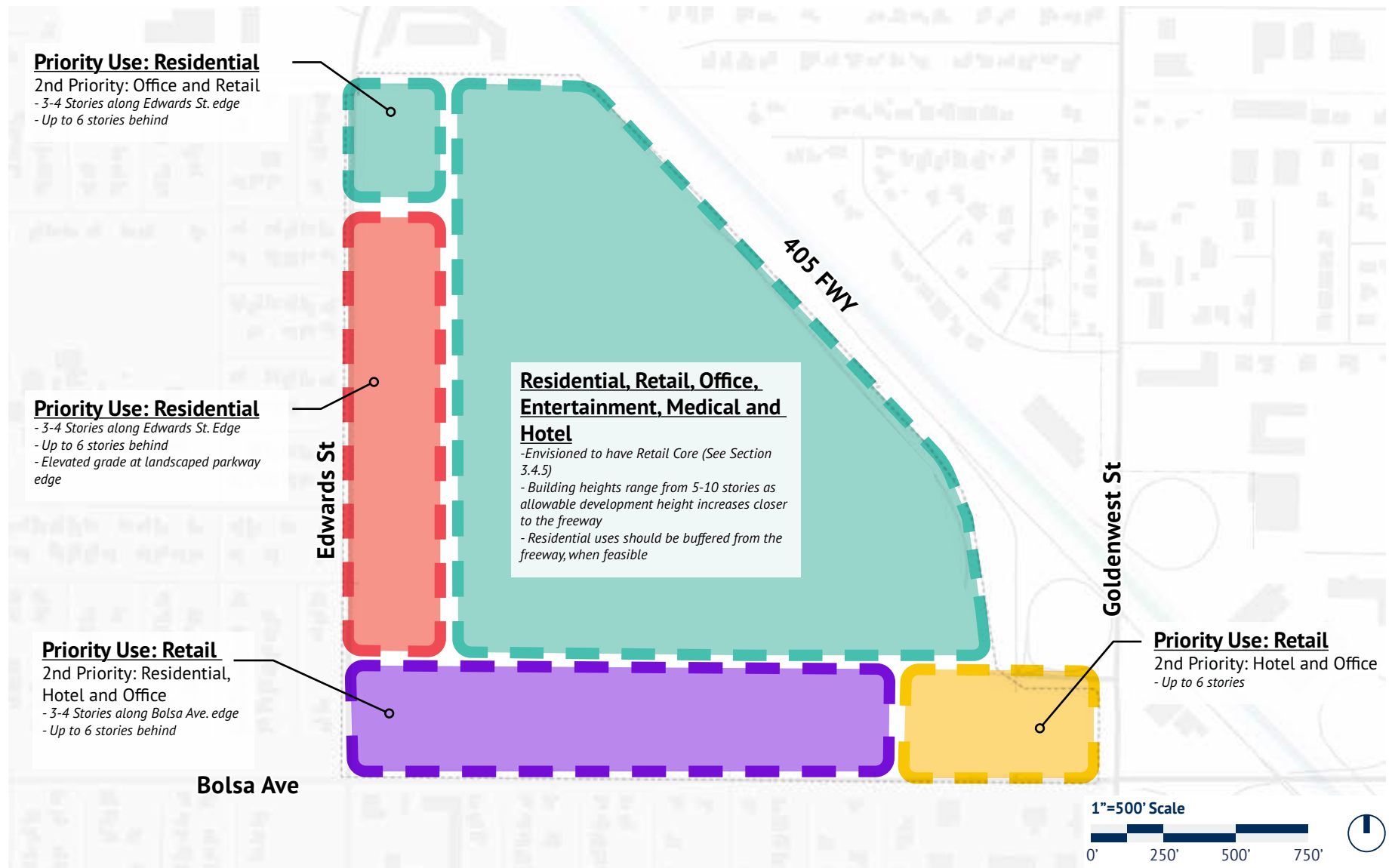
Because Bolsa Avenue will serve as the primary entry into the WMSP (other than the freeway) it's envisioned that a greater mix of non-residential uses would be located on this edge of the site to create an active gateway into the project area. The Edwards Street edge is viewed as a secondary entry into the WMSP project area and therefore the mix of uses on that edge are envisioned to be primarily residential in nature. The core of the WMSP is envisioned to accommodate a mix of taller buildings, including office, medical office, and residential, with retail, park and entertainment uses playing a prominent role in creating an active internal space along with integrated park amenities for the WMSP.

Figure 3.10 also provides locational references for the typical heights envisioned for each area of the plan, providing a general representation of the application of the specific height requirements identified in Figure 5.14, *Maximum Building Height*. Clustering of uses, as shown in the *Land Use Concept*, can help create internal "districts" of activity and compatible uses and can also be a way to provide logical transitions in scale or a transition from one use type to another.

### 3.4.2 ILLUSTRATIVE CONCEPT

Figure 3.11, *Illustrative Concept*, represents a conceptual illustration of what one example of development in the WMSP could look like. The graphic was prepared to show how various uses, circulation and open space concepts required in the WMSP could be designed and integrated on the site. This graphic is not intended to represent the final design of future development (each property owner will submit designs for their individual parcels, which may vary), but rather demonstrates the interdependent relationship of the redevelopment of all parcels to create a cohesive experience site-wide and to achieve the WMSP Vision.

**Figure 3.10: Land Use Concept**



**Notes:**

The concept shown above is intended to be illustrative in nature. Since the entire project site allows for a mixing of uses, the intent of this graphic is to illustrate the general location and preferred priority of specific uses and locations where various heights are permitted on the site. The list of specific uses permitted on the site and entitlements required for each use can be found in Table 4.1, *Permitted Land Uses*. Details related to height regulations on the site can be found in Section 4.3.12, *Building Height and Floor Height*. Additional details related to the various design considerations required for site access and internal and external circulation can be found in Chapter 6, *Mobility*. Final project uses and design including: building locations, footprints, and ultimate site plan configuration will be based upon final concepts approved by the City during the entitlement process and may not necessarily reflect the same uses as illustrated above.

**Figure 3.11: Illustrative Concept**



**Notes:**

The concept shown above is intended to be illustrative in nature. Since the entire project site allows for a mixing of uses, the intent of this graphic is to illustrate one way that the application of uses permitted in the SP could look upon buildout. Details related to height regulations on the site can be found in Section 4.3.12, *Building Height and Floor Height*. Additional details related to the various design considerations required for site access and internal and external circulation can be found in Chapter 6, *Mobility*. Final project uses and design including: building locations, footprints, and ultimate site plan configuration will be based upon final concepts approved through application submittal approved by the City and may not necessarily reflect the same uses as illustrated above.

### 3.4.3 DEVELOPMENT CAPACITY

The amount of development (square feet, units, or hotel rooms) permitted on properties located within the WMSP is listed in Table 3.1, *Land Use Statistical Summary*. The WMSP EIR evaluates impacts to the project site using a maximum of 3,000 dwelling units, 1,200,000 square feet of non-residential uses and 425 hotel rooms as identified in Table 3.1. This mix of uses proposed is based on preliminary design studies to evaluate potential development yield (what can fit on the site) and also on reasonable assumptions for future development with consideration of current market conditions. These numbers have also been used as the basis for the project description and environmental analyses conducted in the EIR.

The WMSP aims to strike a balance of construction of new non-residential and residential uses. Since the site is primarily envisioned to be an activity and jobs center for the community, and because the City needs to ensure that it is maintaining high enough revenues from non-residential uses to maintain city-wide fiscal sustainability over time, residential uses can be integrated into the project area, but

**Table 3.1: Land Use Statistical Summary**

Land Use	Dwelling Units	Non-Residential Sq Ft	Hotel Rooms <sup>1</sup>
Existing Buildings	0	1,360,000	0
Proposed Uses in WMSP	3,000	1,200,000	425
<b>Change</b>	<b>3,000</b>	<b>-160,000</b>	<b>425</b>

*Notes:*

1. *This statistical summary does not include square footage for hotel development; hotels are shown by number of rooms, not square footage.*
2. *The summary provided above is based on reasonable assumptions of future development. Individual projects will be reviewed on a case-by-case basis during the application review process.*

should not be the majority use at the expense of creating a vibrant and economically strong retail and entertainment destination. The unit count and square footage mix identified in Table 3.1 is to be used as the guide to the desired mix of residential to non-residential uses. Because the project will be developed in phases, it is recognized that over time there may be a need to adjust the unit count or square footage of uses allowed within this Specific Plan to reflect the projects submitted and approved by the City.

### 3.4.4 TRIP ALLOCATION

The Specific Plan has built in flexibility to allow for minor adjustments to the ultimate unit count of residential uses and the square footage of non-residential maximums without requiring a specific plan amendment, if the sum of all the projects within the Specific Plan can demonstrate they meet the intent of the Specific Plan *Vision and Objectives*. To do this, the mix of uses will be regulated by trips generated by uses located in the WMSP site.

Projects will be regulated by a maximum number of trips allocated to the WMSP site. The maximum number of trips identified for the WMSP was based upon the unit and square footage maximums identified in Table 3.1. When a project is submitted for review and consideration, it must identify the number of trips the mix of uses proposed will generate and must also demonstrate that there are enough remaining trips left in the overall trip budget capacity for the WMSP Area to accommodate the new uses. Section 9.3, *WMSP Trip Budget*, provides further information regarding trip budget capacity and implementation for the WMSP. This approach allows for slight variations in the ultimate buildout of the uses in the plan (total units or total square footage) without the need to process a specific plan amendment development based upon current market conditions. These numbers have also been used as the basis for the project description and environmental analyses conducted in the EIR.

### 3.4.5 RETAIL PRESERVATION

The WMSP seeks to maintain a vibrant retail sector as well as encourage hotel development. The benefits that retail and hotel land uses can provide, include:

**Community access to commercial amenities:** Local retail and hotel offerings provide residents, employers, and workers access to services and amenities that improve the overall quality of life and business environment of the City;

**Economic diversity:** The retail and hospitality sectors support the City's economic base;

**Reduced vehicle miles traveled:** Retail and hotel offerings integrated within the Plan area can reduce the need for residents and workers to drive elsewhere to access these services;

**Synergy with other WMSP land uses:** Retail and hotel uses can enhance the marketability and value of the residential and office uses within the Plan;

**Fiscal benefits to City:** Retail sales and transient occupancy (or hotel) taxes are an important contributor to the City's General Fund budget, provided revenues that help offset the cost of vital municipal services (e.g. public safety).

The Specific Plan requires a minimum of 600,000 square feet of retail space be maintained in the project area. Figure 3.12, *Retail Overlay Area*, illustrates the locations in the Specific Plan where retail uses must be maintained.

The *Retail Preservation Policy* (described in Section 9.4) provides the mechanism by which retail uses shall be provided by property owners and implemented in the WMSP and should be reviewed prior to the creation of any new development proposal in the Specific Plan Area.



*Where residential is the primary use, retail can be incorporated at the ground level to provide a more vibrant street scene.*



*Where retail is a primary use, office or residential with alternate access can create a live/work environment that keeps the buildings active and reduces the need for vehicular travel.*

Figure 3.12: Retail Overlay



Note: Actual boundaries subject to change based on projects ultimately submitted by property owners





# Chapter 4

## **Permitted Uses**





## 4.1 PERMITTED USES

### 4.1.1 OVERVIEW

This Chapter sets out the permitted uses to ensure all proposed projects in the Specific Plan support the Westminster Mall Vision and Objectives outlined in Chapter 1. This chapter is applicable to all property within the Specific Plan Area and outlines the entitlements and required development standards that must be applied to projects within the Specific Plan.

#### ALLOWABLE USE AREAS

The WMSP project has been divided into “use areas” that affect the type of entitlement required depending on the location of the proposed use. Figure 4.1, *Allowable Use Areas*, identifies the locations of the areas that coincide with the columns identified in Table 4.1.

For example, some uses are more appropriate for internal locations of the site (versus exterior edges along Bolsa Avenue and Edwards Street) due to noise or other design or compatibility-related reasons.

In addition, for uses that are located internal to the site and furthest away from existing residential uses, a lower level of entitlement review may be required because the internal location of the use provides a natural buffer due to the location of the use and the design of the surrounding buildings.

#### PERMITTED USES

Table 4.1, *Permitted Land Uses* provides a list of the permitted, conditionally permitted and prohibited uses that are allowed in the WMSP. It also identifies areas (Area A, B C, or D) where the uses may be located and the level of entitlement required depending on

the location of the Specific Plan site. Chapter 9, *Administration and Implementation*, contains additional provisions regarding the project review and approval process.

It is anticipated that most of the uses proposed in the WMSP project area will comply with the definitions associated with similar classifications that exist in the City’s Zoning Code (code sections have been referenced after each use, where applicable).

However, since the type, scale and quality of development and master planning proposed in the WMSP is unlike any other area of Westminster, additional uses and provisions have been included in this section that expand upon the uses in the WMC and have been customized to the urban environment that is envisioned here.

For uses that are proposed right on the border of one of the Use Areas identified above, the Community Development Director has the authority to make a determination as to what level of entitlement is required.



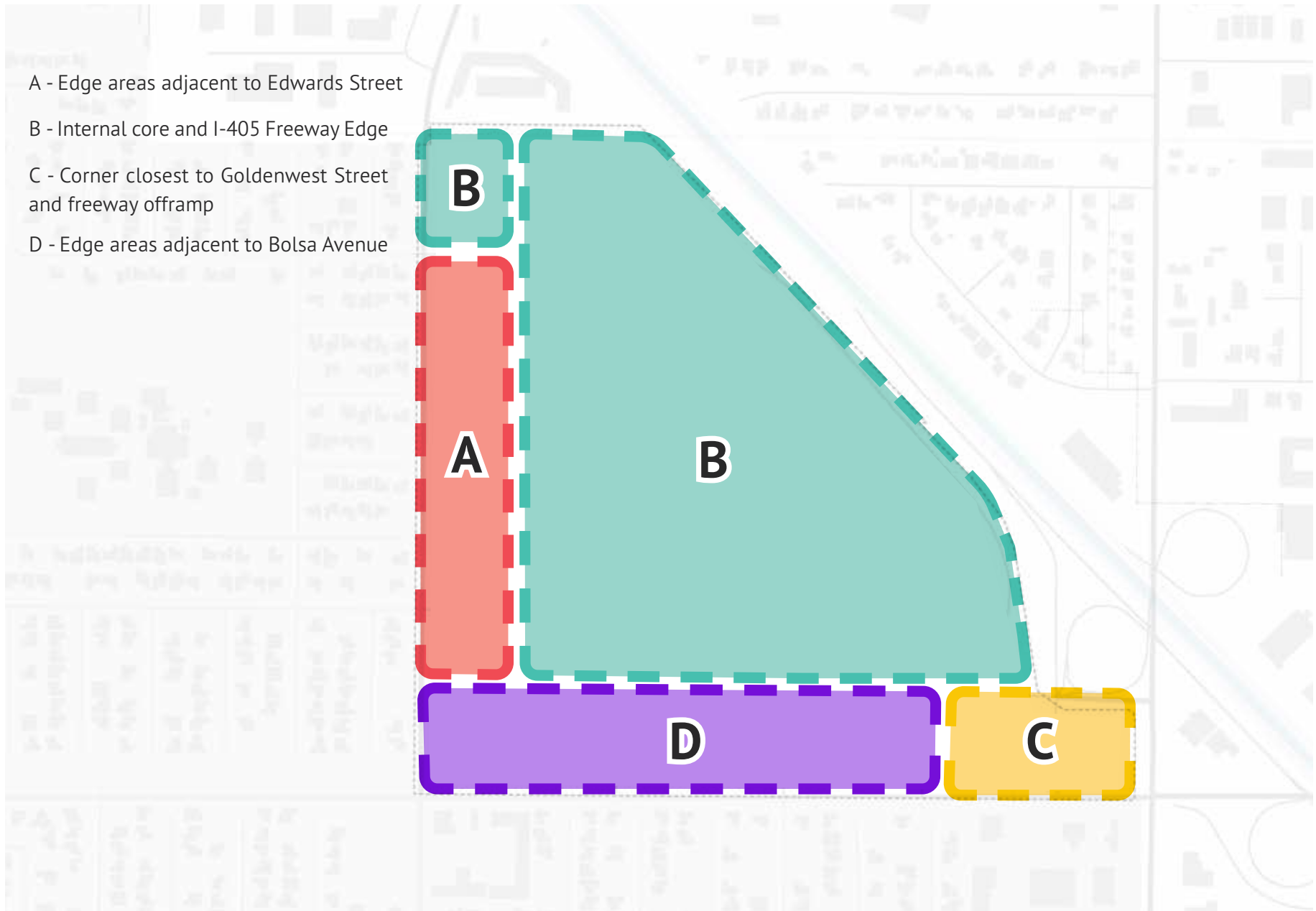
**Figure 4.1: Allowable Use Areas**

A - Edge areas adjacent to Edwards Street

B - Internal core and I-405 Freeway Edge

C - Corner closest to Goldenwest Street and freeway offramp

D - Edge areas adjacent to Bolsa Avenue



**Table 4.1: Permitted Land Uses**

Land Use	Use Area Permit Requirements				Notes
	A	B	C	D	
<b>Recreation, Education and Public Assembly</b>					
Adult day care centers	C	C	C	C	
Broadcast offices for radio, television, internet	--	A	C	C	
Cafe, coffee house, tea house <sup>1</sup>	C	C	C	C	Subject to § 17.400.030
Child day care centers	C	C	C	C	Subject to § 17.400.035
Clubs, lodges, and fraternal organizations	--	C	C	C	
Dance halls, reception halls, banquet facilities or related uses	--	C	C	C	
Entertainment – in conjunction with eating and drinking establishments	--	A	A	A	Subject to § 17.400.055
Family day care home, large	A	A	A	A	Subject to § 17.400.035
Family day care home, small	P	P	P	P	Subject to § 17.400.035
Farmer's Market	--	A	A	A	
Hotels (interior hallway room access)	--	P	C	P	
Indoor amusement/entertainment facilities (i.e. bowling, ice skating)	--	A	A	A	
Outdoor commercial recreation	--	C	C	C	
Private schools	--	C	C	C	
Publicly owned recreational and cultural facilities	--	P	P	P	
Private recreational and cultural facilities	P	P	P	P	

P = Permitted Use; A = Administrative Use Permit Required; C = Conditional Use Permit Required; -- = Prohibited Use  
 T = Temporary Use Permit



Land Use	Use Area Permit Requirements				Notes
	A	B	C	D	
Religious places of worship	--	C	C	C	
Eating and Drinking Establishments (except bars, cafes, nightclubs) <sup>2</sup>	--	P	P	P	See also Alcohol Consumption (on site, and off site)
Theaters and auditoriums	--	C	C	C	
<b>Residential</b>					
Dwelling Units (more than 4 stories)	--	P	C	P	See section 4.3.12 related to height Subject to § 17.400.125 Subject to Section 9.4 Retail Preservation Policy and Figure 3.12 Retail Overlay
Dwelling Units (4 stories or less)	P	P	C	P	See section 4.3.12 related to height Subject to § 17.400.125 Subject to Section 9.4 Retail Preservation Policy and Figure 3.12 Retail Overlay
Emergency shelters	C	C	C	C	
Residential care facilities, 6 or fewer clients	P	P	P	P	
Residential group living, 6 persons or less	P	P	P	P	
Senior housing	C	C	C	C	Subject to § 17.400.085
Accessory Dwelling Unit and Junior Accessory Dwelling Unit	P	P	P	P	
Conversion of apartments to condominiums	C	C	C	C	Subject to § 17.400.045

P = Permitted Use; A = Administrative Use Permit Required; C = Conditional Use Permit Required; -- = Prohibited Use  
T = Temporary Use Permit

Land Use	Use Area Permit Requirements				Notes
	A	B	C	D	
<b>Commercial Service</b>					
Dry-cleaning Pickup/Drop-Off	--	P	P	P	
Government Offices and Facilities	--	P	P	P	
Laundry and/or Dry-cleaning on-site	--	--	--	--	
Massage establishments as defined in Title 5	--	P	P	P	Subject to § 17.400.095
Medical Offices <sup>3</sup>	A	P	P	P	
Professional, Administrative, and Production Offices (non-medical)	--	P	C	P	Consistent with § 17.700.010
Pet day care and grooming (no boarding)	--	A	A	A	Subject to § 17.400.025
Pet day care and grooming (with boarding)	--	C	C	C	Subject to § 17.400.025
Studios – art, dance, music, yoga, photography, gymnasium, etc.	--	P	P	P	Subject to § 17.400.153; if gymnasium proposed to operate 24 hours, AUP required.
Tutoring centers	--	P	P	P	
Veterinary clinics and animal hospitals (no boarding)	--	C	C	C	Subject to § 17.400.025
Wireless communication facilities – fully concealed	--	C	C	C	Subject to § 17.400.175
<b>Retail</b>					
Alcohol Sales (off- site consumption)	--	C	C	C	
Alcohol Sales (on-site consumption)	C	C	C	C	

P = Permitted Use; A = Administrative Use Permit Required; C = Conditional Use Permit Required; -- = Prohibited Use  
T = Temporary Use Permit



Land Use	Use Area Permit Requirements				Notes
	A	B	C	D	
Alcohol Sales (on-site consumption associated with a bona fide eating place not containing a bar or lounge)	A	A	A	A	
Antique or Consignment stores	--	C	C	C	
Bicycle sales, rental, and incidental repair	--	P	P	P	
Drug stores and pharmacies	--	P	P	P	
General retail stores less than 10,000 sq. ft.	--	P	P	P	
General retail stores more than 10,000 sq. ft.	--	P	C	A	
Grocery and Convenience stores	--	P	P	P	Subject to § 17.400.065
Liquor store	--	C	C	C	
<b>Accessory Uses</b>					
Home Based Business <sup>4</sup>	P	P	P	P	Subject to § 17.400.075
Home schooling	P	P	P	P	
Outdoor dining in conjunction with on-site restaurant	A	A	A	A	
Personal Vehicle Rental Incidental to Hotel Use	--	A	A	A	
Reverse vending machines (inside building)	--	A	A	A	Subject to § 17.400.115
Small recycle collection facility (inside building) Incidental to Grocery Use	--	A	A	A	Subject to § 17.400.115
Temporary Outdoor Uses and Events	--	T	T	T	Subject to § 17.540 and § 9.61
Temporary Outdoor Storage (incidental to on-site retail use)	--	A	A	A	Subject to § 17.400.160

P = Permitted Use; A = Administrative Use Permit Required; C = Conditional Use Permit Required; -- = Prohibited Use  
T = Temporary Use Permit



Land Use	Use Area Permit Requirements				Notes
	A	B	C	D	
<b>Prohibited Uses</b>					
Drive-thru	--	--	--	--	
Reverse vending machines (outside building)	--	--	--	--	
Industrial, Distribution and Warehousing	--	--	--	--	Includes fulfillment and last mile centers

P = Permitted Use; A = Administrative Use Permit Required; C = Conditional Use Permit Required; -- = Prohibited Use  
T = Temporary Use Permit

Notes:

- 1-Cafés, coffee houses and tea houses are permitted without a CUP if found in compliance with the provisions of Section 17.400.030
- 2 - Includes Open-air dining Restaurants and Take Out as defined in WMC § 17.700(E).
- 3 - Includes Medical Office, Clinic, and Laboratories as defined in WMC § 17.700(M).
- 4 - Requires property owner authorization.





# Chapter 5

# **Development and Design Standards**





# 5.1 DEVELOPMENT AND DESIGN STANDARDS OVERVIEW

## 5.1.1 OVERVIEW

This Chapter sets out the development and design standards that regulate new development to ensure all proposed projects in the Specific Plan support the Westminster Mall Vision and Objectives outlined in Chapter 1. This chapter is applicable to all property within the Specific Plan Area and outlines the required development and design standards that must be applied to any project. Chapter 9, *Administration and Implementation*, contains additional provisions regarding the project review and approval process.

### DEVELOPMENT AND DESIGN STANDARDS

The following development and design standards supersede all provisions, standards, and requirements of the Westminster Municipal Code (“WMC”) zoning regulations, except in those instances where the Specific Plan development standards remain silent. In instances where these development and design standards do not address specific issues or other relevant considerations, the Community Development Director has the responsibility and authority to make an interpretation based upon the intent of the provisions of the Specific Plan.

In summary:

1. **Permitted Uses:** Where the Specific Plan is silent, the use is prohibited (if it’s not listed in Table 4.1, it is not permitted); provided, however, that upon the request of an applicant, the Community Development Director shall have the authority to administratively determine that a retail use which has not been expressly considered by the WMSP is permitted on the basis that the retail use is reasonably consistent with the types of uses and activities expected in a first-class mixed-use (retail, restaurant, and residential) development in the Los Angeles–Orange County Metropolitan Area, and will not pose greater health, safety, and/or environmental impacts as compared to the expressly permitted uses. If approved, such use shall be administratively added to, and conclusively deemed for all purposes (including without limitation Section 9.1.2 of the WMSP) to be a part of and listed in, Table 4.1.



2. **Development and Design Standards:** Where the Specific Plan is silent on a development standard or an ambiguity arises, upon the request of an applicant, the Community Development Director will make an interpretation based on the intent of the provisions of the Specific Plan as set forth in Section 1.3 of the Specific Plan.

If any provisions in the WMSP and WMC are in conflict, the provisions of the Specific Plan shall prevail.

The Community Development Director is authorized to provide administrative determinations and interpretations regarding uses and development standards in the Specific Plan. Such administrative determinations must be in writing and may be appealed in accordance with the provisions of the WMC.

3. Modification of design standards due to site-specific, utility conflicts, or other unforeseen (or more severe than anticipated) factors shall require approval from the Community Development Director in accordance with the Minor Administrative Adjustment findings and procedures set forth in Section 9.2.4 (A).

## 5.2 DEVELOPMENT AND DESIGN STANDARDS

The development and design standards provide the policies by which all development within the WMSP must follow. The development and design standards and design guidelines in Chapter 5 are intended to be used in conjunction with one another as they have been crafted to promote the best possible design for the development of the WMSP.

### 5.2.1 BUILDING SETBACKS

1. Buildings shall adhere to minimum and maximum setbacks as provided in Table 5.1, *Building Setback Requirements*.
2. Additional setbacks beyond the required maximum “build to” setbacks identified in Table 5.1, *Building Setback Requirements*, may be allowed for entry plazas, courtyards or outdoor dining patios subject to approval by the Community Development Director.
3. Developer shall be responsible for project impacts on adjacent rights-of-way and constructing street segments to match cross-sections as provided in Chapter 6, *Mobility*, of the WMSP.

**Table 5.1: Building Setback Requirements**

Setback from	Min.	Max.
Freeway, Freeway Off-ramp	60 ft	none
Bolsa Avenue	132 ft	142 ft
Edwards Street	72 ft average with a 50 ft min.	82 ft
Goldenwest Street	60 ft; to clear drainage easement	
Primary Internal Circulation Street	50 ft	60 ft
Internal Main Street	46 ft	56 ft
Internal Residential Street	40 ft	50 ft
Internal Paseo	0 ft	10 ft
Building to Building	Setbacks shall comply with Building Code and emergency access requirements	

*Notes: Building setbacks shall be measured from roadway centerline except on the freeway edge and Internal Paseo where it is measured from the property line.*

## 5.2.2 PUBLIC REALM DEVELOPMENT AND DESIGN STANDARDS

Public realm development standards control the frontage conditions along streets within the Specific Plan area. For the purposes of these development standards, the public realm includes the public and private right-of-way, as well as the area between the right-of-way and the adjacent building façade (see Figure 5.1, *Public Realm Zones*). The purpose of combining standards for both the private and public right-of-way and the adjacent private frontage is to clearly illustrate the design for this critical area; this should be applied to Primary Internal Circulation, Internal Main streets, and Internal Residential streets. The public realm must provide sufficient space for window shopping, pedestrian through-traffic, outdoor dining, landscape planters, and streetlights.

The public realm is critical to the pedestrian experience and therefore the development standards section has been organized to address three specific areas that require detailed regulatory guidance. As shown in Figure 5.1, *Public Realm Zones*, these areas, or zones, include a vehicular zone, a pedestrian zone, and a frontage zone. The design of each zone will help contribute to the look and feel that pedestrians and motorists experience along the street. A general description of each zone is provided below.

**Vehicular Zone:** The vehicular zone consists of vehicular travel lanes and may include bike lanes and parking lanes. The vehicular zone is located in the public or private right-of-way. Chapter 6, *Mobility*, provides a more detailed overview of the lane configurations related to the vehicular zone.

**Pedestrian Zone:** The pedestrian zone consists of the pedestrian walkway and an additional landscape parkway adjacent to the curb for trees, planters, lighting, hardscape and other pedestrian amenities. A minimum 5-foot clear and unobstructed path shall be provided in this zone to accommodate pedestrian movement.

Pedestrian zone improvements are required along all parcel frontages where existing conditions do not meet minimum requirements. The pedestrian zone is primarily located in the public right-of-way, however along some streets, a portion or all of the pedestrian zone may be located on private property. In these cases, the required pedestrian zone will necessitate the provision of additional sidewalk width within the adjacent private property.

As development occurs, the applicants are required to provide vehicular and pedestrian zone improvements.

**Frontage Zone:** The frontage zone is the area between the building façade and the sidewalk. This area may include outdoor dining space, plaza space, or landscaping. Frontage zone standards regulate setback areas, encroachments, and the percent of the building façade required to be located along the setback line. The frontage zone is, in all cases, located on private property. Pedestrian zones may require alterations to allow for pedestrian drop-off opportunities. Uses such as medical offices, businesses and valets, may require patient or visitor drop-off and pickup. Where possible, these should be located within the proposed project.

All visitor drop-off and pickup shall be provided on-site unless the applicant can demonstrate there is a hardship that cannot be redesigned to accommodate loading. The applicant must demonstrate there are no other reasonable alternatives on-site before the Community Development Director will consider off-site visitor drop-off and pickup areas. The Community Development Director shall make the final decision regarding the placement of visitor drop-off and pickup, based upon building use and orientation, shape of the site, the ability to maintain fire lanes, the ability to provide appropriate queuing areas, and maintaining turn around or internal traffic flow capabilities. Visitor drop-off and pickup areas may use no more than 100 linear feet or 50% of frontage along a street edge – whichever is smaller. It must be located within the vehicular zone, where there would have been parking, and share an edge with the proposed project, and will be used only for temporary drop-off and pickup of passengers.



*The frontage zone along commercial or mixed-use frontages may be an extension of the sidewalk or used for outdoor dining space.*



*Zero-foot setback*



*6-10 foot setback with outdoor seating*



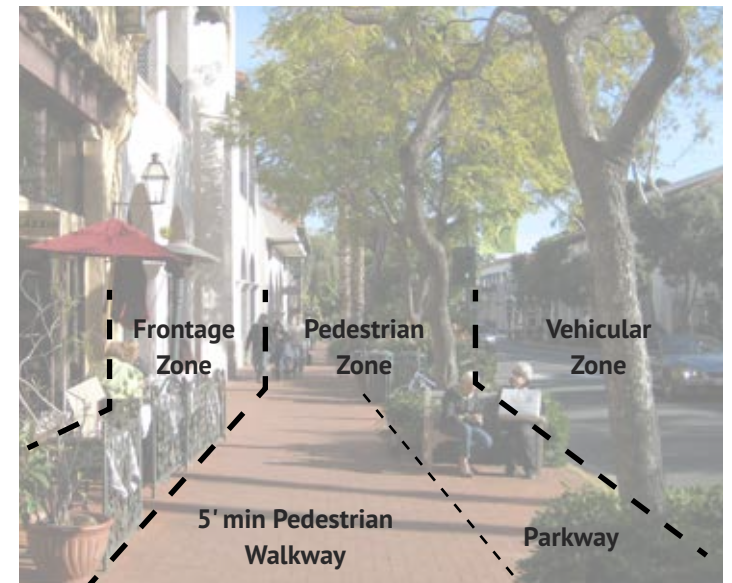
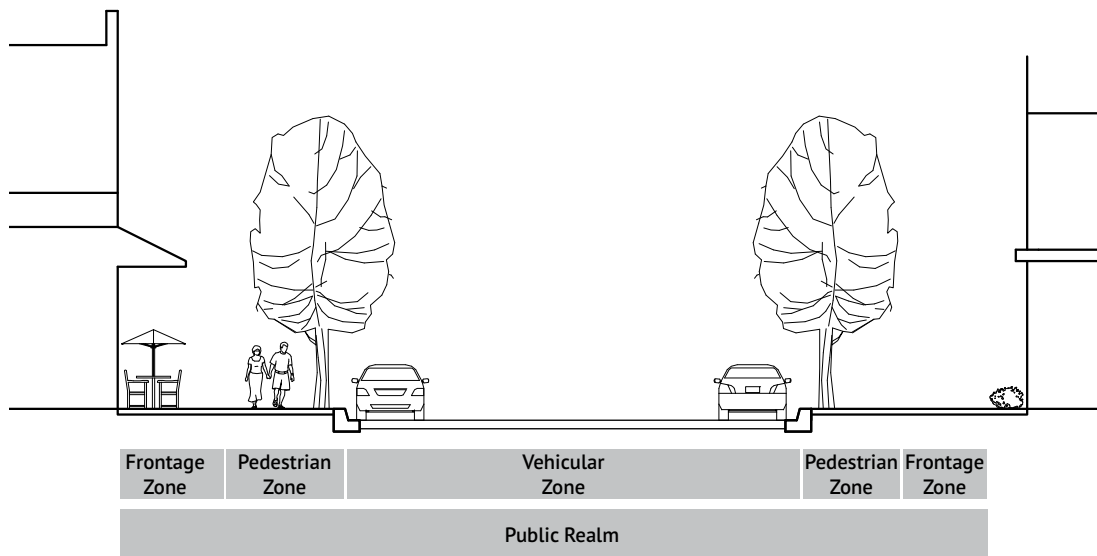
*10-30 foot setback with landscaping and elevated private entrance*



*60-80 foot setback with public plaza and display areas*



**Figure 5.1: Public Realm Zones**



*Depiction of Public Realm that meets the criteria in Section 5.3.2.*

## 5.2.3 BOLSA AVENUE

### VEHICULAR ZONE

Bolsa Avenue is an existing 120-foot wide street. It includes six travel lanes, one right-turn lane, a 16-18 foot median, and sidewalks on both sides. Bolsa Avenue has two possible street section configurations as shown in Chapter 6, *Mobility*.

### PEDESTRIAN ZONE

The pedestrian zone begins 48 feet setback from the centerline of Bolsa Avenue. The pedestrian zone shall consist of a minimum 7-foot wide parkway and an additional minimum 5-foot wide unobstructed pedestrian walkway. Landscape and street lighting shall be located within the parkway.

### FRONTAGE ZONE

The building façade shall be setback a minimum 132 feet from the centerline of Bolsa Avenue. This setback is intended to accommodate the 72 feet needed for the drainage channel easement. This setback is hereafter referred to as the “front setback”.

Awnings, entrance porticos, porches, stoops, stairs, balconies, bay windows, eaves, entrance overhangs, and arcades are permitted within the front setback. Additional requirements particular to specific frontage types can also be found on Section 5.2, *Frontage Types*.

Front setback areas along commercial or mixed-use and residential frontages shall consist of

1. An extension of the sidewalk or used for outdoor dining space
2. A frontage street with parallel parking on one side
3. Landscaped area to serve as a linear park or ramblas
4. Setback areas along frontages without doors or windows shall be landscaped.

Front setback areas along residential frontages shall also provide pathways connecting the sidewalk to the front door and to any parking areas, and shall otherwise be landscaped across the entire property frontage, with the exception of a pedestrian walkway.

Along Bolsa Avenue, due to the non-buildable area created by the drainage easement, a sidewalk may be designed such that it meanders into the property and does not have to be located directly adjacent to the parkway illustrated on the required parkway exhibit. The Community Development Director may approve any variations to all required parkway design, if such changes are consistent with the intent and goals of this Plan.



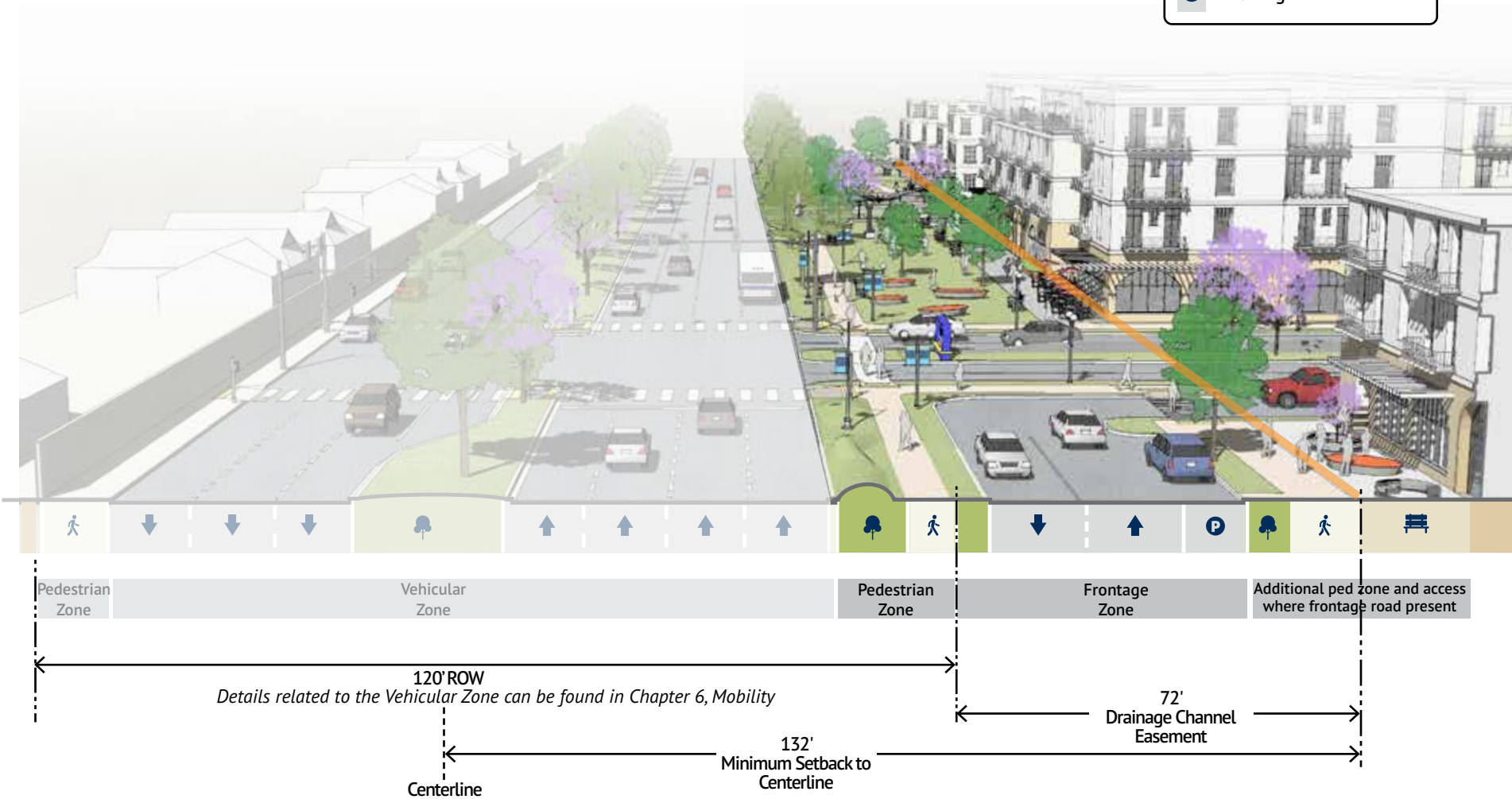
*A frontage street with parallel parking may be included in the frontage zone for Bolsa Avenue over the flood control easement.*



*Awnings, entrance porticos, porches, stoops, stairs, balconies, bay windows, eaves, entrance overhangs, and arcades are permitted to encroach into the required front setback, Section 5.3.1.*

**Figure 5.2: Bolsa Avenue Frontage**

	Pedestrian Walkway 5' min
	Vehicular Travel Lane
	Landscape Zone
	Building Frontage
	Building Area
	Parking



*Note: Section is illustrative only; to be interpreted as conceptual, as one possible design to illustrate a variety of desirable design ideas that could occur, but is not considered the final design. Since no buildings can be located on top of the drainage easement located along Bolsa Ave., expanded green spaces or internal slip streets could be located in this area.*

## 5.2.4 EDWARDS STREET

### VEHICULAR ZONE

Edwards Street is an existing 80-foot right-of-way. It includes four travel lanes, one left-turn lane, a Class II bike lane on both sides, and sidewalks. Edwards Street has two possible street section configurations as shown in Chapter 6, *Mobility*, for the future right-of-way.

### PEDESTRIAN ZONE

The Pedestrian Zone begins 28 feet setback from the centerline of Edwards Street. The Pedestrian Zone shall consist of a minimum 7-foot wide parkway and an additional minimum 5-foot wide unobstructed pedestrian walkway. Landscape and street lighting shall be located within the parkway.

### FRONTAGE ZONE

The building façade shall have an average minimum setback of 72 feet with a minimum of 50 feet, measured from the centerline of Edwards Street. These requirements are hereafter referred to as the “front setback.”

Awnings, entrance porticos, porches, stoops, stairs, balconies, bay windows, eaves, entrance overhangs, and arcades are permitted within the required front setback. Additional requirements particular to specific frontage types can also be found on Section 5.2.10, *Frontage Types*.

The frontage zone along commercial or mixed-use frontages may be an extension of the sidewalk or used for outdoor dining space. The frontage zone along buildings without doors or windows shall be enhanced with potted plants, bench seating, or other pedestrian amenities.

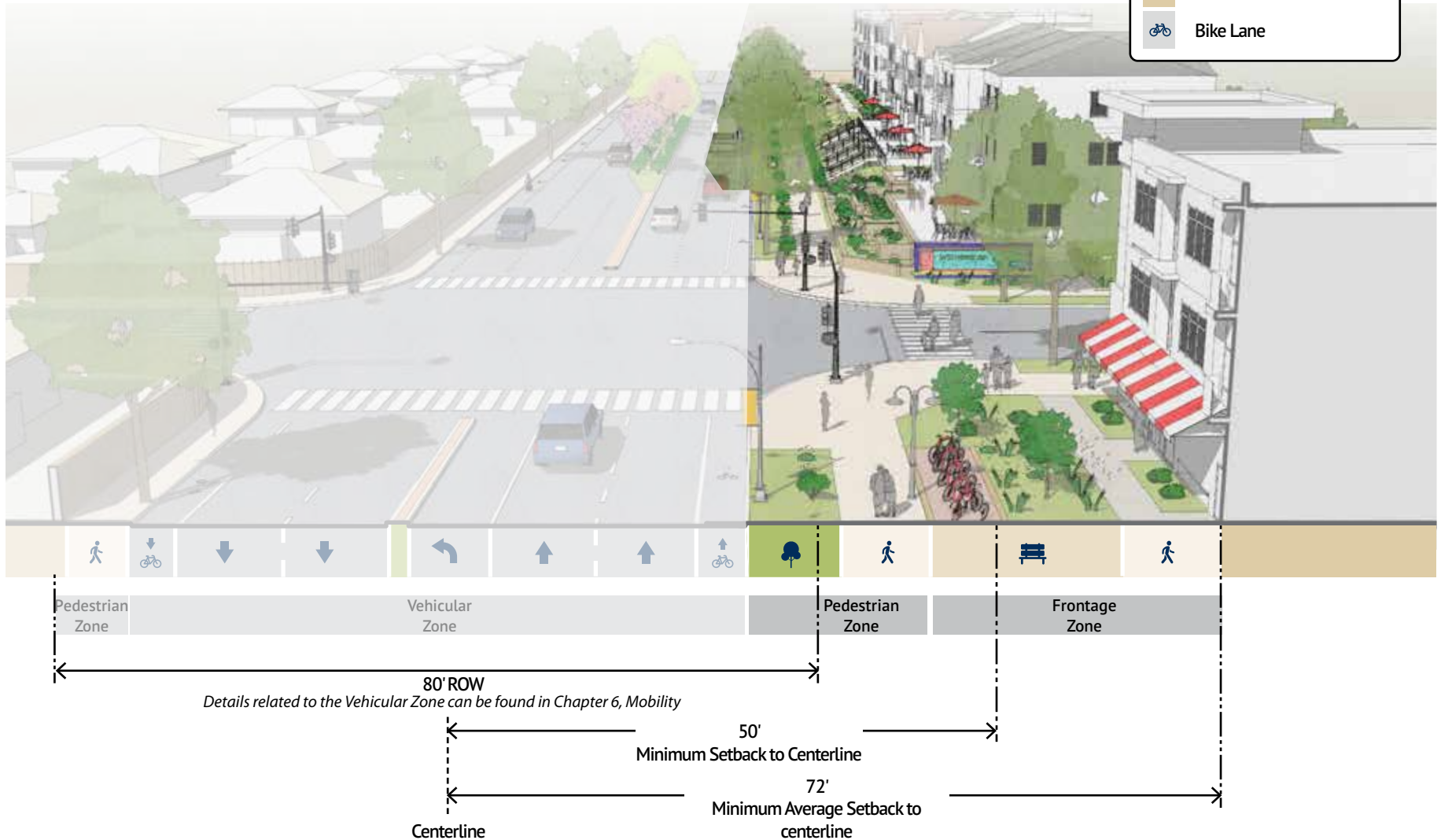
Front setback areas along residential frontages shall provide pathways connecting the sidewalk to the front door and to any parking areas, and shall otherwise be landscaped across the entire property frontage, with the exception of a pedestrian walkway, as described above.



*Entrances to residential units should be elevated above the street level to provide an added sense of separation between public and private space.*

**Figure 5.3: Edwards Street Frontage**

-  Pedestrian Walkway 5' min
-  Vehicular Travel Lane
-  Landscape Zone
-  Building Frontage
-  Building Area
-  Bike Lane



*Note: Illustrative only; to be interpreted as conceptual, as one possible design, and not considered definitive.*

## 5.2.5 PRIMARY INTERNAL CIRCULATION STREET

### VEHICULAR ZONE

The primary internal circulation street is a proposed 100-foot wide street. It has two street sections. Both sections include 4 travel lanes, and class II bike lanes. One has a raised median and the other has on-street parking as shown in Chapter 6, *Mobility*.

### PEDESTRIAN ZONE

The Pedestrian Zone shall begin 33 feet set back from the centerline of the street. The Pedestrian Zone shall consist of a minimum 7-foot wide parkway and an additional minimum 10-foot wide, unobstructed, pedestrian walkway. Landscape and street lighting shall be located within the parkway.

### FRONTAGE ZONE

The building façade shall be setback a minimum 50 feet from the centerline of the street. This setback is hereafter referred to as the “front setback”.

Awnings, entrance porticos, porches, stoops, stairs, balconies, bay windows, eaves, entrance overhangs, and arcades are permitted within the front setback. Awnings, entrance porticos, porches, stoops, stairs, balconies, bay windows, eaves, entrance overhangs, and arcades are permitted within the required front setback up to a maximum of 6 feet. Additional requirements particular to specific frontage types can also be found on Section 5.2.10 *Frontage Types*.

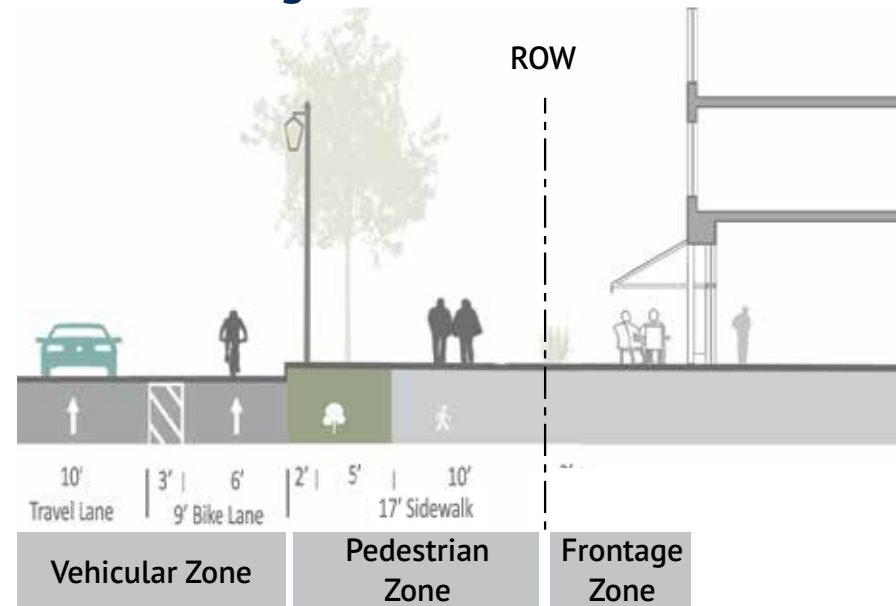
Front setback areas along commercial or mixed-use frontages shall be an extension of the sidewalk or used for outdoor dining space. Setback areas along frontages without doors or windows shall be landscaped.

Front setback areas along residential frontages shall provide pathways connecting the sidewalk to the front door and to any parking areas, and shall otherwise be landscaped across the entire property frontage.



Larger sidewalk adjacent to outdoor seating in the frontage zone.

**Figure 5.4: Primary Internal Circulation Street Frontage**



## 5.2.6 INTERNAL MAIN STREET

### VEHICULAR ZONE

The Internal Main Street is a two-lane street. It limits traffic speeds and creates a narrower cross section with frequent, well-designed pedestrian crossings. Internal Main Street has three possible street section configurations as shown in Chapter 6, *Mobility*.

### PEDESTRIAN ZONE

The Pedestrian Zone begins 29 feet setback from the centerline of the street. The Pedestrian Zone shall consist of a minimum 7-foot wide parkway and an additional minimum 10-foot wide unobstructed pedestrian walkway. Landscape and street lighting shall be located within the parkway.

### FRONTAGE ZONE

The building façade shall be setback a minimum 46 feet from the centerline of the street. This setback is hereafter referred to as the “front setback”. The portions of the building façade that are on the fourth story or higher shall have an additional 10 foot setback from the front setback.

To promote an active and interesting pedestrian environment, retail uses oriented towards the street frontage are strongly encouraged at the ground floor along Internal Main Streets. Refer to Table 4.1, *Permitted Land Uses*, for a list of preferred retail uses.

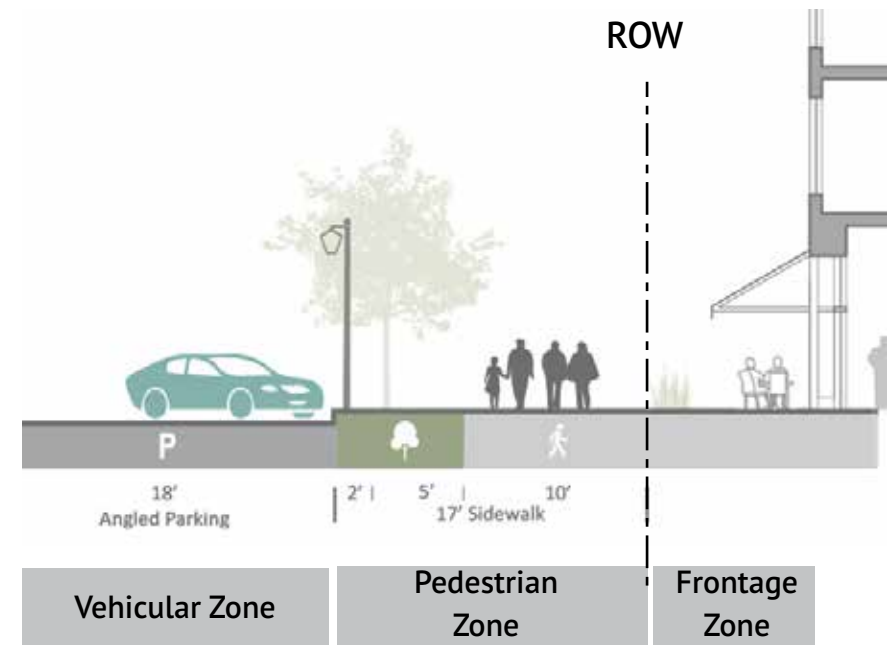
Awnings, entrance porticos, porches, stoops, stairs, balconies, bay windows, eaves, entrance overhangs, and arcades are permitted within the required front setback up to a maximum of 6 feet. Additional requirements particular to specific frontage types can also be found on Section 5.2.10, *Frontage Types*.

Uses along the Internal Main Streets shall provide pathways connecting the sidewalk to the business or residence and to any parking areas, and shall otherwise be landscaped across the entire property frontage with the exception of a pedestrian walkway, as described above.



*Ground level retail and restaurants create an active and interesting street scene for visitors.*

**Figure 5.5: Internal Main Street Frontage**



## 5.2.7 RESIDENTIAL STREET

### VEHICULAR ZONE

Residential streets are designed to accommodate pedestrians, cars, bicycles, and parking in the residential area. The configuration will vary and be dependent on the housing types. Three options for residential street configurations are shown in Chapter 6, *Mobility*.

### PEDESTRIAN ZONE

The pedestrian zone begins 26 feet setback from the centerline of the street. The pedestrian zone shall consist of a minimum 7-foot wide parkway and an additional minimum 6-foot wide unobstructed pedestrian walkway. Landscape and street lighting shall be located within the parkway.

### FRONTAGE ZONE

The building façade shall be setback a minimum 40 feet from the centerline of the street. This setback is hereafter referred to as the “front setback.”

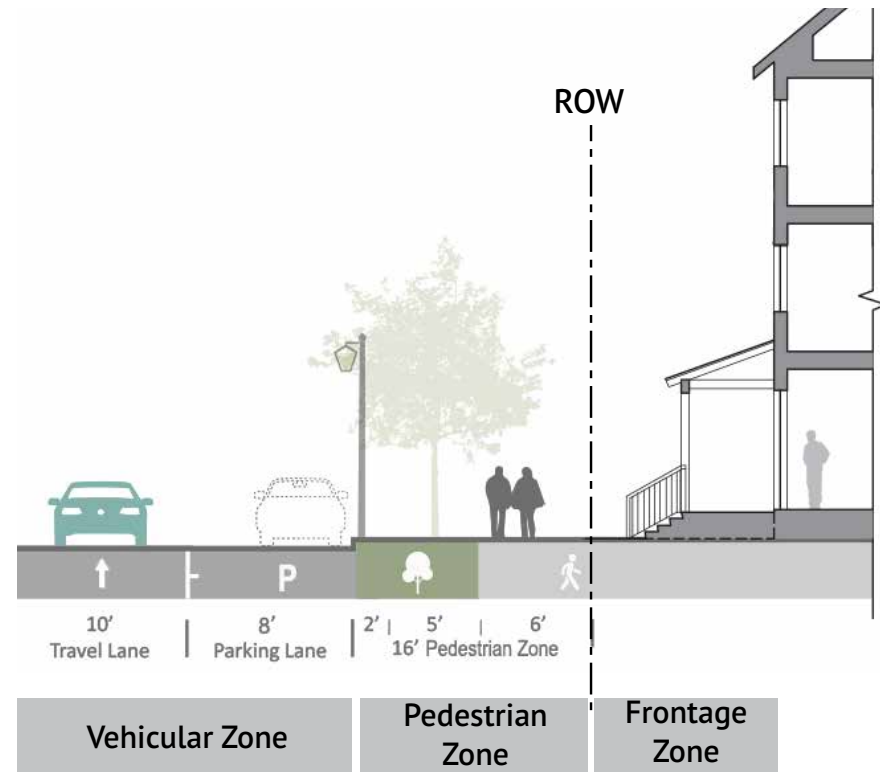
Residential entrance porticos, porches, stoops, stairs, balconies, bay windows, eaves, and entrance overhangs are permitted within the front setback up to a maximum of 10 feet. Additional requirements particular to specific frontage types can also be found in Section 5.2.10, *Frontage Types*.

Storefront awnings may encroach 6 feet into the pedestrian zone and arcades may encroach 8 feet into the pedestrian zone so long as they do not obstruct the minimum clear path of travel.

Front setback areas along commercial or mixed-use frontages shall be an extension of the sidewalk or used for outdoor dining space. Setback areas along frontages without doors or windows shall be landscaped.

Front setback areas along residential frontages shall provide pathways connecting the sidewalk to the front door and to any parking areas, and shall otherwise be landscaped across the entire property frontage with the exception of a pedestrian walkway, as described above.

**Figure 5.6: Residential Street Public Realm**



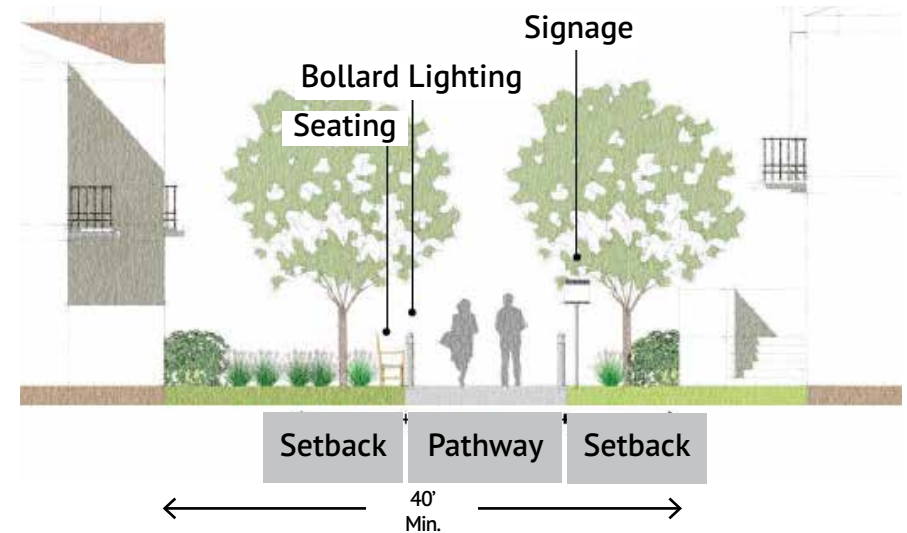


## 5.2.8 PASEOS

Paseos are planned to provide pedestrian and bicycle facilities as part of planned development. Paseos are typically within, adjacent, or through the planned development. Although paseos are not associated with a roadway, they may be adjacent or parallel. Paseos serve as connector trails by improving access and facilitating connections between and through the development. Paseos should have an active frontage, provide an opportunity for amenities, and allow for pedestrian and bicycle travel and highly visible from within the adjacent buildings or areas. All property owners are required to adjust site planning to provide for an interconnected and continuous paseo from the Urban Park Plaza to the northern boundary of the site to connect to the future Westminster Nature Activity Trail.

Paseos, as shown in Figure 5.7, shall be a minimum of 40 feet wide, be free of any encroachment from buildings or private patios or other private spaces, and have a minimum 8 feet of obstruction-free, hard surface or a soft surface that is suitable for use by bicycles and pedestrians. Lighting, wayfinding signage, and landscaping shall be provided as part of paseos while considering the location near adjacent development. There is also an opportunity to locate recreational amenities as part of the paseo to activate the space and provide additional opportunities for recreation. Paseos that incorporate these active elements may qualify as a linear park and will still need to meet the requirements set forth in Table 5.5, *Open Space Requirements*.

Figure 5.7: Paseo Section



Paseo with additional shade structures provide an inviting experience for pedestrian use away from vehicular traffic.



Landscaped residential paseos provide pedestrian access and units that front onto paseos activates them for residential use.

## 5.2.9 OBJECTIVE BUILDING DESIGN

Building design standards are intended to ensure a high level of architectural quality and attention to placemaking throughout the WMSP.

### BUILDING DESIGN

1. A building shall carry the same architectural theme for all elevations.
2. For the purposes of this architectural theme, a theme includes primary (non-accent) materials and colors.
3. Blank walls (façades without doors, windows, landscaping treatments) shall be less than 15 feet in length along sidewalks, pedestrian walks, or publicly accessible outdoor space areas.
4. Buildings over three stories must provide a ground floor elevation that is distinctive from the upper stories by providing a material change between the first floor and upper floors along at least 75% of the building façade with frontage upon a street, adjacent public park or public open space.
5. Trim surrounds shall be provided at all exterior window and door openings. In lieu of exterior window trim, windows can be recessed from wall plane by a minimum of three inches.
6. At least two materials shall be used on any building frontage, in addition to glazing and railings. Any one material must comprise at least 20% of the building frontage.
7. All buildings and retail spaces must include one or more frontage types as selected from those identified in the Section 5.2.10, *Frontage Types*.
8. When dwelling units are abutting open space areas, a minimum of one window from each dwelling shall be located to overlook common area. A clear glass window of not less than 8 square feet shall be used to allow visibility into the common area.

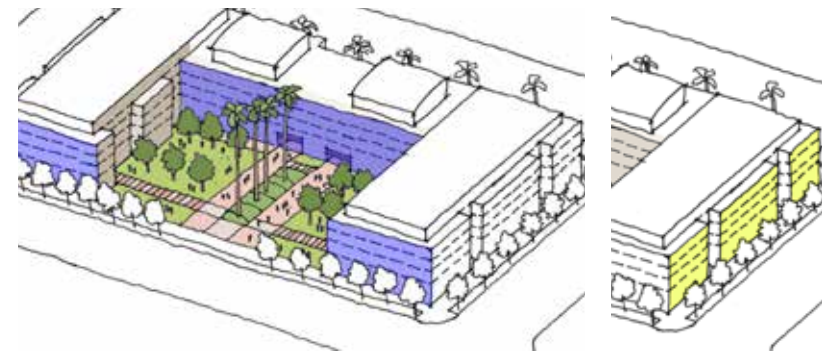


*Façade articulation including notched setbacks, projecting bays, and balconies helps to break up building mass and add visual interest.*

9. Building entries shall face the primary public street with pedestrian access provided from sidewalks to all building entries, parking areas, and publicly accessible open spaces. For larger sites with multiple buildings, building entries may also be oriented to face internal open spaces, paseos, and recreation amenities.
10. For development fronting onto public and private streets (as identified in Chapter 6.4, *Street Classifications and Street Sections*), a minimum of 70% of the street frontage shall be devoted to buildings located between the minimum and maximum front setback lines; where 70% of building is to reside within the Frontage Zone.
11. The Building footprint may be pulled back in some areas to articulate the architecture to create a courtyard and/or green space. If this condition is included in the design, a minimum of 50% of the street frontage shall be devoted to buildings located between the minimum and maximum front setback lines.
12. Building walls that do not front onto public or private streets (as defined in sections 4.3.3 thru 4.3.7) shall be consistent in design with the primary building front. Non-primary building walls are not required to utilize frontage types provided in this Specific Plan.
13. A maximum of 30% of the street frontage shall be used for entry forecourts, paseos, outdoor plazas or parking access.
14. Commercial or office building façades along street frontage shall be built with a minimum of 60% of the ground floor consisting of windows or storefronts with views into the building.
15. Building façades shall include architectural elements to differentiate the vertical plane visually. The differentiation of the vertical plan shall be achieved through a minimum of one of the following, articulation of horizontal bands for separate floor levels, architectural reveals, trims, awnings, and eaves.

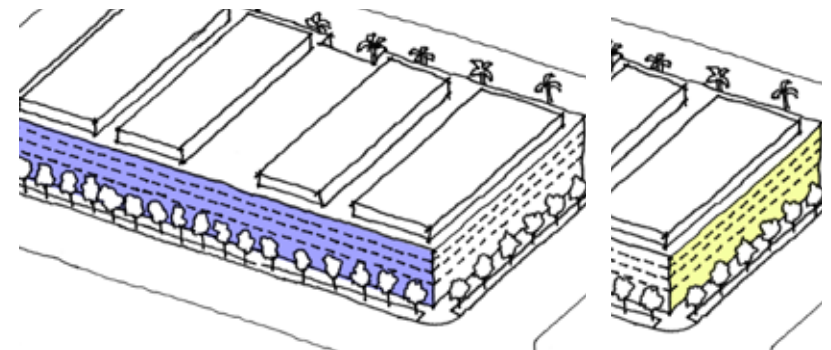
## MASSING/ARTICULATION

16. A minimum of two elements such as balconies, cantilevers, dormers, bay windows, patios, individualized entries, and accent materials shall be incorporated into each residential project building.
17. Buildings shall have wall faces vary in depth with a minimum one-foot offset for any wall plane that exceeds 30 feet in length to avoid monolithic or repetitive building wall façades and minimize the appearance of large buildings.



### Do This:

*Building articulation creates spaces and activates the site. This can also frame views and create areas of respite.*



### Don't Do This:

*The above example shows no activation of spaces or areas of respite when building articulation is not implemented.*

18. A minimum one-foot offset is required for any wall plane that exceeds 30 feet in length.
19. Rooflines shall be vertically articulated at least every 50 feet along the street frontage, through the use of architectural elements such as parapets, varying cornices, reveals, clerestory windows, and varying roof height and/or form.
20. Architectural articulation shall be used to indicate entrances and places of interest, frame courtyards and open space, provide visibility between spaces, and allow light and air into interior spaces.
21. All corners of intersections, entries, or near the center of grouped buildings should feature special architectural elements. Treatments may include: towers that are visually distinct from the rest of the building massing; a corner plaza or gathering space a lobby canopy; a public art installation; a landscape installation with columnar trees; clock towers; other prominent architectural elements.

## ACCESSORY ELEMENTS

22. Roof top equipment shall be concealed from visibility. The point of view for determining visibility shall be 5 feet above grade at a distance of 200 feet. If the roof structure does not provide this screening, include an equipment screen in the design.
23. Any equipment added to the roof in the future, such as antennas, must also be concealed, either behind a parapet or within an architectural feature consistent with the building design.
24. Roof drains and down spouts shall be designed as an integral part of the architectural theme.
25. No portion of an awning shall be lower than 8 feet of clearance.
26. Controlled entrances to parking facilities (gates, doors, etc.) shall be located a minimum of 18 feet from the back of sidewalk to accommodate one vehicle entering the facility.

27. Screen 70% of all parking areas, covered and uncovered, from public street frontages. Screening may be accomplished through building placement, landscaping, a planted earth berm, planted fencing, topography, or some combination of the above.
28. Refuse Containers: All dumpsters shall be shielded within an enclosure a minimum of six feet tall. Enclosures shall be comprised of similar materials used in the architecture of the parcel in which it resides. Allow adequate size to accommodate the needed dumpsters and recycling containers. All enclosures and gates should be detailed to withstand heavy use. Provide wheel stops or curbs to prevent dumpsters from banging into walls of enclosure.

## 5.2.10 FRONTAGE TYPES

The arrangement and design of building frontages largely determines the character of the area and the quality of the pedestrian environment. At the ground floor, they are the means of access into buildings and consequently visually reinforce their human scale. The intent of regulating ground floor building frontages is to ensure that the transition between the proposed development and the public realm is consistent with the Specific Plan's goals for building form, character and quality. This is specific to the ground floor only, and not the floors above. It is an important element of urban design because it is the way that a building engages the public realm.

These frontage standards shall be used along with other development and design standards herein.

Standards for all Frontages:

1. Canopies, awnings, signs, balconies and other architectural projections must clear 8 feet above the adjacent sidewalk.

## Figure 5.8: Stoop Frontage

### Stoop Frontage Description

A stoop is a frontage wherein the building façade is separated from the pedestrian zone, paseo, or courtyard, by an entrance to the elevated ground floor of the building. The entrance is usually an exterior stair and landing and may be covered.

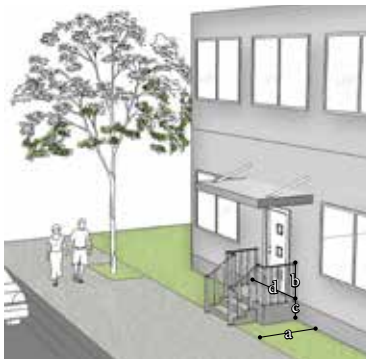
This type is recommended for ground floor residential use as it facilitates a transition onto more residential frontage.

### Stoop Frontage Design Standards<sup>1</sup> CONFIGURATION

- A great variety of stoop designs are possible, but the following shall apply:
- A minimum of 3 feet deep clear.
  - Fences or walls defining the stoop shall not exceed 3 feet from the highest adjacent top of landing of the stoop.
  - Stoops shall be raised to transition into the building. The ground story entry shall not be elevated more than 6 feet above the adjacent sidewalk.
  - Stoops must correspond directly with the building entry(s) and be at least 3 feet wide (perpendicular to or parallel with the adjacent walk).

### ELEMENTS

- Building façade may be setback the depth of the entry stair from the sidewalk.
- The stoop may include a covered roof, awning, or door inset within the building front.
- Remaining open areas within the frontage zone shall be landscaped per Section 5.3.19 *Landscape and Lighting*.



## Figure 5.9: Terrace Frontage

### Terrace Frontage Description

A terrace is a frontage wherein the building façade is set back from the pedestrian zone, paseo, or courtyard, by a paved or planted open area. This frontage type can effectively buffer residences from the sidewalk.

This type is recommended for residential use as it allows for private front gardens.

### Terrace Frontage Design Standards<sup>1</sup> CONFIGURATION

A great variety of terrace designs are possible, but the following shall apply:

- A minimum of 5 feet to maximum of 8 feet deep clear. Terrace design shall take into consideration landscape areas, where adjacent to public landscape easement, to the extent possible.
- Terraces may be at grade or raised to transition into the building. In no case shall terraces be raised more than 6 feet from the adjacent grade of the pedestrian zone. Where terraces are raised, a retaining wall may be built around the terrace.
- A minimum of 6 feet wide clear for entry landing.
- Fences defining the terrace shall not exceed 3 feet in height from the highest adjacent grade of the terrace.

### ELEMENTS

- Planted terraces and remaining open areas within the frontage zone shall be landscaped per Section 4.3.19 *Landscape and Lighting*.

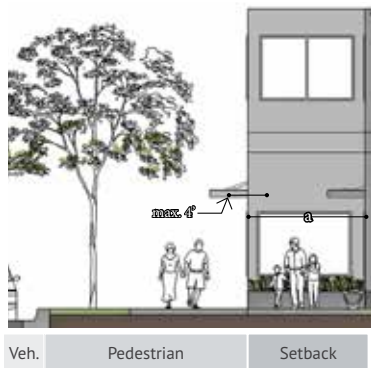


## Figure 5.10: Forecourt Frontage

### Forecourt Frontage Description

A forecourt is a frontage wherein a portion of the building façade is recessed from the primary building façade. The forecourt may be used as an entry court and open space for residential uses, or as additional shopping or seating areas for commercial uses. Forecourts with large trees and lush landscaping offer visual and environmental variety to the urban streetscape.

This frontage type is appropriate for either residential and/or commercial uses. A combination of both uses can be achieved by using the forecourt as a residential entrance while commercial uses occupy street adjacent building space. This type can be used in conjunction with shopfronts and stoops as a transition into residential frontage.



### Forecourt Frontage Design Standards

#### CONFIGURATION

A great variety of forecourt designs are possible, but the following shall apply:

- A minimum of 10 feet and maximum of 40 feet deep clear.
- A minimum of 20 feet and maximum of 50 feet wide or 50% of the lot width, whichever is less.
- At least one building entry must front onto the forecourt.
- The forecourt may also be raised from the sidewalk, creating a small retaining wall at the property line with entry steps to the forecourt, but shall not exceed 3 feet in height from the adjacent sidewalk grade.

#### ELEMENTS

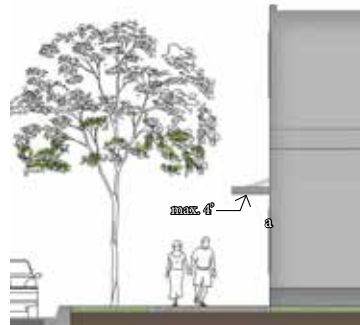
- The proportions and solar orientation of the forecourt should be carefully considered for user comfort.
- A fence or wall at the property line may be used to define the private space of the court.
- Entrances and pedestrian "gateways" may be announced by posts or pilasters, and may be combined with trellises, special landscaping, decorative lighting, public art or other special features.
- Canopies of large trees placed within the forecourt may overhang into the pedestrian zone.
- Open areas within the frontage zone shall be landscaped per Section 5.3.19, *Landscape and Lighting*.

## Figure 5.11: Shopfront Frontage

### Shopfront Frontage Description

A shopfront is a frontage wherein the building façade and entrance are at sidewalk grade and close to the pedestrian zone. Shopfronts include large areas of transparent openings and doors and are commonly equipped with cantilevered roof(s) or awning(s). Shopfronts typically provide access directly from sidewalks and are oriented to display ground-level commercial uses.

This frontage type is conventional for commercial use. This frontage type can be used in conjunction with terrace, and or forecourt to create a more engaging street.



### Shopfront Frontage Design Standards

#### CONFIGURATION

A great variety of shopfront designs are possible, but the following shall apply:

- Shopfront façade area shall be at least 15 feet tall, as measured from the adjacent walk, and minimum 10 feet wide.
- Shopfronts may be recessed from the primary building façade by up to 5 feet.
- The shopfront shall provide clear views of merchandise displays.

#### ELEMENTS

- A base of similar or visually "heavier" materials than the walls is recommended below display windows.
- Doors should be substantial, well-detailed, and match the materials, design, and character of the display windows.
- Canopies and awnings shall cover the entire length of shopfront openings.
- Remaining open areas within the frontage zone shall be landscaped per Section 5.3.19, *Landscape and Lighting*.

**Figure 5.12: Gallery Frontage**

**Gallery Frontage Description**

A gallery is a frontage wherein the building façade has an attached, cantilevered shed or a lightweight colonnade overlapping the sidewalk. The colonnade may support a roof or balcony above. Balconies may be used for outdoor dining<sup>1</sup>. The gallery covering the sidewalk provides pedestrian shelter and opportunities for covered outdoor dining.

This frontage type is conventional for commercial use.

**Gallery Frontage Design Standards**

**CONFIGURATION**

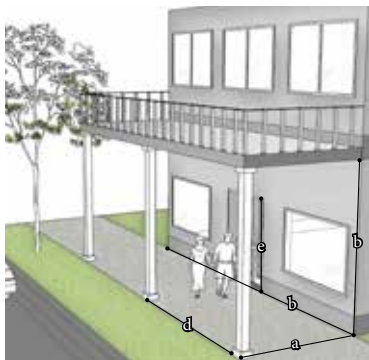
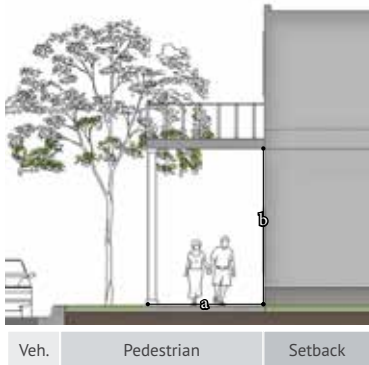
A great variety of gallery designs are possible, but the following shall apply:

- a. A minimum of 10 feet and maximum of 15 feet deep clear. Galleries may overlap the whole width of the sidewalk within the pedestrian zone but may not encroach into the landscape area.
- b. Galleries shall be no less than 12 feet wide and 10 feet tall.
- c. Galleries shall be in alignment with adjacent galleries and/or arcades to the extent possible.
- d. Spacing between openings and/or columns shall be a minimum of 10 feet.
- e. Shopfront openings within galleries shall be at least 10 feet tall and shall provide clear views of merchandise displays within the space.

**ELEMENTS**

- f. The height and the proportions of the gallery shall correspond to storefront openings and the building façade shall be consistent with the architectural style of the building.
- g. Soffits, columns/arches shall be treated consistent with the architecture of the building.
- h. Entrances shall be at sidewalk grade.
- i. The pedestrian zone sidewalk and gallery frontage portions within the frontage zone and shall be consistent in design and material.
- j. Remaining open areas within the frontage zone shall be landscaped per Section 5.3.19, *Landscape and Lighting*.

1. An Encroachment Agreement may be required if impact to public right-of-way or public easement area.



**Figure 5.13: Arcade Frontage**

**Arcade Frontage Description**

An arcade is a frontage wherein the building façade is aligned close to the pedestrian zone with a colonnade recessed into the building. The colonnade supports habitable commercial or residential space above. The colonnade covering the sidewalk provides pedestrian shelter and opportunities for covered outdoor dining.

This frontage type is conventional for commercial use.

**Arcade Frontage Design Standards**

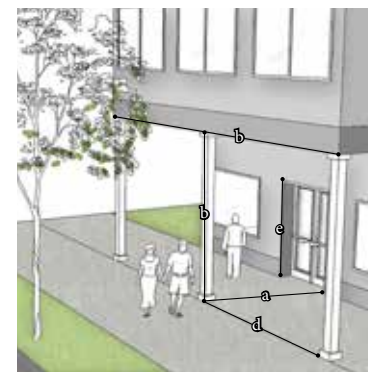
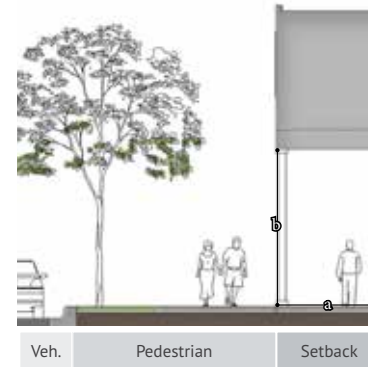
**CONFIGURATION**

A great variety of arcade designs are possible, but the following shall apply:

- a. A minimum of 10 feet and maximum of 15 feet deep clear.
- b. Arcades shall be no less than 12 feet wide and 10 feet tall.
- c. Arcades shall be in alignment with adjacent galleries and/or arcades to the extent possible.
- d. Spacing between openings and/or columns shall be a minimum of 10 feet.
- e. Shopfront openings within arcades shall be at least 10 feet tall and shall provide clear views of merchandise displays within the space.

**ELEMENTS**

- f. The height and the proportions of the arcade shall correspond to storefront openings and the façade consistent with the architectural style of the building.
- g. Soffits, columns/arches shall be treated consistent with the architecture of the building
- h. Entrances shall be at sidewalk grade.
- i. Remaining open areas within the frontage zone shall be landscaped per Section 5.3.19, *Landscape and Lighting*.

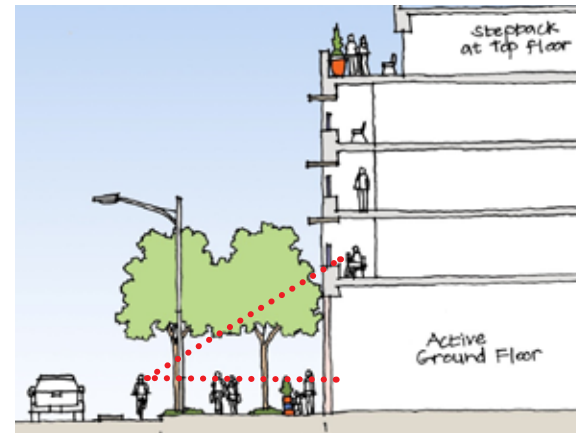


## 5.2.11 BUILDING AND FLOOR HEIGHT

1. The maximum allowed building heights are determined from base point established at the corner of Bolsa Avenue and Edwards Street.
2. Allowed maximum height zones:
  - Zone 1: maximum of 50 feet above the base point
  - Zone 2: maximum of 80 feet above the base point
  - Zone 3: maximum of 135 feet above the base point
3. Buildings shall adhere to minimum floor-to-ceiling height requirements as provided in Table 5.2.
4. Buildings shall adhere to maximum heights as provided in Figure 5.14, *Maximum Building Height*. Architectural features (not habitable) may project above the maximum building height and are exceptions that may be allowed to create visual interest.
5. Buildings greater than three stories shall provide variation by including features such as balconies and varied upper floor step backs with a minimum of step back of 10 feet.



Not to Scale



The design of building frontages, storefronts, window displays and street-level landscaping are critical components of a the pedestrian experience. Although the topography of the WMSP site has significant variation that will allow buildings up to 10 stories in height, the pedestrian experience will mainly shaped by building or street features that can be seen at eye level (generally the first 2-3 stories of a building, as depicted by the illustrations).

**Table 5.2: Building Floor Height Requirements**

Floor-to-Ceiling Height	Min.
Non-Residential Ground Floor	15 ft
Residential Ground Floor	12 ft
Upper Floor Non-Residential	10 ft
Upper Floor Residential	9 ft

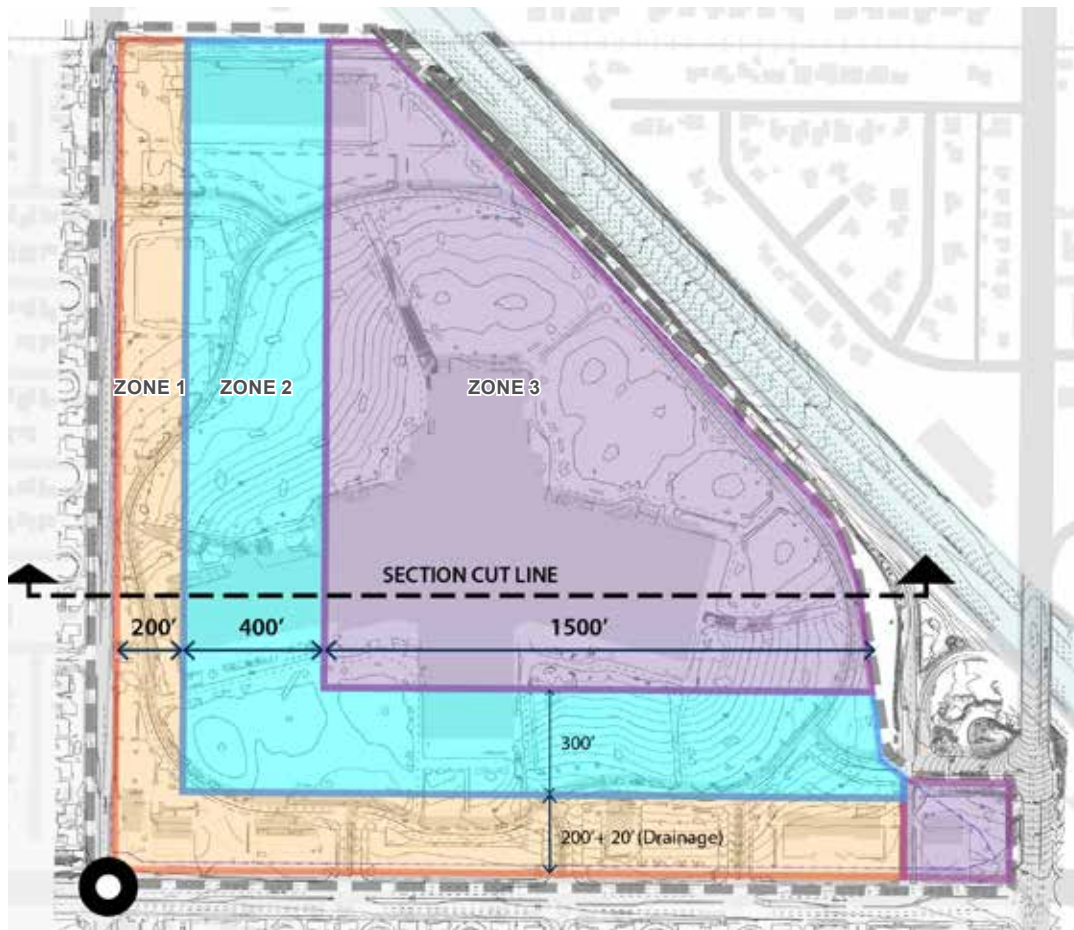
Note:  
Heights are measured from floor plate to floor plate.  
Intent is to provide the minimum allowed, heights for floors may be larger.



Varying heights to create visual interest and reduce visual impacts. WMSP locates taller buildings further into the site to achieve this.



**Figure 5.14: Maximum Building Height**

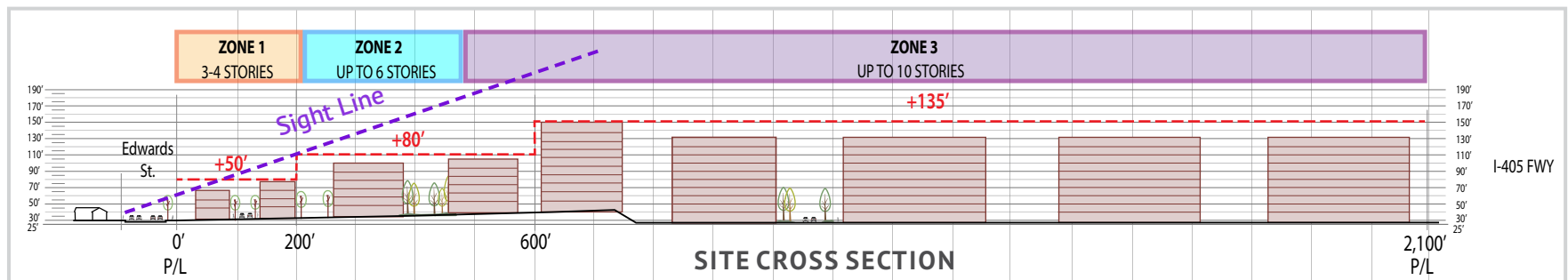


**MAXIMUM BUILDING HEIGHT**

- Base Point (Elev. 25 feet)
- Zone 1 (+50 feet)
- Zone 2 (+80 feet)
- Zone 3 (+135 feet)
- Section Cut Location
- Proposed Plan Building Skyline

**Notes:**

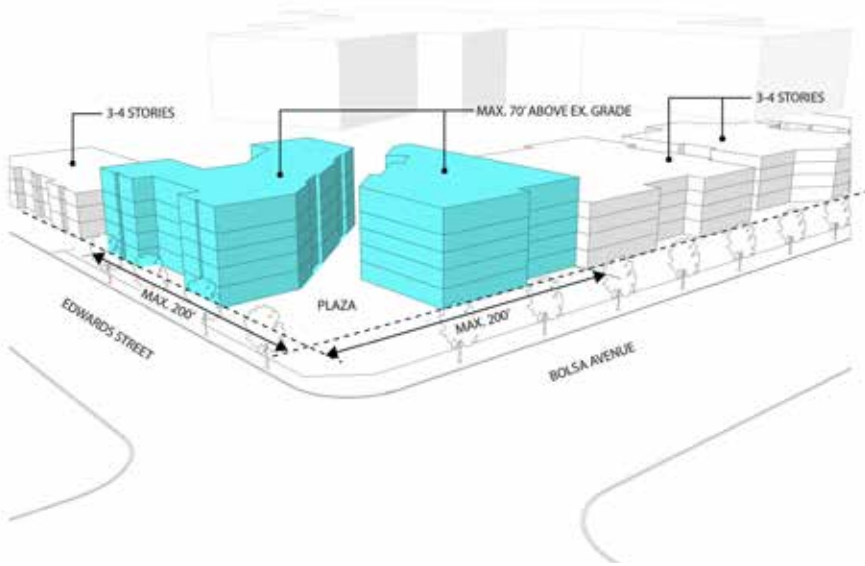
1. The maximum allowable building heights in each zone are measured relative to the base point. The base point is at the southwest corner (Edwards and Bolsa) of the site, the existing ground elevation at this point is 25 feet above the sea level.
2. Within the height zone along Bolsa includes 60 feet of drainage easement that cannot be built upon (structures prohibited).
3. Architectural features (not habitable) may project above the maximum building height and are exceptions that may be allowed to create visual interest.
4. Hotel uses at the southwest corner of the site (Edwards and Bolsa), may go up to +70 feet above the existing ground elevation in Zone1 (25 feet above the sea level) so long as they meet the required frontage setbacks identified in Figures 5.2, Bolsa Avenue Frontage and 5.3, Edwards Street Frontage, and the additional height may measure no longer than 200 feet in length as measured from the intersection of the two building setback lines. The corner shall also include a corner plaza or gathering space that is connected to internal pathways or other pedestrian areas on site (see Section 5.2.9, Objective Building Design, Massing/Articulation, item 21).



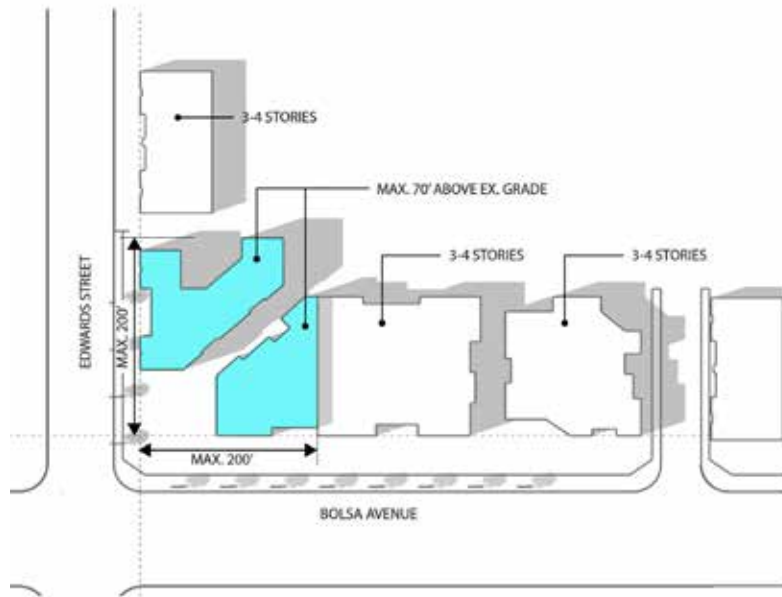
Note: Illustrative only; to be interpreted as conceptual, as one possible design, and not considered definitive.

## 5.2.12 AFFORDABLE HOUSING REQUIREMENT

1. Ten percent (10%) of all housing units built within the WMSP must be income restricted.
2. Only projects proposing residential are required to provide affordable units or the in lieu fee as described in Chapter 9.
3. Projects of 30 units or less can also use an in lieu fee as described in Section 9.10.
4. Property owners may choose to include affordable units in a market development, as a stand-alone project, or a combination of both.
5. All affordable units must be provided within the WMSP project area. They may be provided within each development site, or may be located elsewhere in the WMSP project area so long as an equivalent number of units are provided on another deed restricted site in the WMSP.
6. All affordable units shall be designed on the exterior to the same level of architectural detail and design as market rate units.
7. Table 5.3, *Affordability Levels*, illustrates the percent mix required at each income level. The total number of affordable units will be dependent upon the total units constructed in the WMSP (which may be less than the maximum 3,000 units allowed). Percentages associated with each income level represent the minimum share of units for that level. Units may be replaced by a lower income level, but not a higher one.
8. Property owners may share the costs of construction by paying into a fund, in lieu fee reference in Chapter 9.



*Illustrations of maximum height for hotel uses at the southwest corner of the site (Edwards St. and Bolsa Ave.) shown in blue. (Above: 3D visualization, Below: Plan view)*



**Table 5.3: Affordability Levels**

Income Bracket	% of RHNA Restricted Allocation	Unit Count (based on 10% of 3,000 total)
Very Low (<50% AMI)	37%	110
Low (50-80% AMI)	29%	86
Moderate (80-120% AMI)	35%	104

### 5.2.13 RESIDENTIAL UNIT SIZE AND MIX

A variety of housing unit types and sizes promotes a more balanced community. A mix of dwelling unit types and sizes is encouraged for all development projects.

The required minimum unit size is 400 square feet. All affordable units shall be designed with similar size floor plans as market rate units within a proposed project.

### 5.2.14 VIEW CORRIDORS

As described in Chapter 3, *Community Structure and Land Use Plan*, the location of the WMSP is unique in that it serves as a regional gateway into Orange County. Views from the I-405 provide a unique, elevated vantage point into the site, while gateway entries from Bolsa Avenue and Edwards Street can provide opportunities to maintain views that attract local visitors to the site. Special attention to the treatment of buildings and the public realm terminating vistas can greatly enhance the character of the area and establish a unique sense of identity.

1. Projects shall be sited and designed to enhance views, by creating views that terminate at project amenities, activity centers, or other areas of visual interest that enhance the WMSP’s draw and interest from visitors with specific consideration of those views into the site from the freeway.

### 5.2.15 OPEN SPACE REQUIREMENTS

The Westminster Mall Specific Plan is envisioned as a mixed-use urban environment to be interwoven by a series of parks, open space, paseos, promenades and plazas. Creation of a recreational, healthy and active lifestyle-focused community is a fundamental component of the Westminster Mall Specific Plan. A variety of open space types as shown in Table 5.4, *Open Space Types* will be provided in both privately and publicly accessible areas.

The City’s Parks and Recreation Facilities Master Plan requires a total of 17 acres of Public Open Space be provided in the Specific Plan area. Of the 17 total acres, 9.5 acres will be provided through the combination of the Urban Park Plaza, Neighborhood Park, and additional open space areas as identified in Table 5.5, *Open Space Requirements*. The locations of the Urban Park Plaza and Neighborhood Park are shown in Figure 5.15, *Open Space Location and Sizing*. The precise location and boundaries of these parks or open spaces may shift as the build-out process unfolds so long as alternative sites of equivalent size are identified and secured. The two larger amenities are distributed over the WMSP site to encourage walkability and to prevent them from being grouped all in one location, which may have implications for pedestrian access and internal circulation. The additional 7.5 acres of open space will consist of privately owned, but publicly accessible passive park and open space areas which will be incorporated into the WMSP area as development projects are designed and approved through the City’s site planning review process.

1. Public open spaces shall include flexible areas for public gatherings, such as a lawn area or a paved plaza, at a scale that maintains intimacy, form, and character and contributes to a well-connected public realm.

2. Public plazas shall be located at intersections of streets or adjacent to midblock pedestrian crossings and be prominently integrated with internal sidewalks and streets. Plazas at corners are encouraged to include outdoor dining space for adjacent restaurants.
3. A public open space such as a corner plaza, public art, or architectural landmark form shall be provided at the intersection of Bolsa Avenue and Edwards Street to enhance the attractiveness of the Gateway.
4. Paseos must be a minimum of 5,000 square feet and larger than 40 feet on any side to qualify as open space for public and private common open space.
5. Required build-to lines and street setback areas cannot be used to satisfy required open space areas.
6. Bicycle and pedestrian trails not included within the public right-of-way may be considered usable open space.
7. Open space can be located at grade not limited, above grade limited, including on rooftops, parking structure rooftop decks, decks, patios, and the like.
8. The Community Development Director may approve alternative configurations on a project specific basis, if such changes would be consistent with the intent and goals of this plan. See Chapter 9, *Administration and Implementation* to determine the appropriate review process.
9. Public open space, trails, pathways and bicycle trails shall be constructed for each development in a manner that will be generally accessible to the public and that will interconnect with similar facilities in adjacent developments so as to form an integrated system of open space and trails connecting activity centers, important views and destinations in the WMSP project area.
10. Private open spaces include yards, courtyards, plazas, paseos balconies, decks, porches, roof decks and patios. Indoor gyms associated with a residential or hospitality use may also be counted as usable open space. Usable open space does not include driveways, aisles, parking spaces or side or rear yards less than eight feet in width or front yards unless permitted by the provisions of the Westminster Municipal Code.
11. No individual project approval shall be denied or delayed on the basis of a project-specific or WMSP wide open space deficiency so long as the project provides 1.75 acres of public open space per 1,000 residents within the project, pays all required park fees (e.g., Ch. 3.62 of the Westminster Municipal Code), and has paid or is obligated to pay by condition of approval on the project land use entitlements any Multi-Owner Equitable Reimbursement Obligation fair share payment which maybe owed.

**Table 5.4: Open Space Types**

Type	Provisions
Public Open Space	Privately owned and publicly accessible (e.g. Special Use Park, Neighborhood Park promenades, plazas, paseos, trails, and the Navy Trail)
Private Common Open Space	Privately owned and privately accessible common space (e.g. common recreation areas, pool decks, court yards, balconies, patios)
Private Open Space	Privately owned and privately accessible (e.g. balconies, patios or roof terraces)

**Table 5.5: Open Space Requirements**

Type	Min.	Provisions
Urban Park Plaza	3.0 acres	Public Open Space A minimum 3-acre urban plaza or promenade and event space must be provided.
Neighborhood Park	2.5 acres	Public Open Space Urban-oriented neighborhood park (minimum 2.5 acres) with space for play, social gatherings, and fitness/active recreation.
Westminster Nature Activity Trail	1.0 acres	Public Open Space The Westminster Nature Activity Trail is a required improvement of the WMSP and may be counted toward Public Open Space requirements. Currently the City maintains an easement for the Westminster Nature Activity Trail over privately owned land.
Internal Community Paseo	1.5 acres	Public Open Space The internal community paseo would be an extension of the Westminster Nature Activity Trail, providing a direct pedestrian connection from within the project to provide regional access.
Bolsa Promenade	1.5 acres	Public Open Space Bolsa Avenue frontage requires a setback of 72ft for the drainage easement. A promenade or linear park consisting of hardscape and softscape features allowing for social gatherings and events must be provided.
Linear Park	20,000 sf	Public Open Space Must have a minimum dimension of 100ft in any direction.
Any Development	10% of project area	Maximum 25% of project area open space requirement may be private open space (balconies, patios, etc.). Minimum 75% of the project area open space requirement must be either be Public Open Space or Private Common Open Space. Open space may be satisfied elsewhere within the Westminster Mall Specific Plan boundary. A minimum 50% of the open space requirement must be satisfied on site. Sidewalks may not be counted as part of the open space unless they are part of a larger open space element
Residential Uses	100 sf per unit as Private, Private Common Open Space or Common Open Space	Private open space shall be provided on a balcony, patio or roof terrace, with a minimum of 36 sf and a minimum width of 6 ft. Courtyards shall have a minimum dimension of 40 ft in any direction (building face to building face). A minimum of 50 percent of the courtyard space (including courtyards that are on-structure) shall be landscaped.
<p><i>Note: To qualify as open space for public and private common open space, areas must be a minimum of 5,000 square feet and larger than 40 feet on any side.</i></p>		

**Figure 5.15: Open Space Location and Sizing**



*Note: The precise locations of the 3-acre Urban Park Plaza and 2.5-acre Neighborhood Park are shown above. The precise location and boundaries of these two parks may shift as the build-out process unfolds so long as alternative sites of equivalent size are identified and secured. In contrast, the locations of the Internal Community Paseo and Bolsa Promenade are conceptual, and are not considered definitive.*



*Landscape berm provides separation between play space and walkway.*



*Seating area using alternative paving material to highlight a separate space from the sidewalk.*



*Planted seat wall to force views into the project and acts as a sound barrier from circulation traffic.*



*Passive courtyard space creating areas of respite.*



*Sculptural play equipment adds visual appeal.*



*Color and artfully designed seating can activate the space in any area.*

## 5.2.16 LANDSCAPE DESIGN

The harmonious design of landscaping elements, including plant material and decorative hardscaping, can increase visual interest, provide identity, and encourage pedestrian activity.

1. Landscaping for projects (including public right-of-way medians) within WMSP shall conform to the standards set forth in the Specific Plan. Whenever a standard or regulation is not set forth in the Specific Plan the project must be consistent with the provisions of Chapter 17.310 *Landscape Standards* in the Zoning Code. Landscaping shall be consistent with the efficiency standards in Title 21 of the California Building Code as well. Landscaping shall also be designed to be consistent with Section 7.2.5, *Landscaping*.
2. The design and landscape along each internal roadway shall be consistent along the entire roadway, formalized, and composed of signature plantings to create an attractive and cohesive identity. Unique qualities of individual developments or uses may be highlighted through variation and landscape design while using similar landscape elements.
3. A minimum of 15% of all project sites must be landscaped, See Municipal Code Chapter 17.310 Landscaping.
4. All commonly owned property and landscaped setback areas exclusive of structural improvements shall be landscaped and maintained in a weed-free condition with a combination of trees, shrubs, and ground cover in a variety of sizes.
5. Landscaping shall be used to connect buildings with outdoor spaces, entries, transitional spaces, and gathering places. Trees should be used to create an intimate scale, enclose spaces, and frame views.

## PLANT MATERIAL

6. Projects in mixed-use designations shall utilize at least 75 percent native California or drought tolerant plant and tree species appropriate for the climate zone region (per Section A4.106.3 of CALGreen, 2019).
7. Landscaping shall be drought-tolerant and feature native, non-invasive, adaptive plants (per CALGreen standards and Cal-IPC species).
8. The selected plant species and design and placement of landscaping shall provide for visibility and surveillance of pedestrian areas.
9. Trees and shrubs shall be located and spaced to allow for mature and long-term growth of canopies and root spaces.
10. Trees planted in parkways along Internal roadways (Primary Internal Circulation Street, Internal Main Street and Residential Streets) shall be selected from the tree palette provided in Table 5.6, *Internal Parkway Tree Species*.



*Parkway uses planting to delineate spaces for seating and bike parking.*



11. Each block of an internal roadway shall have a single species of large, deciduous trees in planting wells with tree grates a maximum spacing of 40 feet on-center.
12. All newly planted trees shall be a minimum 36-inch box size, unless otherwise noted.
13. Landscape planters shall be adequate in width to provide ample root ball space and growing room for the selected plant material.

### LANDSCAPE IRRIGATION AND MAINTENANCE

14. Irrigation systems shall be designed to apply water slowly, allowing plants to be deep watered and reducing runoff.
15. Low volume irrigation drip systems shall be used in all areas except turf irrigation and small ornamental planting.
16. Each street tree shall be watered by at least two deep watering bubblers separate from all other irrigation.
17. Drip irrigation systems shall be used with roof gardens to conserve water.
18. Irrigation systems shall incorporate water conservation methods and water efficient technologies, such as drip emitters, evapotranspiration controllers, and moisture sensors.

### STORMWATER TREATMENT

19. Urban runoff can be greatly reduced by diverting storm water from impervious areas, such as concrete surfaces, to landscaped areas. Infiltration basins where water can seep into the ground should be used.
20. Grading and plan layout shall be designed to capture and slow water runoff.
21. Landscape-based water treatment methods should be used in coordination with green infrastructure techniques, such as bioretention areas and rain gardens.



*Rain gardens are an attractive use for collecting and mitigating water runoff around the site.*



*Biofiltration adds another benefit to medians by collecting and filtering water runoff from parking lots, helping to lower pollution and storm water.*

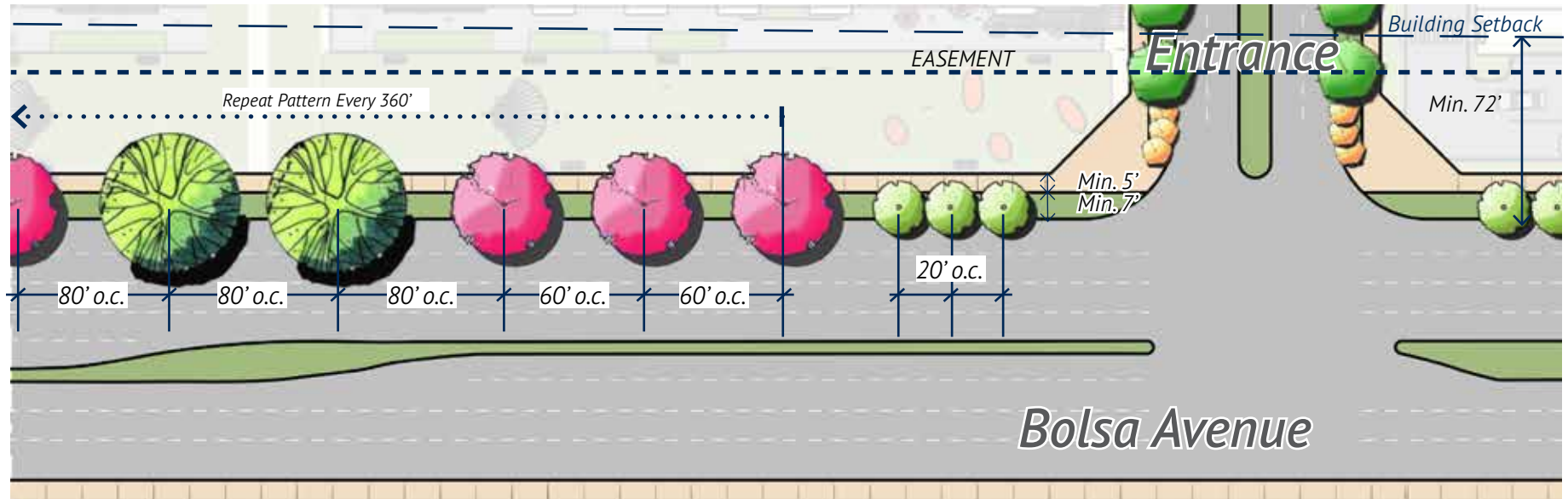
**Table 5.6: Internal Parkway Tree Species**

Botanical Name	Common Name	Water Use	Color Impression
<i>Acacia baileyana</i>	Bailey Acacia	Low	Gray
<i>Acacia stenophylla</i>	Shoestring Acacia	Low	Gray
<i>Afrocarpus gracilior</i>	African Fern Pine	Medium	Green - dark
<i>Agonis flexuosa</i>	Peppermint Tree	Low	Green - gray
<i>Albizia julibrissin</i>	Mimosa	Low	Pink
<i>Arbutus marina</i>	Strawberry Tree	Low	Green - green
<i>Bauhinia x blakeana</i>	Hong Kong Orchid Tree	Medium	Pink
<i>Cassia leptophylla</i>	Gold Medallion	Medium	Yellow
<i>Ceiba speciosa</i>	Pink Silk Floss	Low	Pink
<i>Chionanthus retusus</i>	Fringe Tree	Medium	White
<i>Corymbia ficifolia</i>	Red Flowering Gum	Low	Red
<i>Ficus rubiginosa</i>	Rusty Leaf Fig	Medium	Green - dark
<i>Fraxinus velutina</i>	Arizona Ash	Medium	Green - yellow
<i>Geijera parvifolia</i>	Australian Willow	Low	Green - gray
<i>Gingko biloba</i>	Maidenhair Tree	Medium	Yellow
<i>Jacaranda mimosifolia</i>	Jacaranda	Medium	Purple


Botanical Name	Common Name	Water Use	Color Impression
<i>Koelreuteria paniculata</i>	Golden Rain Tree	Low	Yellow
<i>Lagerstroemia indica</i>	Crape Myrtle	Medium	Pink
<i>Lagunaria pattersonii</i>	Primrose Tree	Low	Pink
<i>Laurus 'Saratoga'</i>	Saratoga Laurel	Low	Green - dark
<i>Lophostemon confertus</i>	Brisbane Box	Medium	Green - green
<i>Magnolia grandiflora</i>	Southern Magnolia	Medium	Green - dark
<i>Melaleuca linariifolia</i>	Flaxleaf Paperbark	Low	White
<i>Pinus eldarica</i>	Afghan Pine	Low	Green - green
<i>Pinus halepensis</i>	Aleppo Pine	Low	Green - green
<i>Pinus pinea</i>	Stone Pine	Low	Green - dark
<i>Platanus 'Bloodgood'</i>	London Plane Tree	Medium	Green - green
<i>Prosopis alba</i>	Argentine Mesquite	Low	Green - green
<i>Spathodea campanulata</i>	African Tulip Tree	Medium	Red
<i>Tiuna tipu</i>	Tipu Tree	Low	Yellow
<i>Ulmus Parvifolia</i>	Chinese Elm	Medium	Green - green



**Figure 5.16: Parkway Requirement -Bolsa Avenue**



Note: Illustrative only; to be interpreted as conceptual, as one possible design, and not considered definitive.

 Not to Scale

**Plant List:**



**Common Name:**  
Chinese Pistache  
**Scientific Name:**  
*Pistacia chinensis*  
**Height:** 35 ft.  
**Spread:** 35 ft.  
**E / D:** Deciduous  
**Min. Planter Width:**  
6 ft.

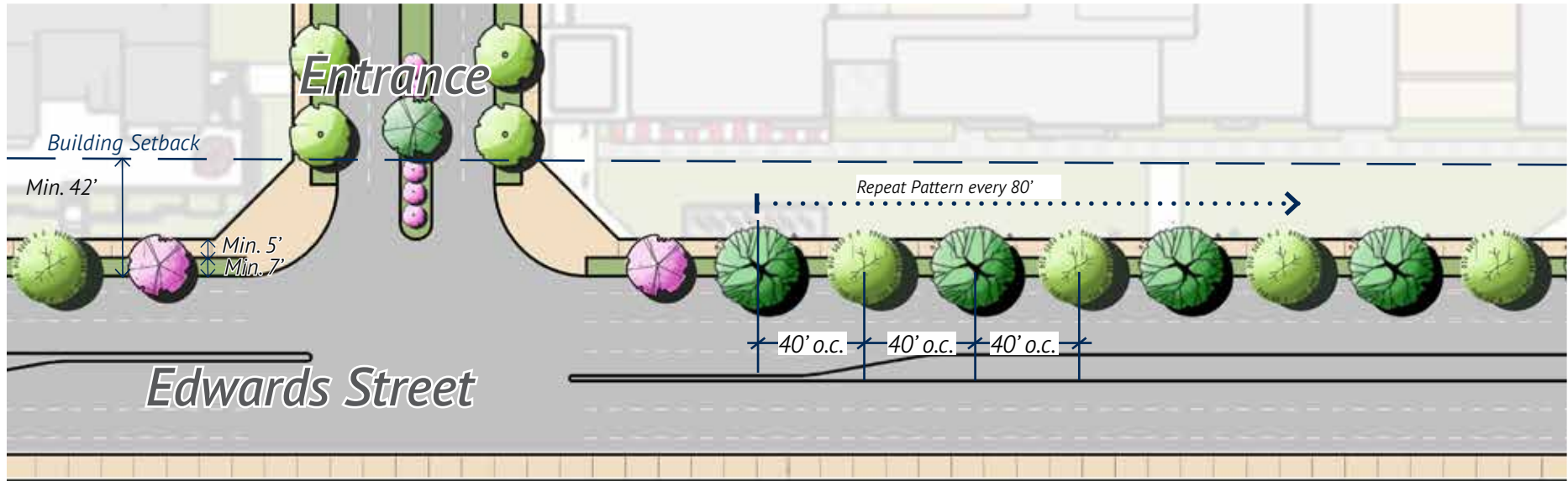


**Common Name:**  
Brisbane Box  
**Scientific Name:**  
*Lophostemon confertus*  
**Height:** 40-60 ft.  
**Spread:** 35 ft.  
**E / D:** Evergreen  
**Min. Planter Width:**  
10 ft.



**Common Name:**  
Strawberry Tree  
**Scientific Name:**  
*Arbutus unedo*  
**Height:** 20 ft.  
**Spread:** 20 ft.  
**E / D:** Evergreen  
**Min. Planter Width:**  
4 ft.

**Figure 5.17: Parkway Requirement - Edwards Street**



Note: Illustrative only; to be interpreted as conceptual, as one possible design, and not considered definitive.

 Not to Scale

**Plant List:**



**Common Name:**  
London Plane  
(Bloodgood)  
**Scientific Name:**  
*Platanus acerifolia*  
**Height:** 50 ft.  
**Spread:** 30 ft.  
**E / D:** Deciduous  
**Min. Planter Width:**  
6 ft.



**Common Name:**  
Golden Rain Tree  
**Scientific Name:**  
*Koelreuteria  
paniculata*  
**Height:** 25 ft.  
**Spread:** 30 ft.  
**E / D:** Deciduous  
**Min. Planter Width:**  
6 ft.



**Common Name:**  
Crape Myrtle  
**Scientific Name:**  
Lythraceae  
**Height:** 20 ft.  
**Spread:** 10 ft.  
**E / D:** Deciduous  
**Min. Planter Width:**  
4 ft.

## 5.2.17 FENCES AND WALLS

1. Walls and fences shall be made of durable, yet attractive materials that complement and blend with the adjacent architecture.
2. Both sides of all perimeter walls or fences shall be articulated. This primarily relates to solid walls separating internal uses (residential transitions to non-residential uses), as opposed to perimeter fences along Bolsa Avenue and Edwards Street, which are envisioned to be a combination of solid wall and see-through fencing (wrought iron) and landscaping.
3. Perimeter fencing utilized along public street shall be constructed of decorative iron, pre-painted welded steel, or wood picket material.
4. The height of solid fencing between private yards and common open spaces shall be limited to 4.5 feet in height. If a 6 foot fence is desired, the top 18 inches shall allow for vision in and out of the yard.
5. Walls shall have breaks, recesses, and/or offsets, especially at entries and important intersections.



*Planted edge creates a visually softer barrier from the transit while the low fence behind the hedge adds a structural barrier.*

## 5.2.18 LIGHTING

1. Outdoor lighting should be consistent with Westminster Municipal Code, Chapter 17.300.040.
2. Lighting shall be used to incorporate Crime Prevention Through Environmental Design (CPTED) to provide illumination for the security and safety of on-site areas, such as parking, loading, shipping and receiving, building entrances and pedestrian parkways.
3. Energy-efficient ENERGY STAR® certified lighting fixtures and equipment shall be used.
4. Pedestrian-scale decorative street lighting shall be a maximum spacing of 80 feet on-center. Light source should be located 12-14 feet above finished grade.



*Overhead area lighting on street corners for pedestrian safety.*



*Lighting poles used for hanging plants as additional planting areas.*



*Raised planter parkway to create a barrier from vehicular traffic.*

## 5.2.19 STREET FURNITURE

1. Street furniture includes items such as benches with middle arm-rests, bus shelters, trash receptacles, plant containers, tree grates and guards, bicycle racks, and bollards.
2. The design and selection of street furniture shall include considerations for the security, safety, comfort and convenience of pedestrians, including the handicapped.
3. Street furniture shall be conservative in use of sidewalk space, and maintain a clear width sufficient to accommodate pedestrian flows.
4. All street furniture shall be constructed of long-wearing, vandal-resistant materials.
5. The selection, siting and layout of street furniture shall ensure that each item or structure is designed and situated to be in harmony with both the surrounding furnishings and the area as a whole. All street furniture to be installed by private parties as part of a development shall be subject to approval by the City.
6. Appropriate materials for street furniture may include stucco, concrete, painted metal or anodized aluminum. Decorative tiles may be used as an accent.
7. Freestanding trash receptacles shall be bolted into the ground, have a plastic liner and rounded cover.
8. Large plant containers and planters shall be open to the ground below. A permanent irrigation system shall be installed.
9. Tree grates and tree guards shall be made of cast iron.
10. Tree grates shall be expandable, so that the inner rings can be broken out as the tree grows.



*Outdoor dining area separated by a pedestrian walkway creating a casual dining experience.*



*Shared picnic-style outdoor dining space for use by multiple restaurants.*



*Minimal post and wire with planting for dining with alcoholic beverages.*

## 5.2.20 OUTDOOR DINING

The following standards apply to outdoor dining associated with eating and drinking establishments.

1. The sale of alcohol shall be subject to the review and approval of a Conditional Use Permit.
2. Outdoor dining shall be an extension of an existing or proposed eating establishment on contiguous property and shall be located directly adjacent to the eating establishment.
3. Outdoor dining located along a street front may encroach into the frontage zone of the setbacks. If located on a non-street adjacent side of a building, dining areas may encroach into the side yard setbacks. Outdoor dining areas that are located along public or private streets shall be located within the frontage zone and shall not encroach into the pedestrian zone.
4. Outdoor dining along street frontage or located within a courtyard must include a low fence or wall to provide clear separation from public walkways. There shall be a clear distinction between dining area and the sidewalk through the use of planters and/or physical barriers. A minimum 36-inch high physical barrier shall surround any outdoor dining where alcoholic beverages are served.
5. All tables and chairs shall be of sturdy construction and made of quality materials.
6. Carts and kiosks shall adhere to the standards contained in Section 5.2.22 of this Specific Plan and shall be located in designated areas shown on an approved development plan.
7. The applicant or operator shall pay all fees and deposits required by the Westminster Municipal Code prior to operation of the outdoor dining use.

## 5.2.21 OUTDOOR SALES DISPLAY

Outdoor sales displays allow businesses additional opportunities to sell their products, and simultaneously enhance the pedestrian experience by creating interesting outdoor spaces. These areas are encouraged in the Specific Plan and they should be used in a manner that promotes the character and visual appearance desired by the community.

Outdoor sales displays shall be subject to the standards below. Outdoor exhibit events, including art, cultural, educational displays, arts and crafts exhibits on non-residential properties, seasonal sales lots and outdoor sales events are reviewed separately and require approval of a Temporary Use or Temporary Event Permit per Chapter 17.540 of the WMC.

1. The owner or primary lessee of the private property that immediately abuts the sidewalk or plaza (the Frontage Zone) is the only one who possesses the right sell or display goods.
2. Display areas may extend up to 4 feet from the edge of a building into the Frontage Zone, and may not exceed 60 percent of the width of the business frontage, or 30 linear feet, whichever is smaller. Display areas may not encroach into the Pedestrian Zone.
3. Outdoor sales display areas shall be used by merchants to provide a sample of merchandise that is available inside the store, and are limited to flowers and produce (fruits and vegetables). They are not intended to provide additional retail space or locations for clearance/damaged items.
4. Uses other than flowers or produce may be allowed within the outdoor sales display area subject to approval of the Community Development Director.
5. Outdoor sales displays shall be limited to the regular hours of operation of the respective business. All items placed outside shall be removed each evening.

6. Outdoor sales display areas shall not restrict the free movement of pedestrians.
7. Outdoor sales display areas shall not interfere with access to any commercial establishment. All required exits and access ways, including disabled access, to the tenant space shall be maintained clear of obstructions.
8. Display merchandise shall not obscure or interfere with any official notice, public safety sign, or device.
9. Display merchandise shall not extend into the safe line-of-sight distances at intersections, as determined by the City Engineer.
10. Outdoor sales display areas shall be maintained in an orderly, uncluttered, and aesthetically pleasing condition.
11. Outdoor storage in conjunction with any outdoor use shall be prohibited.
12. Display of merchandise from cars, trucks, or other vehicles is prohibited.
13. Noise shall not be generated by outdoor merchandise.
14. All temporary signs shall be professional in appearance.
15. Portable and A-frame signs shall not be posted outside of the designated display area.
16. Sales to motorists or persons in vehicles is prohibited.
17. Incidental outdoor uses shall not require additional parking beyond that which is otherwise required for the primary permanent use.

## 5.2.22 CARTS AND KIOSKS

1. Carts and kiosks are permitted subject to approval by the Community Development Director and compliance with the following standards. Carts and kiosks may be permitted as a temporary use on Public Open Spaces, privately owned and publicly accessible, subject to approval by both the Community Development and Public Works Directors.
2. No portion of a cart or kiosk shall overhang the property line.
3. The placement of carts or kiosks shall not:
  - Obstruct access to or occupy a parking space
  - Interfere with access to public property or any adjoining property
  - Interfere with maintenance or use of street furniture
4. The cart or kiosk shall be a maximum of 4 feet in width excluding any wheels, a maximum of 8 feet in length including any handle, and a maximum of 6 feet in height excluding canopies, umbrellas or transparent enclosures unless a larger size is approved by the Community Development Director.
5. The number, location and storage of kiosks shall be evaluated as part of the development review process.
6. The following factors shall be considered regarding the location of carts or kiosks:
  - The width of the sidewalk or paseo
  - The location of building entrances
  - Physical obstructions including, but not limited to signposts, light standards, benches, newsstands, utilities and landscaping
  - Vehicle activity in the adjacent street including but not limited to bus stops, truck loading zones, passenger loading zones or parking spaces
  - Pedestrian traffic volumes
  - Handicapped accessibility



7. The following factors shall be considered regarding the design of carts or kiosks:
  - Appropriateness of the cart or kiosk design, color scheme, and character relative to its location
  - Appropriateness and location of signing and graphics
8. During hours of operation, the cart or kiosk must remain in the location specified on the approved site plan
9. Sales to motorists or persons in vehicles is prohibited.
10. The cart or kiosk operator shall pay all fees and deposits required by the Westminster Municipal Code prior to the establishment of the use.
11. Personal services, such as tattoos, hair braiding, and massages, are not permitted outdoors, except for table service of food in conjunction with approved outdoor seating, or as approved by the Community Development Director.
12. The sale of alcoholic beverages shall be prohibited.
13. Fire extinguishers may be required at the discretion of the Orange County Fire Authority.
14. All cart and kiosk uses shall be self-contained for water, waste, and power to operate.
15. A cart or kiosk operator shall provide a method approved by the Community Development Director for disposal of business related waste.



*(Above and below): Boutique carts with plenty of space for circulation and visibility.*



## 5.2.23 SIGNAGE

### MASTER SIGN PROGRAM

A Master Sign Program shall be prepared for the Westminster Mall Specific Plan area that establishes the placement and design criteria for the following:

**Primary Entrance Signage.** Signage to identify the WMSP and City of Westminster (where applicable) for vehicular traffic. Primary entrance signage is anticipated at key intersections, particularly along the I-405 Freeway.

**Secondary Entrance Signage.** Smaller scale identity monumentation to be installed at select intersections where arterials and connector streets (as identified in the City's General Plan Circulation Element) meet the Specific Plan boundary.

**Bolsa Avenue and Edwards Street Entrance Signage.** Signage to identify Westminster Mall for vehicular traffic entering the Specific Plan area at Bolsa Avenue and Edwards Street. Since there are multiple driveways along both streets, one intersection shall be identified as the primary entry from that street and shall require monument signage clearly identifying the intersection as a main entry into the site.

**Freestanding Freeway Signage.** Freestanding signs and billboards are prohibited on this edge.

**Wayfinding and Directional Signage.** Site-wide directional signage identifying parking locations, loading areas (pick up and drop-off), park and open space areas, restrooms, information and the like.

The Master Sign Program shall include the following for each sign type:

1. The location(s) and design for each sign type
2. Sign copy, including potential secondary information at pedestrian scale

3. Maximum sign area, letter size and font
4. Sign construction material(s) and illumination
5. Any specific requirements for Project Level Sign Programs to maintain consistency with the intent of the Master Sign Program.
6. Other considerations, such as placement outside of sightline restrictions, security and safety considerations

The Master Sign Program may also include other types of architectural and environmental graphic enhancements, such as paving accents, street banner layout, or transit station design elements that integrate the Westminster Mall brand into other elements of the built environment (Chapter 7, *Design Guidelines*). The Master Sign Program is subject to approval by the Community Development Director.

### PROJECT LEVEL SIGN PROGRAM

Sign programs are required for individual projects with multiple users/tenants, and shall be reviewed and are subject to approval by the Community Development Director based on consistency with the Master Sign Program as well as Project Level Sign Standards and Guidelines. The following general design guidelines shall apply:

7. Signs should be compatible with the building in terms of color, material, and style.
8. Signage should be visually interesting and informative.
9. Signs shall be compatible in theme, color and size with the architectural elements of the building.
10. Pedestrian pass-throughs shall have clearly marked and consistent signage, well lit with bollard lighting as appropriate.

## PROJECT LEVEL SIGN STANDARDS AND GUIDELINES

The following standards and guidelines supersede those in Chapter 17.330 Signs, of the Westminster Municipal Code. Where the Specific Plan is silent, the Community Development Director is authorized to provide determinations and interpretations regarding development standards. These standards and guidelines apply to Project Level Sign Programs and/or a single use project for which there is no sign program. Any modifications shall be considered a Minor Modification per Section 9, *Administration and Implementation*, and shall not require a Specific Plan Amendment.

11. The following types of signs are prohibited in the Westminster Mall Specific Plan area:

- Pole signs
- Billboard signs
- Roof signs
- Human Signs
- Inflatable Signs
- Can Signs (unless it is a trademark logo)
- Supergraphics

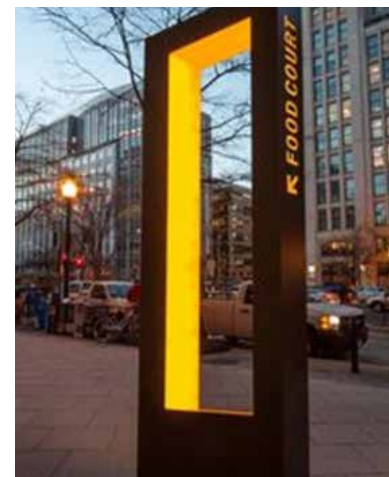
12. The following types of signs are allowed within the Specific Plan area:

- A-Frame/Temporary Signs
- Address Signs
- Awning Signs
- Band Signs
- Blade Signs
- Building Marquees
- Building Sign
- Nameplates

- Outdoor Display Cases
- Real Estate Signs
- Window Signs
- A Pylon Sign, in the general location identified in Figure 5.18 and in accordance with development standards set forth in an approved Sign Program for on-site advertising purposes only and subject to Planning Commission review and approval of a Conditional Use Permit.

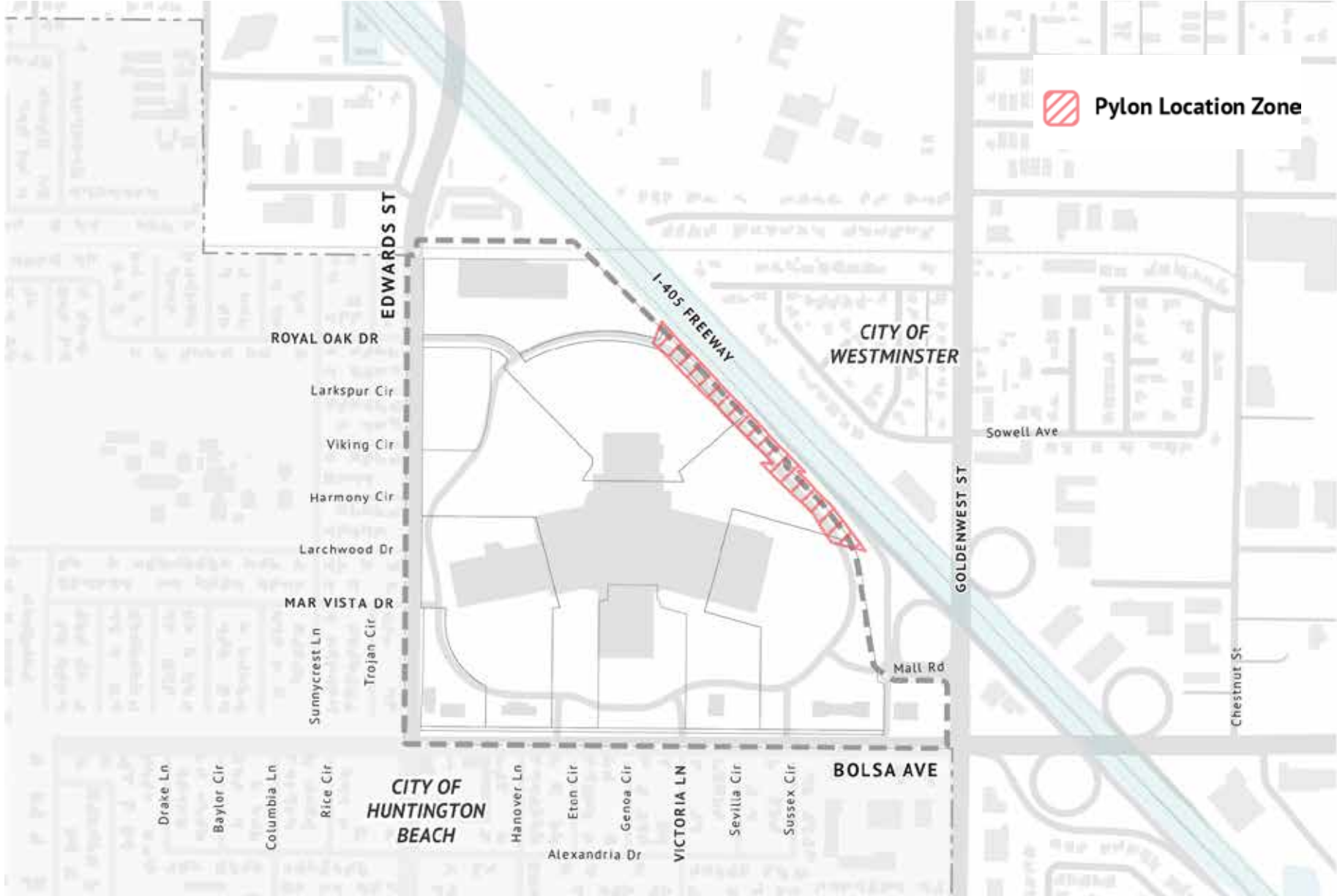
13. The following sign illumination techniques are allowed within the Specific Plan area:

- Wall-mounted down-pointing lights that externally illuminate signs
- Back-lighting of cut-out letters and symbols that are mounted to the wall
- Internal illumination of band signs or outdoor display cases where the letters and symbols are illuminated, but not the sign background.
- Internal illumination of address signs, awning signs, marquees and murals or building art.



*Human scale wayfinding signage make on site navigation easier.*

Figure 5.18: General Location for Pylon Signage



## A-FRAME/TEMPORARY SIGNAGE

A movable freestanding temporary sign, typically double-sided and placed near the entrance of a business.

14. Maximum height allowed is three feet, not to obstruct views into shops, or pedestrian and automobile line of sight.
15. Sign must be temporary, showing name, logo, and phone number.
16. Sign cannot obstruct pedestrian passage.
17. Sign cannot be attached to landscape (i.e. trees).



## ADDRESS SIGNAGE

A small sign located near the entry door that displays the building's dwelling or business address.

18. Address sign numerals applied to retail, office, residential, institutional, or industrial buildings shall be between four and six inches tall. Address Sign numerals applied to individual dwelling units in Apartment buildings shall be at least two inches tall.
19. Address signs shall be easily visible by using colors or materials that contrast with their background.
20. Address signs shall be constructed of durable materials.
21. The address sign shall be attached to the front of the building in proximity to the principal entrance or at a mailbox.



## AWNING SIGNAGE

Lettering may be applied directly on the valance or other vertical portion of an awning (the face).

22. The following variations of awnings, with or without valances, are permitted:
  - Fixed or retractable awnings
  - Shed awnings
  - Dome awnings
23. Awnings shall be a minimum of four feet in depth.
24. Awnings shall not extend beyond the width of the building or tenant space, nor encroach above the roof line or the story above.
25. Awning signs shall contain only the business name, logo, and/or street address.
26. Letters, numbers, and graphics shall cover no more than 70% of the valance area.
27. Letters, numbers, and graphics coverage shall cover no more than 20% of the awning face area.
28. Lettering is not permitted in any area other than the awning valance or vertical portion of an awning.
29. Awning signs may be internally illuminated.



*Lettering on valances shall cover no more than 70% of valance area.*



*The lettering on this awning is located on the awning valance; the star symbol/ design feature in the yellow area is located on the awning face.*

## BAND SIGNAGE

A flat sign attached to the exterior wall of any building or structure on the lower floors over the retail face.

30. All businesses are permitted one band sign on each first story façade.
31. Band signs shall include only letters, background, lighting, and an optional logo. Information shall consist only of the name and/or logo of the business. Band signs shall not list products, sales, promotional messages, or contact information.
32. The following construction types are permitted:
  - **Cut-out Letters.** Letters shall be individually attached to the wall or on a separate background panel, and shall be externally illuminated.
  - **Flat Panel Letters.** shall be printed or etched on same surface as the background, which is then affixed to the wall and externally illuminated.
  - **Channel Letters.** Each letter shall have its own internal lighting element, individually attached to the wall or onto a separate background panel. The letter shall be translucent, or solid to create a backlit halo effect.
33. Height and width shall be measured using the smallest rectangle that fully encompasses the entire extent of letters, logo and background.
34. Band signs shall not be wider than 90% of the width of the building façade or tenant space.
35. Band signs shall not project vertically above the roof line.
36. Band signs may be illuminated from dusk to dawn or during hours permitted by the lighting ordinance. External lights shall be shielded from direct view to reduce glare and downward facing.
37. Electrical raceways, conduits and wiring shall not be exposed. Internal lighting elements shall be contained completely within the sign assembly, canopy channel, or inside the wall.
38. Band signs should be placed where the architectural features suggest the best placement for signage. They should be vertically aligned with the center of an architectural feature, such as a storefront window, entry portal, or width of a bay or overall retail space.
39. Where multiple band signs are present on a single building (i.e. for retail tenants in a shopping center), signage shall be coordinated in terms of scale, placement, colors and materials.



## BLADE SIGNAGE

A sign mounted near the main entry of the business, projecting at a 90 degree angle.

40. Blade signs may be double-sided.
41. Blade signs shall be permitted only for businesses that have an entrance on the ground floor.
42. Blade signs may encroach into the public frontage up to four feet and shall clear the sidewalk by a vertical clearance of at least eight feet.
43. Blade signs shall not encroach above the roof line nor above the bottom of the second story window.
44. Text and graphics on the blade signs shall be limited to the name and/or logo of the business. Slogans, address labels, operating hours and contact information shall not be permitted.
45. Mounting hardware, such as supports and brackets, may be simple and unobtrusive or highly decorative, but shall complement the design of the sign, the building, or both.
46. For buildings with multiple signs, mounting hardware or sign shapes, sizes and colors shall be coordinated.



## BUILDING MARQUEE SIGNAGE DESCRIPTION

A structural feature of a building that provides shelter and sign space.

47. Marquees shall be located only above the principal entrance of a building.
48. No marquee shall be wider than the entrance it serves, plus two feet on each side thereof.
49. No portion of a marquee shall be lower than ten feet of vertical clearance.
50. No marquee shall extend closer to the curb than three feet to the curb.
51. Columns or posts may be used as supports for marquees eight feet deep or deeper by CUP.
52. All marquees, including anchors, bolts, supporting rods and braces, shall be constructed of non-combustible materials and shall be designed by a structural engineer and approved by the Building Inspector.
53. Marquee components and materials may vary. Anchors, bolts, and supporting rods should be limited to the interior of the Marquee.
54. Message boards are permitted as part of marquees.
55. Band signs are permitted as a part of a marquee sign.





## BUILDING SIGNAGE

A permanent flat sign attached to the exterior wall of any building or structure on the top of the building.

56. Building sign may be located below the parapet, or below the eave. No building sign can extend above the roof line or past the wall edge.
57. Over the main entrance, a building sign can be three square feet for each lineal foot of building face were the main entrance is located.
58. Signs painted on walls must comply with standards.
59. Building signs shall include only letters, background, lighting, and an optional logo. Information shall consist only of the name and/or logo of the business.
60. The following building sign construction types are permitted:
  - **Cut-out Letters.** Letters shall be individually attached to the wall or on a separate background panel, and shall be externally illuminated.
  - **Flat Panel Letters.** Letters shall be printed or etched on same surface as the background, which is then affixed to the wall and externally illuminated.
  - **Channel Letters.** Each letter shall have its own internal lighting element, individually attached to the wall or onto a separate background panel. The letter shall be translucent, or solid to create a backlit halo effect.
61. Height and width shall be measured using the smallest rectangle that fully encompasses the entire extent of letters, logo and background.
62. Building signs shall not be wider than 50% of the width of the total building façade.

63. Building signs may be illuminated from dusk to dawn. External lights shall be shielded from direct view to reduce glare and downward facing. Building signs may be illuminated during business hours or as permitted by the project approvals.
64. Electrical raceways, conduits and wiring shall not be exposed. Internal lighting elements shall be contained completely within the sign assembly or inside the wall.
65. Building signs should be placed where the architectural features suggest the best placement for signage. They should be vertically aligned with the center of an architectural feature or façade articulation, such as towers, entry portal, or window bays. They shall not interrupt or obscure these features or cause visual disharmony.



## CANOPY SIGN

Canopy Signs are signs which are mounted partially or entirely above the front of a canopy and oriented parallel to the building wall surface.

66. Canopy Signs are permitted above the canopy with a maximum height of two feet.
67. Canopy Signs shall only be permitted for non-residential uses with a dedicated ground floor entrance and multi-family buildings with a common lobby entry.
68. Canopy Signs shall project no further than the front of the associated canopy.
69. No portion of a Canopy Sign shall be less than eight feet above the level of the sidewalk or other public right-of-way over which it projects.
70. Lettering for Canopy Signs shall include only one line of lettering using individual letters only.
71. Exposed letters used on Canopy Signs should be wood, metal, and painted materials.
72. If Canopy Signs are illuminated, they shall be lit by external, halo, exposed neon tube, exposed incandescent bulb, or exposed LED illumination only. They may also be translucent letters that are edge-lit.



*Signs above awnings or canopies may be considered as part of a Project Level Sign Program.*

## NAME PLATE SIGNAGE

A panel with listing of names of business or building tenants located near entry.

73. Nameplates shall consist of either a panel or individual letters applied to a building wall within ten feet of an entrance to the building.
74. One Nameplate shall be permitted per address.
75. Nameplates shall not exceed three square feet.
76. Nameplates shall be constructed of durable materials.



## OUTDOOR DISPLAY CASE SIGNAGE

A lockable metal or wood framed cabinet with a transparent window, mounted on the wall. It may contain maps, menus, or other information.

77. Each outdoor display case shall not exceed six square feet.
78. Outdoor display cases may be externally or internally illuminated.
79. Theaters may be permitted larger outdoor display cases through approval by the Community Development Director.
80. Outdoor display cases shall not be attached to shopfront windows.



## WINDOW SIGNAGE

A sign placed, or painted on the interior of a shopfront window or business door.

81. Only the following window sign types shall be permitted:
  - Vinyl applique letters applied to the window (no background)
  - Letters painted directly on the window
  - Hanging signs that hang from the ceiling behind the window
  - Neon signs
  - Door signs applied to or hanging inside the glass portion of an entrance doorway
82. Window signs shall not interfere with the primary function of windows, which is to enable passersby and public safety personnel to see through windows into premises and view product displays.
83. Window signs shall be no larger than 25% of the total area of the window onto which they are applied. Sign area shall be measured using the smallest rectangle that fully encompasses the entire extent of letters, logo and background.
84. Window signs may list services and/or products sold on the premises, or provide phone numbers, operating hours or other messages, provided that the total aggregate area of these messages not exceed the limit provided above.
85. Letters on window signs shall be no taller than eight inches.



## MONUMENT SIGNAGE

Monument signage will identify the Mall to visitors from the I-405 freeway ramps, Bolsa Avenue, and Edward Street. It can also be used to identify districts and projects within the Plan Area. See Figure 3.5 for recommended locations.

86. Monument signage should have a consistent theme throughout the Plan Area and be coordinated with the plan for the Mall site's branding and identity
87. Monument signage should be architectural and freestanding or gateways
88. Appropriate locations may include corners, adjacent to or above entryways, or in planted medians.
89. Signage should be coordinated with appropriate perimeter landscaping
90. Moving signage visible from the I-405 must adhere to Caltrans' guidelines



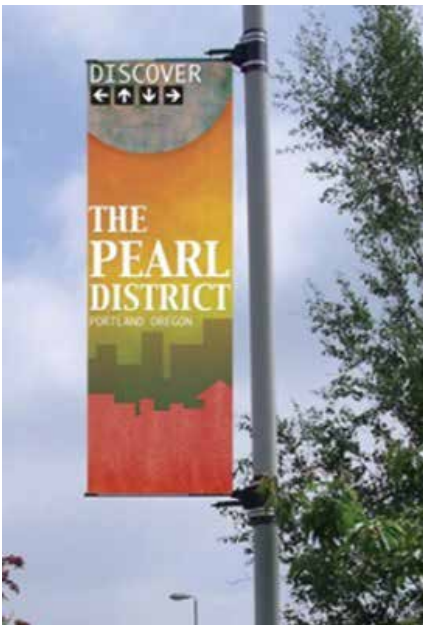
*Simple monument signs that creates a statement and sense of place.*



## STREET BANNERS

Street banners hung on utility and light poles throughout the Plan Area are encouraged to reinforce the site's identity. Banners' function may be expanded to provide information about current cultural events or community activities. Refer to *Chapter 6, Mobility*, for more information on street design.

91. Banners should be coordinated throughout the Mall area
92. Banners that identify properties in the WMSP should be coordinated with the overall Mall branding and identity. Banners that identify events may reflect the branding of the temporary activity.



*Banners encourage site identity along vehicular routes.*



*Pedestrian and vehicular signage for site navigation.*

## STREET-LEVEL SIGNAGE

Street-level signs play an important role in giving direction, providing information, and attracting residents, shoppers, and visitors. Street level signage can aid visitors to locate residences and businesses.

93. Signs should be consistent with the proportion and scale of building elements within the façade.
94. Signs should be professionally or artfully constructed and made of quality materials.
95. Figurative signs, or signs reflecting the type of business through design, shape, or graphic form are encouraged.
96. Creative signs that identify and accentuate building entries are encouraged.
97. To conserve energy, there should be a standard shutoff time for illuminated signs of businesses that do not operate at night.
98. For signs on awnings, the sign or logo areas should not occupy more than 30 percent of the awning panel.
99. A-frame and sidewalk signs placed in the public right-of-way are encouraged. These allow flexibility for businesses to alert shoppers to sales, specials, and other promotional information.
  - Sidewalk signs shall not impede foot traffic or limit ADA compliant access.
  - One sidewalk sign is permitted per business.
100. Signs shall not impair views into shopfront windows. This may include sidewalk signs and window-mounted signage.

## 5.2.24 PROGRAMMING

During the visioning efforts for the Westminster Mall site, the community strongly supported the reactivation of the area. Explaining how the Mall used to be “the” place to be, the community clearly voiced their desires for activities and entertainment at the Mall site such as: Farmer’s Market, food hall, youth-oriented activities, special events, playgrounds, indoor entertainment, and new nightlife activities. The success of any thriving center is not only created by the built environment; the way that people utilize the spaces and create community in these areas is also an element that should be considered for the Mall site.

### WHAT EVENTS OR PROGRAMS SHOULD TAKE PLACE AT WESTMINSTER MALL THAT WILL REINFORCE THE IDENTITY OF THE PLACE AND PROVIDE THE NEW AMENITIES AND ACTIVITIES DESIRED BY THE COMMUNITY?

The following programming ideas were identified during the visioning process. In the future, programming of events will be the responsibility of the Mall property owners, which may form a Property-Based Improvement District (PBID) to oversee and fund programming at the Mall site.

1. Plan for community events such as food festivals, outdoor concerts, and art shows focused on activating the Urban Park Plaza and the Neighborhood Park.
2. Offer a rotating activities calendar that utilizes indoor and outdoor areas of the Mall.
3. Offer weekly events such as a Farmer’s Market or other ongoing activity (possibly in coordination with Community Services Department) and develop a “signature” event over time, such as an annual festival, concert, movies in the park, performing arts exhibition or other event.
4. Don’t wait until everything is built to initiate programming of spaces and activities in the WMSP. It is important to start building the Westminster Mall site as a destination and activity center now in temporary ways and even before full build-out.



*Making spaces transform for multiple uses throughout the day and week.*



*Large multi-use courtyard provides open space for events.*



*Dining patio can be used as group style seating for event viewing.*

### 5.2.25 ENHANCED TREATMENTS

Special paving with distinctive patterns shall be used at significant intersections and at gateways into the Westminster Mall Specific Plan area. Refer to Chapter 6, *Mobility*, for locations of intersections requiring special treatments or enhancements. These treatments include the use of one of the following stamped and/or colored concrete, brick, or pavers.



*Raised planters in crosswalk for pedestrian safety and directing traffic flow*



*Change in paving and bulb-out bring driver awareness to crosswalk.*



*Alternate paving used at parking entrance slows traffic.*

### 5.2.26 CORNER TREATMENTS

Well-designed corners contribute to placemaking and corner treatments are recommended for buildings located at major public street corners, such as Bolsa Avenue and Edwards Street and Bolsa Avenue and Goldenwest Street, and at primary entrances that occur on streets or driveways leading into the Plan Area. Corner treatments are also appropriate at key intersections on private roadways internal to the Plan Area.

1. All corners of intersections, entries, or near the center of grouped buildings should feature special architectural elements. Treatments may include: towers that are visually distinct from the rest of the building massing; a corner plaza or gathering space; a lobby canopy; a public art installation; a landscape installation with columnar trees; clock towers; other prominent architectural elements.
2. The corner of the building(s) should be set back to create space that marks the corner as significant with diagonal or curved walls at the corner.



*Activating the corner with outdoor seating encourages interaction with pedestrians.*

## 5.2.27 PUBLIC ART

A public art program shall be instituted through WMSP and consist of a hierarchy of art intended to establish character and sense of arrival to the Plan Area from the I-405 corridor, Bolsa Avenue, and Edward Street. The use of art can generate a regionally recognizable landmark that assists motorists and visitors in locating this unique area, strengthen the Mall's identity and branding, beautify the Plan Area and create long-term community assets. Section 7.2.8, *Public Art and Landmarks*, provides additional guidance for design and placement of artwork in the WMSP.

Public art within the Specific Plan area shall be provided for the following:

1. New residential or commercial development having total project costs of \$300,000 or more, as determined by the City's valuation of building permits issued for the development;
2. Expansion of existing buildings or remodeling of existing buildings when any such work has a building permit valuation of \$300,000 or more.

Public art provided shall have a value equal to one-half of 1 percent (0.50 percent) of the total building costs (as measured by building permit valuations), excluding land, site development, off-site requirements, and remediation costs. The value of the public art shall include the art piece itself and the cost of installation.

Public art shall be installed by the developer of a parcel and may be installed concurrently with each building that triggers the public art requirement, or the value of the required public art for each building can be consolidated, or banked, and applied to the provision of larger installations that serve multiple buildings.

Public art requirements shall not apply to reconstruction of structures that have been damaged by fire, flood, wind, earthquake or other calamity.



*Interactive art engages users within the site.*



*Colorful art, even if temporary, can transform the scale and experience of a space to a more pedestrian level.*



## 5.2.28 PARKING STANDARDS

Providing for adequate parking is one of the most significant issues for the Specific Plan area. This section contains standards to ensure that parking throughout the WMSP area is convenient and accessible, accommodates all uses, and maintains the character envisioned for the area. See also additional parking guidelines in Section 7.2.2.

### VEHICULAR PARKING STANDARDS

To encourage new developments to include facilities and amenities that promote biking, walking and transit use, mixed-use projects in the WMSP are eligible for a parking reduction by incorporating Transportation Demand Management (TDM) strategies, pending project approval.

1. TDM strategies applicable to reduced parking requirements, subject to the approval by the City Traffic Engineer, include:
  - Car sharing (For additional Shared Parking standards see Chapter 6, and Section 9.2.5.)
  - Carpool/vanpools
  - Unbundled parking spaces are rented or sold separately, rather than automatically included with the rent or purchase price of a residential or commercial unit
  - Joint use (shared parking)
  - Transit, bicycle, and pedestrian system improvements
  - Trip reduction incentives to employees, such as free transit passes
  - Other proposals
2. A “park once” policy shall be promoted for the WMSP. Rather than driving from one use to another, visitors are highly encouraged to park once and walk to one or more destinations within the project area. Similarly, residents and employees are encouraged to walk from residences or workplaces to WMSP destinations.



*Green transportation electric vehicle parking spaces.*



*Additional screening of surface parking separate from the sidewalk.*

3. Controlled entrances to parking facilities (gates, doors, etc.) shall be located a minimum of 18 feet from the back of sidewalk to accommodate one vehicle entering the facility.
4. A parking reduction of up to 15% of the required spaces may be considered upon receipt of a parking study and/or trip reduction information. Any reductions beyond 15% will require a discretionary action, such as a variance.
5. Additional parking spaces for the outdoor dining portions of a restaurant are not required provided the outdoor dining portion does not exceed 25 percent of the restaurant gross floor area or 1,000 square feet, whichever is smaller.
6. Shared, bundled or pooled parking, off-site parking or valet parking plans within the WMSP area must be reviewed by the City Traffic Engineer.
7. The City may allow for conversion of required car parking to parking options that support alternate mode choices as travel trends change over time. Conversions of vehicular spaces could be considered for alternatives such as additional bicycle parking, spaces for Transportation Network Companies (such as Uber or Lyft) or autonomous vehicle loading areas, etc.
8. Chapter 6, *Mobility*, provides information regarding additional parking management strategies that could be applied in the Specific Plan.
9. Electric vehicle charging facilities are required and must comply with the applicable provisions of the Westminster Municipal Code.
10. Parking lot areas and carports shall not be located along street frontages facing the existing single-family neighborhoods.
11. All parking reduction requirements shall be reviewed by the City Traffic Engineer, which will determine the appropriate level of parking demand reduction generated by these strategies on a project-specific basis. Vehicular Parking shall be provided for projects within the WMSP consistent with the provisions of Chapter 17.320 of the WMC. Minimum vehicular parking for residential and non-residential uses shall be the same as required Citywide by the zoning code for each use.
12. New and reconfigured surface parking lots shall provide a tree canopy plan with a goal of 50% or greater coverage at maturity, which may be offset by the substitution or mixing of solar panels.
13. All parking areas, covered and uncovered shall be 70% screened from public street frontages. Screening may be accomplished through building placement, landscaping, a planted earth berm, planted fencing, topography, or some combination of the above.



*Permeable paving breaks up asphalt and can reduce the heat island effect.*

## PARKING STRUCTURES, GARAGES AND CARPORTS

14. Parking structure entries shall be integrated into building façade design. Design treatments and details should minimize the apparent width of the entrance in accordance with the building's predominant architectural character.
15. Any exposed structured parking floors and the ground level of parking structures must be screened and shall include architectural skins, integrated landscaping, and detailing to improve visual appearance and reduce impacts from noise and light.
16. Structured parking shall be designed with compatible materials, color, and detail as the building it is attached to (or associated with) or, if not attached, the most proximate, adjacent buildings so that it blends into the surrounding built environment.
17. Parking structures with shared entry for residential, commercial or office uses shall have parking spaces designated for non-residential, residential, or shared uses and shall be marked by the use of posting, pavement markings, or physical separation.
18. Garages and carports shall be designed to include a minimum of two of the following from the main building(s): materials, detailing, roof materials, and colors.
19. The placement of residential garages and tuck-under parking shall not face primary access streets (orientation of entryways shall be obscured and minimized from primary circulation routes internal to the project).



*Adding art or other architectural detailing can help to minimize the appearance of long, uninterrupted building faces created by parking structures.*



*Signage at the entry of a parking structure can help visitors know where the most parking spaces are available to help minimize unnecessary circling for spots.*

## BICYCLE PARKING STANDARDS

Minimum bicycle parking for residential and non-residential uses shall adhere to the standards provided in Table 5.7, *Bicycle Parking Requirements*. In addition to the bicycle parking identified above, the WMSP site supports future mobility options including scooters and bikeshare stations.

Future developers shall work with the City to identify potential locations for these micromobility locations to be consolidated for use by WMSP site users.

**Table 5.7: Bicycle Parking Requirements**

Use	Minimum Bicycle Capacity	Location
Residential	1.0 space per unit, 1 enclosed locker required for every 60 dwelling units  (Enclosed locker requirement does not apply to residential units that provide enclosed garages)	Near main entrance with good visibility, not to obstruct auto or pedestrian movement
Commercial/ Retail/ Entertainment	1.0 space per 3,000 sq. ft. of building area  If it is determined that there is a high demand for bike parking for retail uses, retail owners can consider adding a bike corral.	
Restaurant	High Turnover - 1.0 space per 250 sq. ft.  High Quality - 1.0 space per 750 sq. ft.	
Hotel or Lodging	1.0 space per 10 guest rooms	
Office	1.0 space per 3,500 sq. ft. of building area	
Medical	1.0 space per 10,000 sq. ft. of building area	
Public or Institutional Uses	1.0 space per 2,500 sq. ft. or building area	
Notes:		
1. Calculations which result in a fraction of 0.5 or higher shall be rounded up to the nearest whole number.		
2. If bicycle parking facilities that can be used by all properties on site are provided near the Navy Trail entrances, the City could consider a potential reduction in on-site bicycle parking requirements		



*Bicycle parking throughout the WMSP area can help support the use of the future Nature Activity Trail.*



*Maintenance stations at mobility hubs promote alternate travel options.*

## 5.2.29 TRANSPORTATION DEMAND MANAGEMENT (TDM) AND TRANSPORTATION MANAGEMENT ASSOCIATION (TMA) ESTABLISHMENT

Transportation demand management strategies are intended to accomplish two broad objectives.

1. Reduce reliance on automobiles and associated congestion and emissions.
2. Provide economic incentives for residential, hospitality and recreation uses in the area by allowing opportunities to reduce the number of parking spaces required for projects in a mixed-use area where shared parking can be facilitated.

### REDUCTION OF PEAK HOUR TRIPS

To reduce peak hour trips, the establishment and continuing maintenance of the Transportation Management Association (TMA) or a Transportation Management Organization (TMO) is a high implementation priority for the success of this Plan. Every property owner will be required to participate in the Transportation Demand Management (TDM) and Transportation Management Association (TMA).



*Alternate modes of transportation on and to the site promotes a walkable site and can help reduce the demand for on-site parking.*

### 5.2.30 NOISE ATTENUATION

Noise attenuation applies to any new development that includes residential or other noise-sensitive uses. The City's General Plan Noise Element identifies two future noise contour levels on the Westminster Mall site, 70 dBA CNEL (closest to the freeway) and 65 dBA CNEL (transitioning midway in the property toward the single-family residential neighborhoods in Huntington Beach).

Applicants for new noise-sensitive development (e.g., residential, hospitals, etc.) must demonstrate to the Community Development Director that all habitable rooms would meet the 45 dBA CNEL interior noise standard required by the State Title 24 before the City issues building permits. This can be accomplished with enhanced construction design or materials, such as upgraded dual-glazed windows and/or upgraded exterior wall assemblies.

To ensure exterior noise compatibility, applicants proposing projects that fall within areas located within the 70 dBA CNEL contour lines must demonstrate that the noise levels for residential outdoor common areas and recreational areas are at or below 70 dBA CNEL to ensure compatibility with the ambient noise levels. Noise reduction measures could include increased setback from the freeway, shielding with noise barriers, or placing outdoor noise-sensitive areas behind buildings. For noise attenuation purposes, outdoor common or recreation areas do not include parking and loading areas, ornamental landscaping, or walking/biking trails.



*Sensitive noise receptors such as new housing will need to take into account the proximity to the I-405 freeway.*

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# Chapter 6

# **Mobility**





## 6.1 OVERVIEW

Much of Westminster's transportation system developed 50 years ago, at a time where it was an auto-dominated culture and transportation was largely influenced by freeway expansions and an interest and desire to travel by personal vehicles. In the WMSP project area, the local transportation system has largely been implemented and the City is currently overseeing many improvements that are a result of the I-405 Freeway Improvement Project.

Demographics within the state are also changing. People are expected to live longer, which has changed the transportation needs for that population as they age. As such, this Specific Plan has a complete streets approach, providing for the needs of pedestrians, bicyclists, motorists, and transit users. Figures 6.1 through 6.3, *Conceptual Circulation Plans* and Figure 6.4, *Proposed Access Points* identify the principles behind the internal roadway hierarchy, connectivity, and vehicular access points that the City envisions when implementing the Specific Plan.

The WMSP Mobility Plan addresses all aspects of the public realm which, for the purposes of this Specific Plan, includes the public right-of-way and the area between the public right-of-way and adjacent building façade (the public realm). Design and development standards for the public realm can be found in Chapter 5, *Development and Design Standards*.

### 6.1.1 IMPLEMENTING THE GENERAL PLAN

In recent years, the City has been taking steps to support complete streets upon finalization of the City's General Plan Mobility Element and Active Transportation Plan (ATP). Mobility in the WMSP Area therefore must take a complete streets approach. The Plan identifies key facilities for users of the system and provides infrastructure for those users. This is consistent with the goals and policies in the City's General Plan Mobility Element that aim to accommodate users of all ages and abilities.

The General Plan designates Bolsa Avenue as an Arterial street and Edwards Street as a Connector street. The General Plan sets a process for prioritizing modes of travel along corridors, recognizing that some corridors should sacrifice automobile movement to make walking and biking more pleasant, and vice-a-versa on other corridors. The General Plan assigns priority travel modes on Edwards Street for bikes, pedestrian, and transit; while Bolsa Avenue has a priority for automobiles.

### STATE LAW THAT GUIDES CIRCULATION IN WMSP

**AB 1358** – This requires all substantial changes to a General Plan in the State of California to include complete streets policies that plan for all modes of travel.

**SB 743** - This directed the State Office of Planning and Research (OPR) to identify an alternative metric (other than automobile level of service (LOS)) to be used for identifying transportation impacts as part of CEQA (California Environmental Quality Act).

**SB 375** – This required Metropolitan Planning Organizations (MPOs) to look at the interface between land use and transportation; it also requires MPOs to develop strategies to reduce vehicle miles of travel (VMT), which is a precursor to greenhouse gas emissions (GhG).

**AB 32** – This was a state commitment to reduce greenhouse gas emissions (GhG) statewide with specific reduction targets identified in the state law.

During public outreach workshops held for the WMSP, the community reaffirmed a desire to minimize and slow traffic in neighborhoods adjacent to the WMSP site. Specifically, the community supported installation of traffic calming measures (either speed or volume) that would minimize WMSP Area traffic from using local neighborhood streets. Additionally, the community supported specific driveway access locations being relocated mid-block to further minimize the traffic intrusion potential. Finally, the community supported the notion of an internal circulation system to keep traffic on the WMSP site, instead of routing traffic around the site, which would further impact regional streets.

Several land use and urban design attributes identified in other sections of the WMSP are integral features that support and are integrated into the WMSP Mobility Plan. Critical items include:

1. Development of a mix of uses to capture more internal trips to the area.
2. Creation of a main street or Downtown to make the WMSP location a destination.
3. Improved biking and walking environments such that people do not have to get into their cars to make short trips, including connectivity to the future Westminster Nature Activity Trail and better accessibility for bicycles and pedestrians from neighborhoods to the WMSP site and to destinations beyond.

In addition to these key urban design attributes, the ongoing I-405 Freeway Improvement Project includes vehicle and capacity upgrades to Westminster Mall Road and the I-405 Southbound off-ramp intersection, which serves as a key gateway into the WMSP. These improvements were analyzed as a part of the General Plan to ensure that planned uses at the Mall site could be accommodated with development of the Mall property. The Specific Plan maintains access to the site via this gateway point and it was reviewed in great detail to ensure that the off-ramp intersection operates appropriately with increased demand from the site (e.g. queues will not spill back to the freeway).

## WHAT ARE COMPLETE STREETS?

Complete Streets have been defined by the National Complete Streets Coalition as, “...Streets for everyone. They are designed and operated to enable safe access for all users, including pedestrians, bicyclists, motorists and transit riders of all ages and abilities. Complete Streets make it easy to cross the street, walk to shops, and bicycle to work. They allow buses to run on time and make it safe for people to walk to and from train stations.”

*“Creating Complete Streets means transportation agencies must change their approach to community roads. By adopting a Complete Streets policy, communities direct their transportation planners and engineers to routinely design and operate the entire right-of-way to enable safe access for all users, regardless of age, ability, or mode of transportation. This means that every transportation project will make the street network better and safer for drivers, transit users, pedestrians, and bicyclists – making your town a better place to live.”*

## 6.2 WMSP MOBILITY PLAN

### 6.2.1 A COMPLETE STREETS APPROACH

In the WMSP, a complete streets approach means providing mobility for all travel modes that serves users of all ages and abilities. Given that the existing transportation network in the WMSP Area was previously focused largely on automobiles, the Specific Plan provides for additional infrastructure focused on the mobility for other users of the system, as well as the automobile. The Specific Plan provides guidance regarding the integration and development of improvements for both public and privately-owned transportation networks within the area. Providing enhanced mobility for these modes will improve accessibility to the WMSP Area, which is a key component of this plan.

The WMSP Mobility plan incorporates all aspects of the public realm, which is comprised of a vehicular zone, a Pedestrian Zone, and a Frontage Zone. All three of these zones should be implemented consistent with the information previously presented in Chapter 4, *Permitted Uses* and are critical to the pedestrians' and motorists' experience along public and internal streets. Careful attention to the design of each zone will contribute significantly to the look and feel of the WMSP. This Chapter (*Mobility*) focuses primarily on the improvements to the Vehicular Zones (roadways) and Pedestrian Zones (trails). The Frontage Zones are addressed in greater detail in Chapter 5.

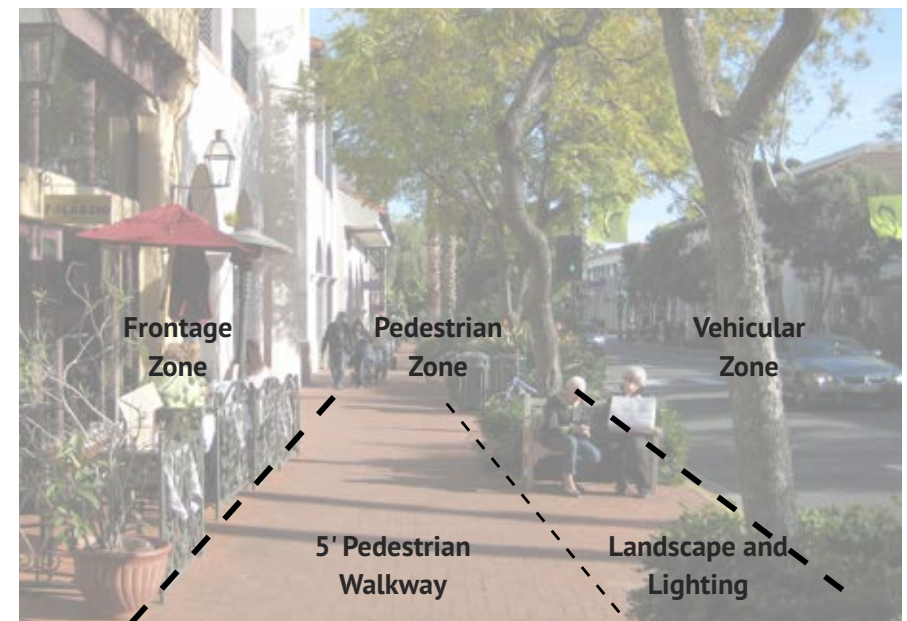
**Vehicular Zone.** The Vehicular Zone consists of vehicular travel lanes and may include bike lanes, parking lanes, and medians. Parklets may be included in the Vehicular Zone to extend the Pedestrian and Frontage Zones near specific uses (such as restaurants or key gathering areas).

**Pedestrian Zone.** The Pedestrian Zone consists of the pedestrian walkway and landscape area. A minimum 5 foot clear and unobstructed path shall be provided in this zone to accommodate pedestrian

movement. The landscape area may include trees, planters, lighting, pedestrian amenities, street signs, and other public utilities. It may also include paved walkways to access adjacent buildings.

**Frontage Zone.** The Frontage Zone is the area between the Pedestrian Zone and the adjacent building facade. This area is typically used as the primary entry to adjacent buildings and may include outdoor dining space, plaza space, private outdoor patios for residential, landscaping, and drop-off areas for medical uses. Frontage Zone standards regulate building facade placement, frontage types, and encroachments (see Chapter 5, *Development and Design Standards*).

Effectively providing services for all these zones, while accommodating users of all ages and abilities, is achieved through the application of a Complete Streets approach.

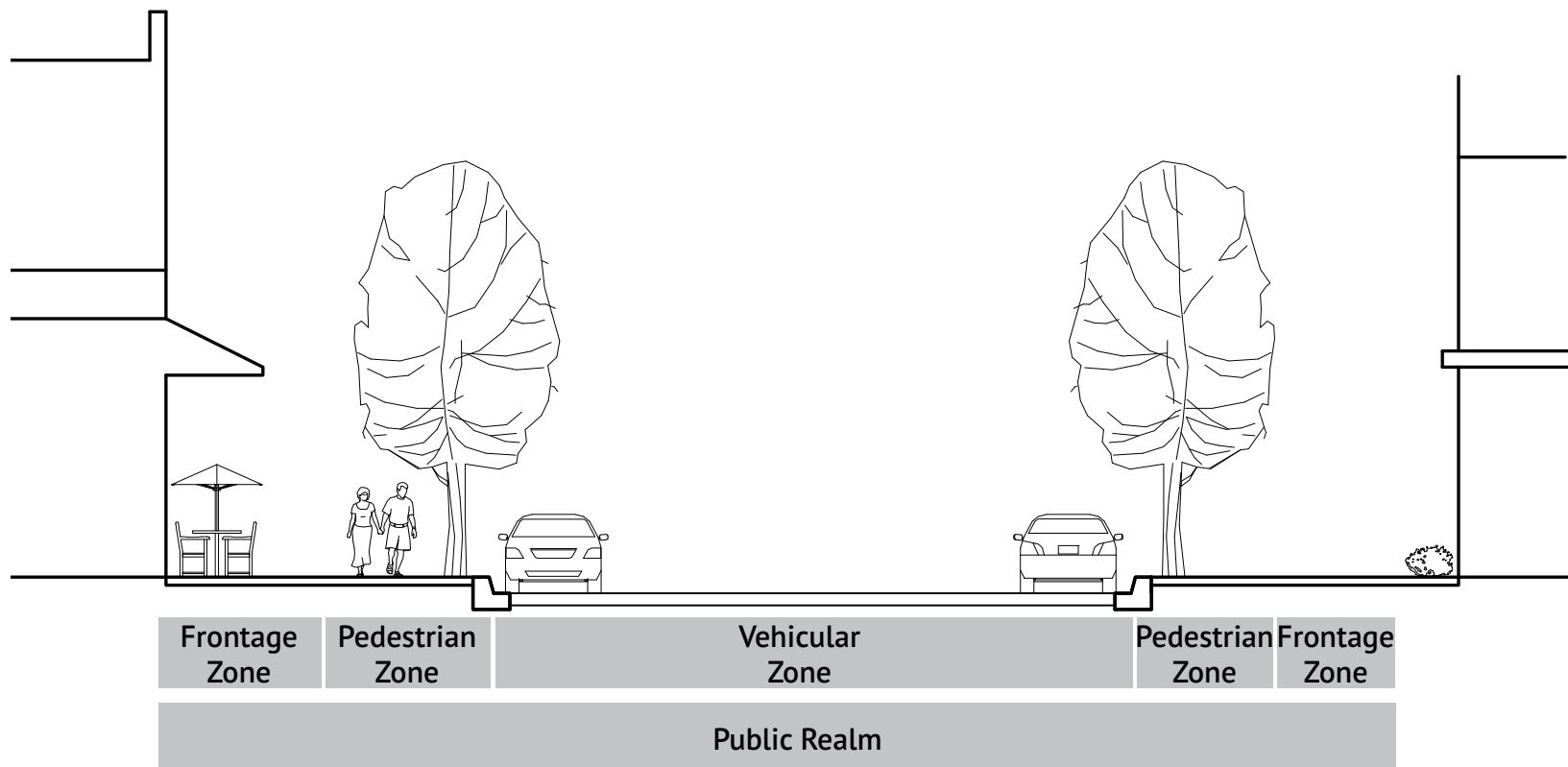


*The complete streets zones and their locations.*

## 6.2.2 MOBILITY PLAN REQUIREMENTS

The Westminster Mall Specific Plan (WMSP) includes a mobility system that focuses on a variety of key concepts, as noted below:

1. Any vehicular traffic generated from future development at the WMSP Area must be able to be accommodated within the existing street rights-of-way and roadway capacity on Bolsa Avenue and Edwards Street. These roadways will not be widened with additional vehicular lanes to accommodate future development. Refinements to pedestrian improvements or bike lanes are acceptable.
2. A connected internal circulation system that provides accessibility to and within the site without exiting onto the regional roadway network.
3. Minimize traffic intrusion into adjacent neighborhoods through the effective design and placement of vehicular access points and use of traffic calming devices such as traffic diverters to prohibit through movements into the neighborhoods.
4. Creation of an internal Main Street or Downtown area.
5. Connectivity to future bicycle networks in the area, including the Westminster Nature Activity Trail and other key corridors in the City.
6. Consistency with mobility goals outlined in the City's General Plan Mobility Element.



## 6.3 VEHICULAR CIRCULATION AND ACCESS

The WMSP provides for flexibility in the alignment of future vehicle circulation and access facilities to provide opportunities for future implementation of development consistent with market demands. The Specific Plan does not propose to specify the exact alignment of the interior roadways. Instead it allows flexibility in the design, phasing and construction process to be coordinated between the site's multiple property owners as each project is proposed. However, there are some key elements this Specific Plan requires of future development, as noted below.

1. The internal circulation system shall be designed to provide connectivity within the site such that vehicles do not need to exit the site onto Bolsa Avenue or Edwards Street to gain access to another part of the site.
2. Internal circulation on the site will be multi-modal and should prioritize pedestrian orientation where feasible.
3. Internal roadways shall be designed to provide complete streets to accommodate users of all ages and abilities.
4. Internal roadways will be maintained by the property owners of the WMSP.
5. Gateway access into the project shall be designed to minimize traffic cut-through or intrusion into adjacent neighborhoods.
6. Internal streets shall be designed to provide adequate queuing distances from intersections and gateways along Bolsa Avenue and Edwards Street.

Figures 6.1 through 6.3, *Circulation Concept Plans* and 6.4, *Potential Vehicular Access Locations*, illustrate various ways to implement the intent of the elements listed above related to the internal circulation system. Further, Figures 6.11-6.17 identify potential access points

that will be implemented along with the traffic calming signalized intersection improvements proposed for Bolsa Avenue and Edwards Street. Additionally, Figures 6.7 and 6.8 illustrate the street cross-sections that should be implemented for roadways internal to the WMSP site.

As it is anticipated that the internal roadway system will be developed in phases as the property owners submit applications for review, the ultimate configuration of all internal streets is not yet known. However, the internal roadway network shall be developed, at a minimum, with a roadway hierarchy that is consistent with the following:

### PUBLIC RIGHTS-OF-WAY

External to the project site, Bolsa Avenue, Edwards Street and the I-405 Freeway off-ramp provide the primary access into the project site (illustrated in Figures 6.5 and 6.6). These roadways are dedicated rights-of-way that are regulated and maintained by the City of Westminster (in some instances under a joint agreement with Huntington Beach, since Bolsa Avenue and Edwards Street are located along jurisdictional boundaries).

### PRIMARY INTERNAL CIRCULATION STREET

A primary internal circulation roadway shall provide internal connectivity to make it more convenient to travel from one side of the Specific Plan Area to the other within the project site, rather than exit the site onto Edwards Street or Bolsa Avenue. Illustrated in Figure 6.5, this street configuration prioritizes vehicular circulation on the site without being interrupted by parking aisles or dead ends and places a secondary priority on accommodation of bicycles and pedestrians. The street shall be designed to align with primary gateways into the area. Future developments will need to be coordinated and phased around the Primary Internal Circulation Street to ensure that utilities and other infrastructure are adequately provided and phased into future roadway construction.

## SECONDARY CIRCULATION

A second tier of the internal roadway network shall consist of two other types of roadways that are intended to connect into the Primary Internal Circulation street to key destinations. These streets consist of an Internal Main Street concept and Residential Streets.

- » **Internal Main Street.** Creates a walkable “Downtown” feel in strategic locations inside the WMSP (see Figure 6.8, *Options 1 and 2*). The width of this street section is narrower than the Primary Internal Circulation street and is intended to be more of a “slow go” street for vehicles while prioritizing slower speeds and pedestrians. It is anticipated that in these areas there will be parking on one or both sides of the streets, or in the median. Parking in this area is intended to experience a higher turnover than parking provided elsewhere on the site.
- » **Residential Streets.** Illustrated in Figure 6.9, *Options 1 and 2*, *Residential Streets* are also internal to the project but are intended to provide access to the new residential developments proposed in the project area. They are not intended to provide significant capacity to circulate vehicular traffic around the site but are intended to provide the main access to new residential development integrated into the site.

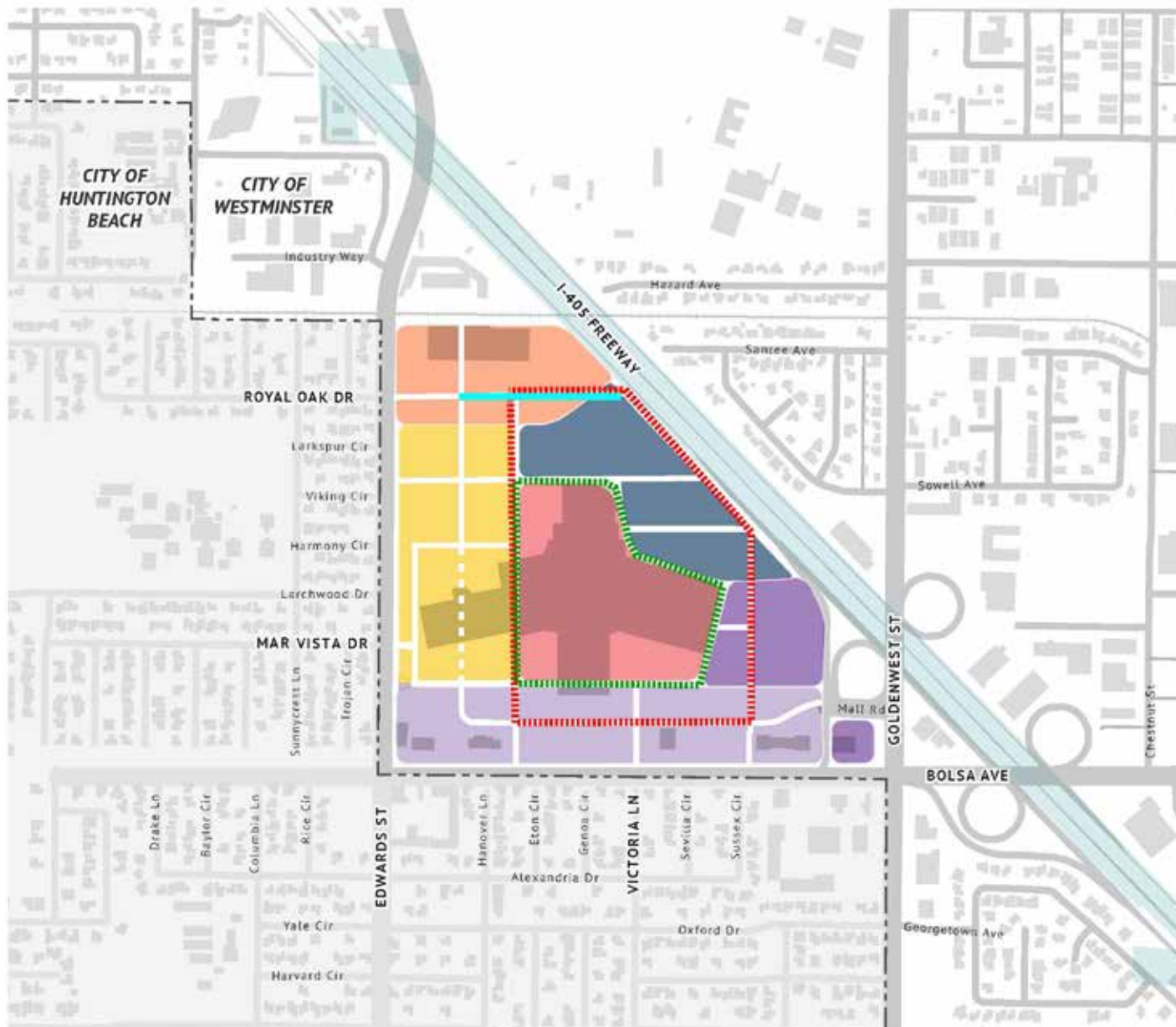
With the development of the internal network and other proposed intersection modifications, property owners will be responsible for evaluating and maintaining safe circulation. This will include verification of adequate corner site distance at each intersection and driveway to provide drivers from the side street with a clear line of sight to view approaching vehicles. Adequate sight distance aids in reducing collisions and, as such, landscaping elements should also provide adequate sight distance at these locations.



*The photo above represents the implementation of Figure 6.8, Internal Main Street Section (Option 2). Parking going both directions is provided in a center median, which can also serve as a gathering space for Farmers Markets or other temporary outdoor events to help create activity in the project area.*



**Figure 6.1: Internal Circulation Concept (Alt 1)**



*Note: Existing Mall footprints shown for reference. Underlying uses may remain, conceptual land use areas on this figure show one option of desired grouping of future uses on the site and are conceptual only.*

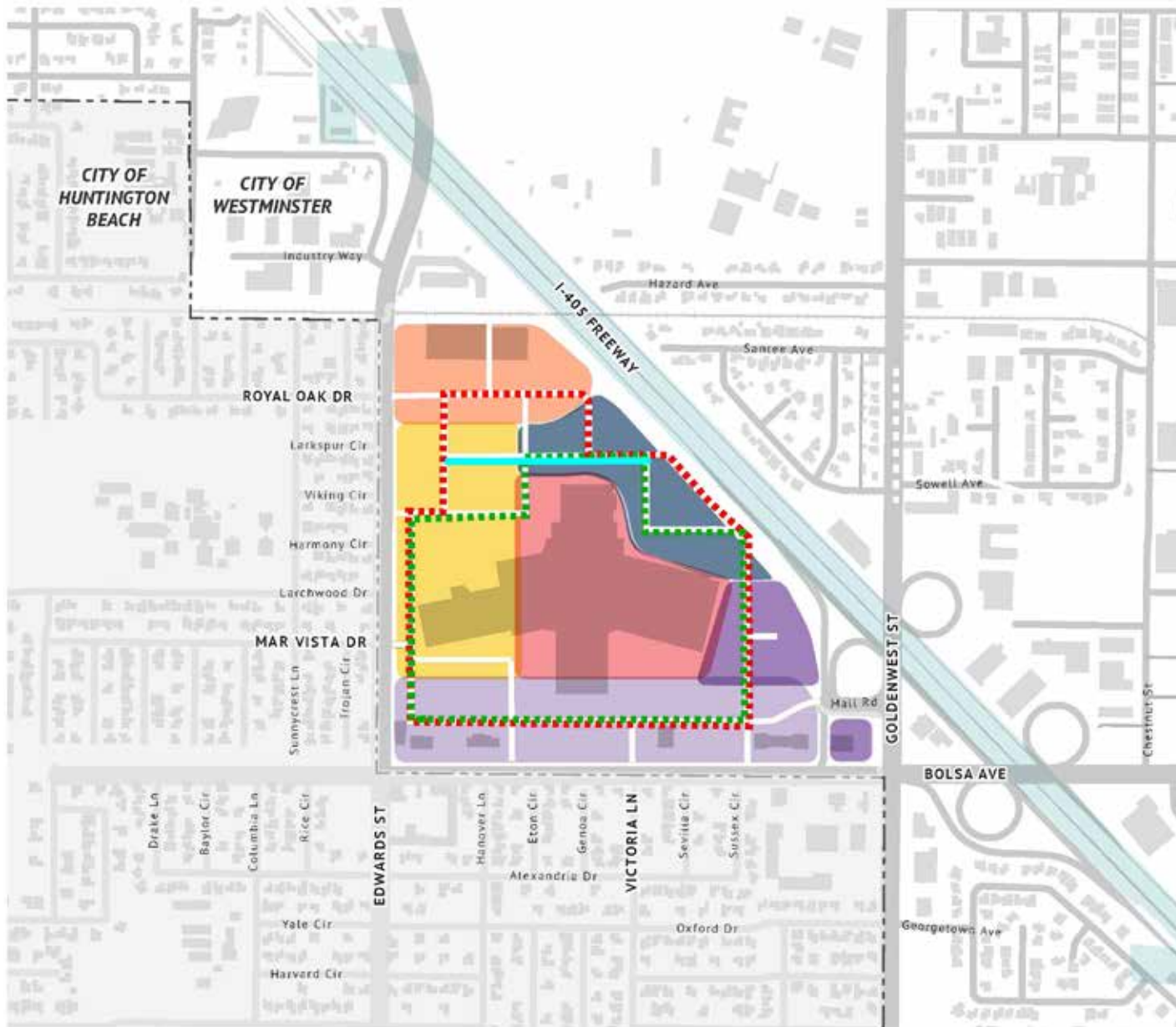
*Primary Internal Circulation could be allowed to have one lane in each direction based on approval from the City Traffic Engineer, if volumes do not warrant two lanes.*

- ..... Primary Internal Circulation
- ..... Secondary Internal Circulation
- Internal Main Street



Source: Fehr & Peers

**Figure 6.2: Internal Circulation Concept (Alt 2)**



*Note: Existing Mall footprints shown for reference. Underlying uses may remain, conceptual land use areas on this figure show one option of desired grouping of future uses on the site and are conceptual only.*

*Primary Internal Circulation could be allowed to have one lane in each direction based on approval from the City Traffic Engineer, if volumes do not warrant two lanes.*

- ⋯⋯⋯ Primary Internal Circulation
- ⋯⋯⋯ Secondary Internal Circulation
- Internal Main Street



Source: Fehr & Peers

**Figure 6.3: Internal Circulation Concept (Alt 3)**



*Note: Existing Mall footprints shown for reference. Underlying uses may remain, conceptual land use areas on this figure show one option of desired grouping of future uses on the site and are conceptual only.*

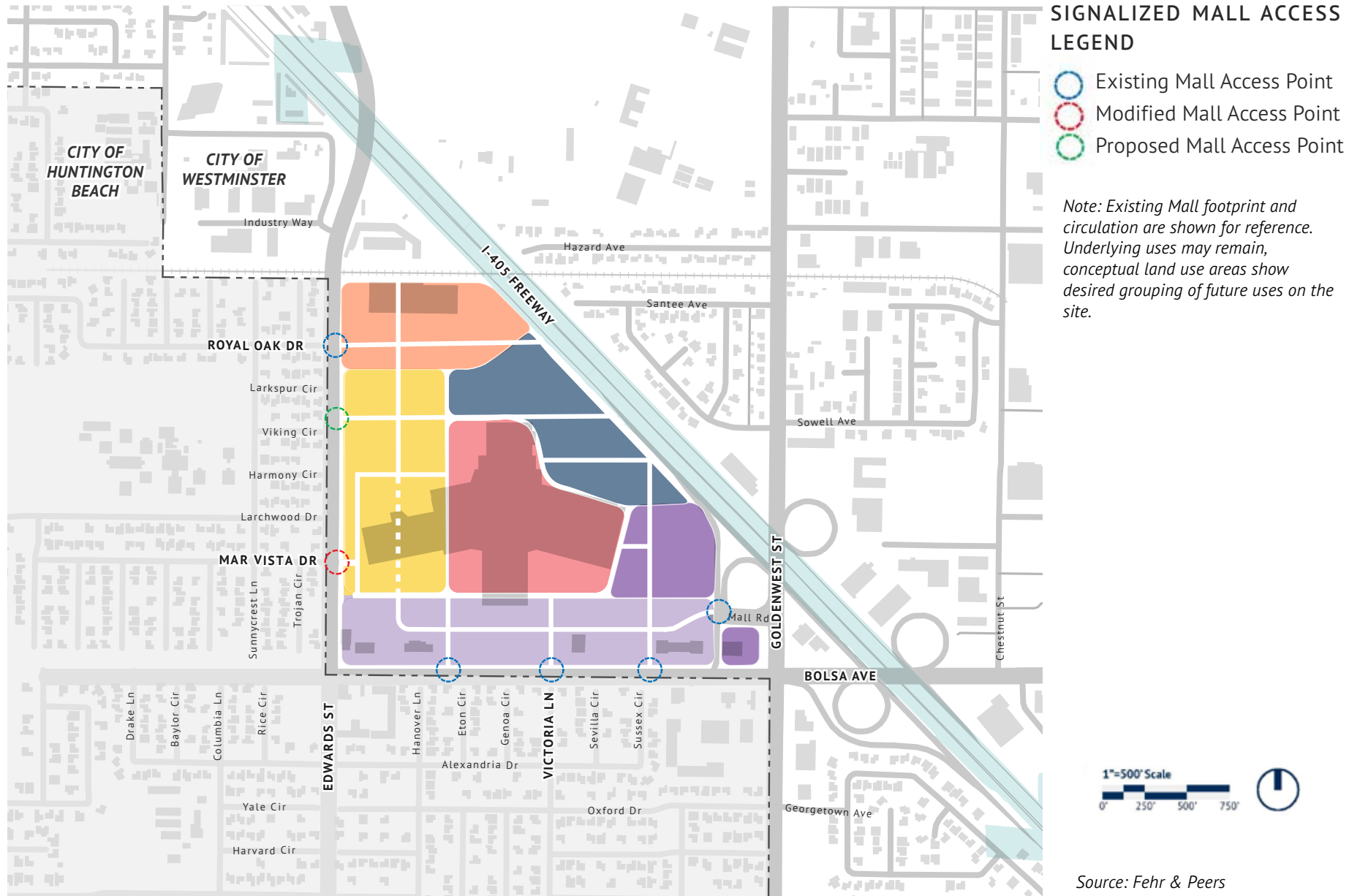
*Primary Internal Circulation could be allowed to have one lane in each direction based on approval from the City Traffic Engineer, if volumes do not warrant two lanes.*

- - - - - Primary Internal Circulation
- - - - - Secondary Internal Circulation
- Internal Main Street



Source: Fehr & Peers

**Figure 6.4: Potential Vehicle Access Locations**



## 6.4 STREET CLASSIFICATIONS AND STREET SECTIONS

The following street cross-sections have been developed using a complete streets approach and respond to community feedback the City received during public workshops held in preparation of the Specific Plan. They are consistent with the City's General Plan Mobility Element, which classifies Bolsa Avenue as an Arterial, prioritizing automobiles and transit along the corridor, and classifies Edwards Street as a Connector Street, prioritizing bicycles, pedestrians, and automobiles along that corridor.

The street sections provided for the WMSP project area have been developed and are responsive to context sensitive opportunities and constraints (existing right-of-way widths, drainage easements, internal circulation hierarchy, etc.). The street classifications in the Mobility Element were used as a starting point for the WMSP circulation network, but the sections have been refined and customized based on local context. Therefore, the Mobility Element still serves as a guide, but this section provides the more detailed direction to implement improvements and more specifically provides guidance for the design of internal private roadways and circulation features, such as bikeways or paseos (referenced in more detail in Chapter 5). All internal streets should be designed to provide adequate queuing distance from intersections and gateways along Bolsa and Edwards.

The following roadway sections have been developed for the WMSP:

Public Roadways:

1. Bolsa Avenue
2. Edwards Street

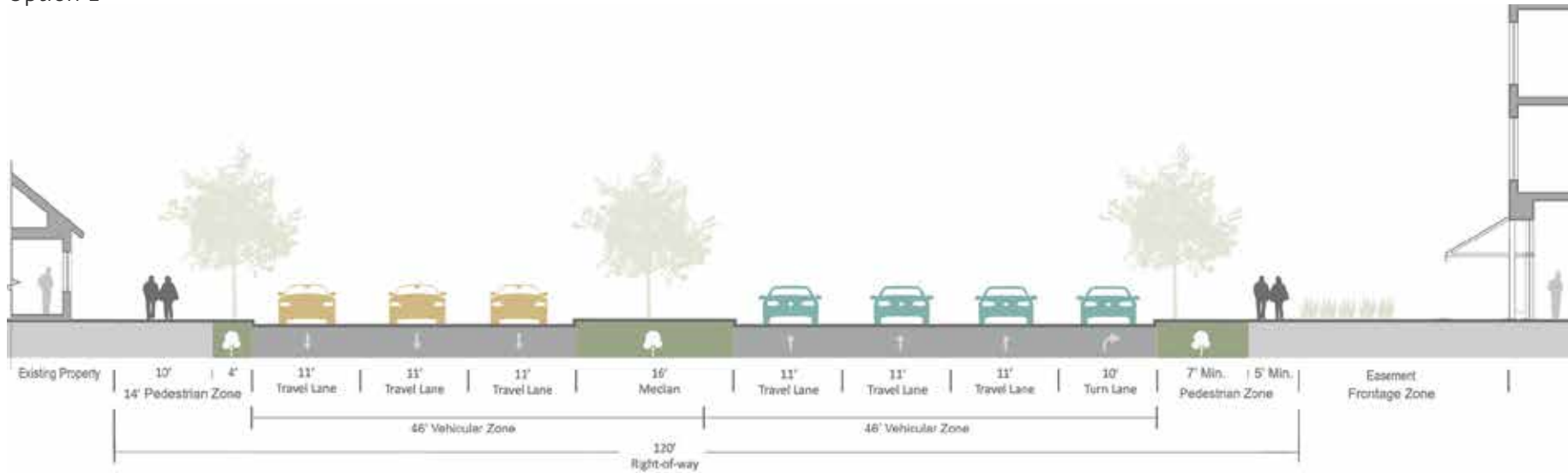
Private Roadways (Internal to the Project):

3. Primary Internal Circulation Street (site-wide; vehicular priority)
4. Internal Main Street (smaller scale; slower traffic, short-term on-street parking)
5. Residential Streets

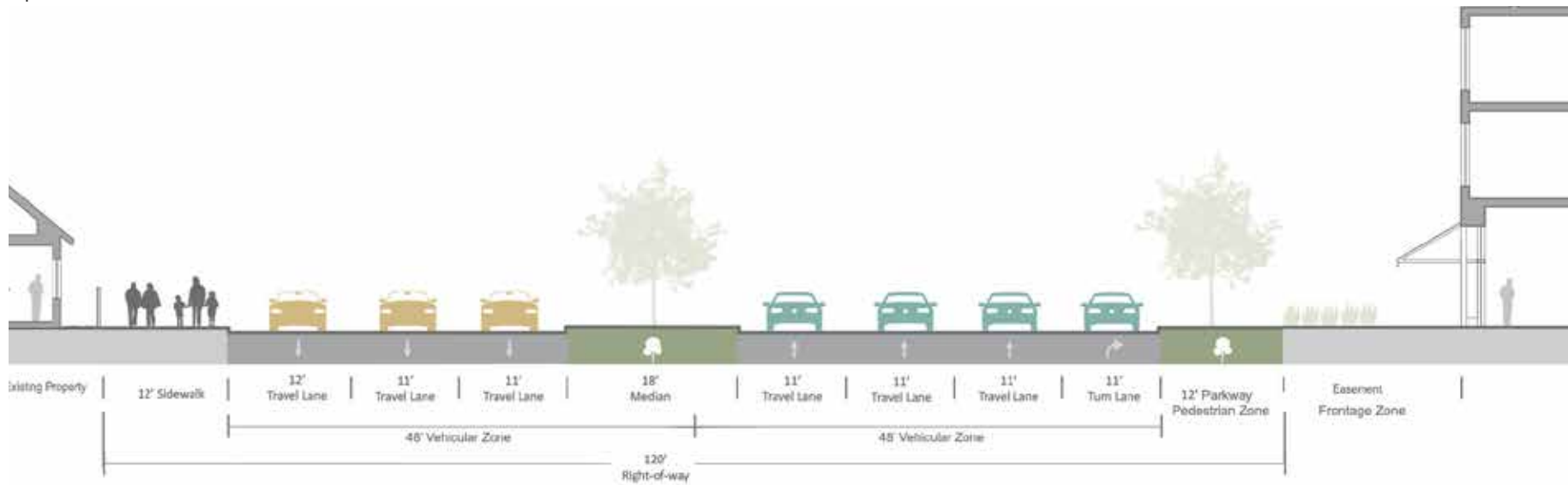


# Figure 6.5: Bolsa Street Sections

Option 1

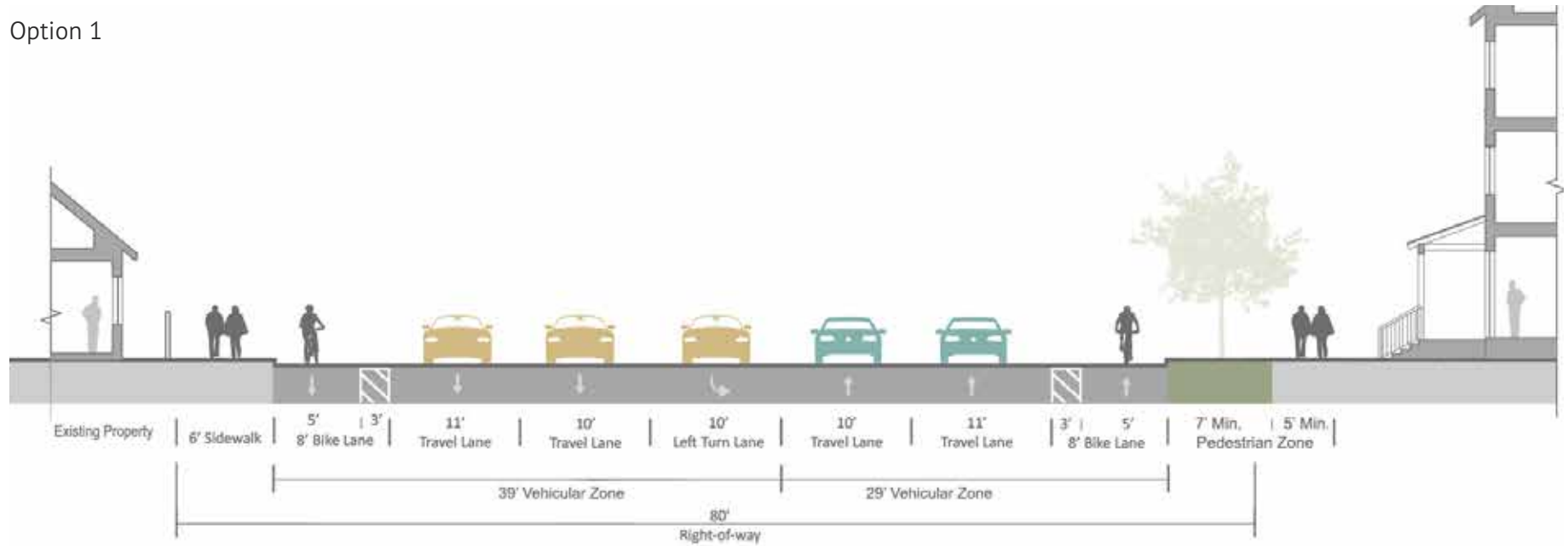


Option 2

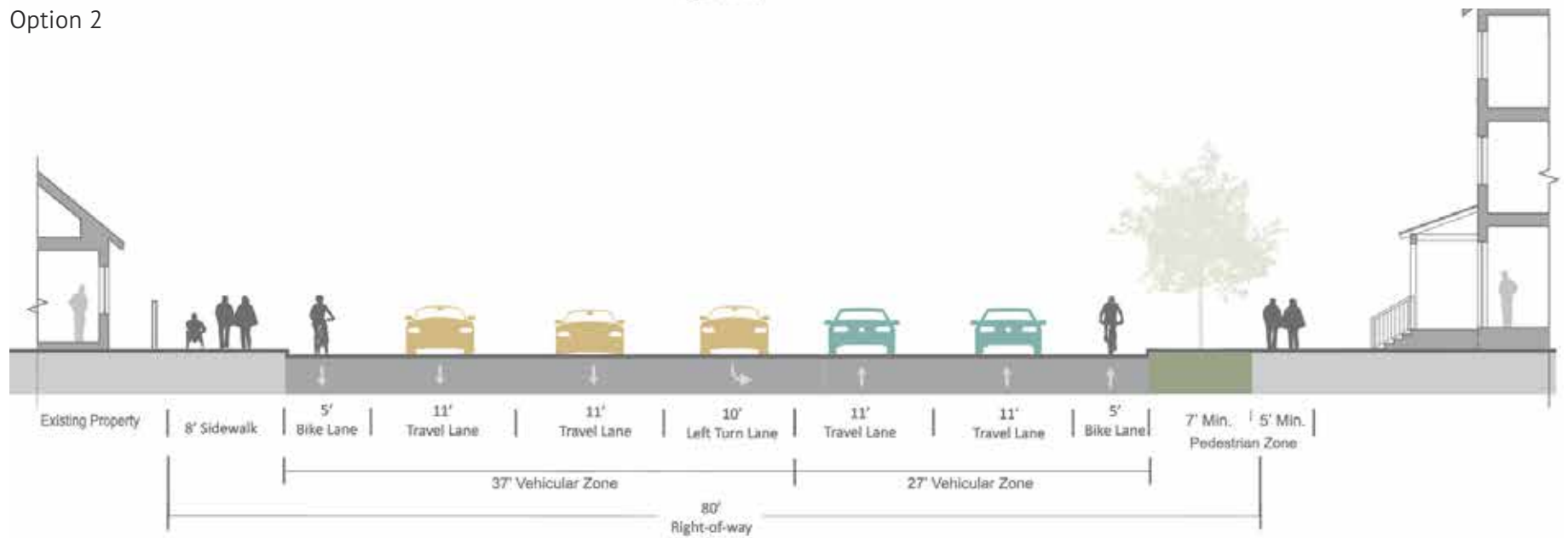


# Figure 6.6: Edwards Street Sections

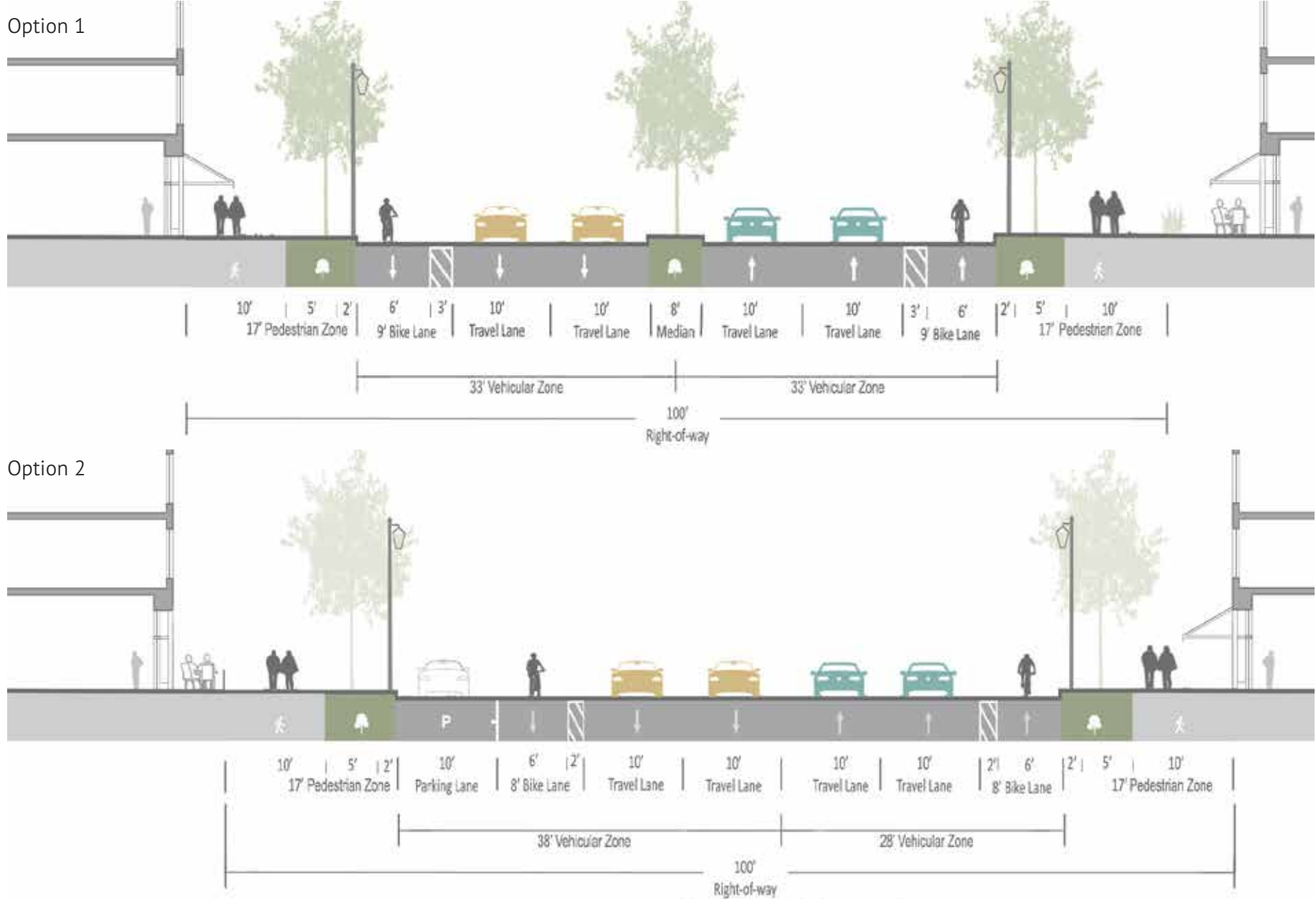
Option 1



Option 2



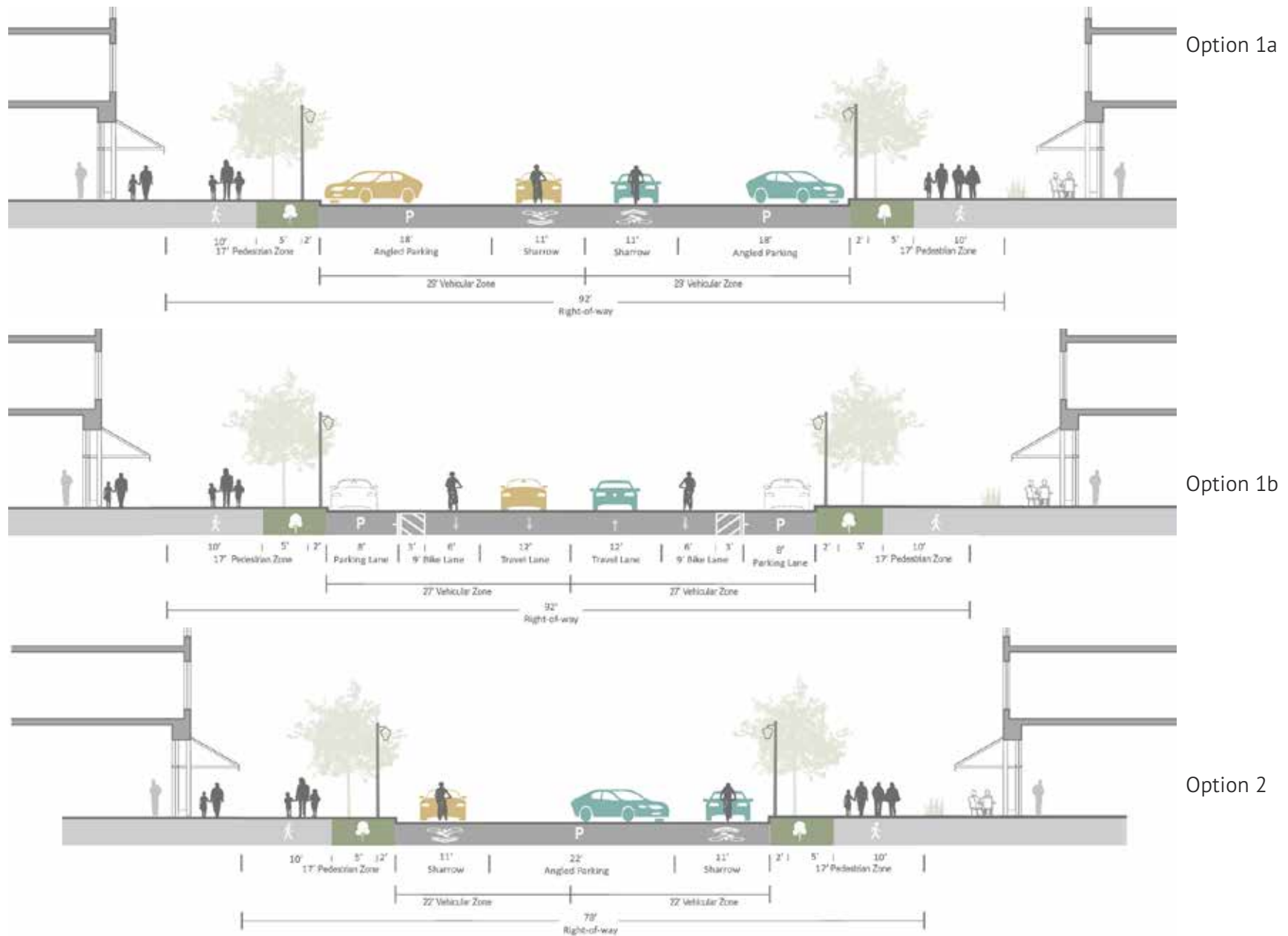
## Figure 6.7: Primary Internal Circulation Street Sections



Note: One lane each direction could be allowed based on approval from the City Traffic Engineer, if volumes do not warrant two lanes. Alternative street cross-sections and right of way dimensions may be administratively approved without a Specific Plan Amendment on a case-by-case basis with approval from the City Traffic Engineer.



**Figure 6.8: Internal Main Street Sections**



Note: Alternative street cross-sections and right of way dimensions may be administratively approved without a Specific Plan Amendment on a case-by-case basis with approval from the City Traffic Engineer.

**Figure 6.9: Residential Street Sections**



*Note: Alternative residential cross-sections could be approved on a case-by-case basis with approval from the City Traffic Engineer.*

## 6.5 TRAFFIC CALMING

### 6.5.1 REQUIRED IMPROVEMENTS

In addition to the streets described above, the WMSP also envisions implementation of traffic calming measures as part of the project. These measures can include either traffic volume or traffic speed controls. Key measures to minimize traffic intrusion into adjacent neighborhoods include traffic diverters or speed control measures. Although specific treatments will need to be designed concurrent with development and under the review of a licensed engineer, conceptual treatments were developed for several key intersections surrounding the project site, as shown on Figures 6.11 through 6.17.

Each of these key intersections includes ADA improvements, such as the addition of high-visibility crosswalks, upgraded curb ramps, and traffic signal modifications that include the implementation of an Intelligent Transportation Systems (ITS) communications system. Traffic calming improvements for some of the key intersections are listed in Table 6.1. It should be noted that the development of these treatments may be subject to approval by the City of Huntington Beach.

### IMPLEMENTING IMPROVEMENTS

Traffic calming improvements were identified based on conversations with City staff and treatments outlined in the traffic study, as well as input received during the public outreach process. There are several ways to deliver the identified improvements. One is to develop a fee program or funding mechanism that would pay for all of the identified improvements up front or fund them as fees are collected.

Another way to implement improvements is to incrementally implement them as development occurs. One key component to incremental implementation is determining when these

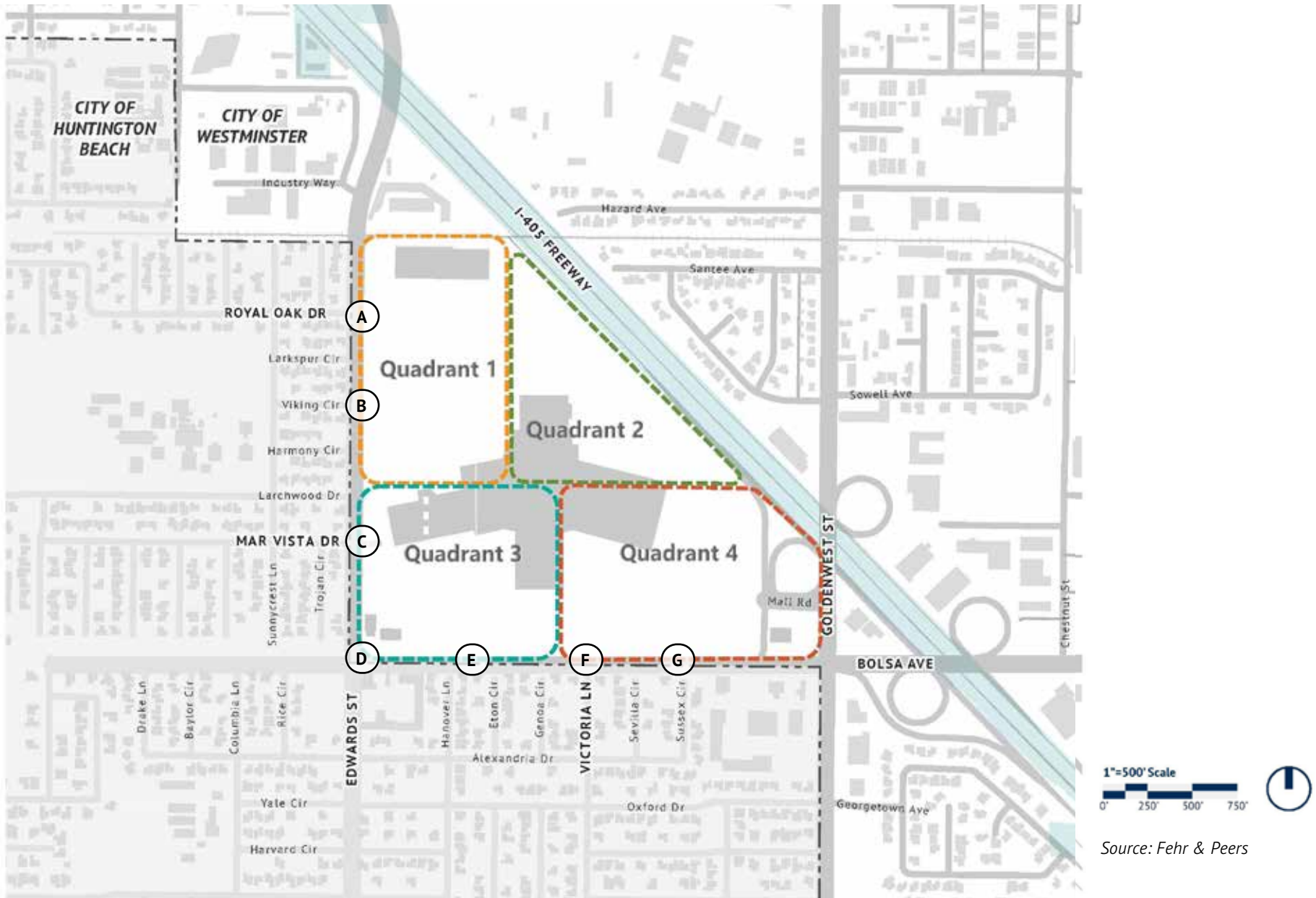
measures should be implemented and ensuring that measures are shared equitably across the entire Specific Plan Area. To assist in this equitable implementation, it is recommended that the project site be divided into quarters, as shown on Figure 6.10, *Traffic Calming Implementation Quadrants*. When development occurs in each quadrant, that development should implement the traffic calming measures identified in Table 6.1.

To assist with assigning implementation responsibility for these traffic calming improvements, the WMSP Area was divided into quadrants and development in each quadrant will be responsible for implementing the referenced traffic calming improvements.

These identified improvements are subject to City modification to ensure equitable implementation as the site develops.



**Figure 6.10: Traffic Calming Implementation Quadrants**



Source: Fehr & Peers

**Table 6.1: Conceptual Treatments For Key Intersections**

Intersection	Conceptual Treatment	Reasoning
<b>Quadrant 1</b>		
Royal Oak Drive/Edwards Street (A)	<ol style="list-style-type: none"> <li>1. Installation of a traffic diverter that will discourage traffic from intruding into the neighborhood as shown on Figure 6.11.</li> <li>2. Upgrade intersection to be compliant with current ADA standards.</li> <li>3. Modify traffic signal to include improved communications system for intelligent transportation systems (ITS) implementation.</li> </ol>	Quadrant 1 will add the most traffic to this intersection and therefore should be responsible for implementation.
<b>Quadrant 2</b>		
New Driveway Between Mar Vista Drive and Royal Oak Drive (B)	<ol style="list-style-type: none"> <li>1. New full access traffic signal as shown on Figure 6.12.</li> <li>2. Ensure new signal includes communications system for ITS implementation.</li> </ol>	Quadrant 2 disperses traffic to all access points. However, these improvements have been allocated to this quadrant to ensure equitable distribution of costs for improvements.
Mall Road/I-405 Ramp	<ol style="list-style-type: none"> <li>1. Upgrade intersection to be compliant with current ADA standards.</li> <li>2. Modify traffic signal to include improved communications system for ITS implementation.</li> </ol>	
<b>Quadrant 3</b>		
Mar Vista Drive/Edwards Street (C)	<ol style="list-style-type: none"> <li>1. Installation of a traffic diverter that will discourage traffic from intruding into the neighborhood or removal of the Mall approach to this intersection.</li> <li>2. Modify traffic signal to include improved communications system for ITS implementation or removal of the Mall approach to this intersection.</li> <li>3. Upgrade intersection to be compliant with current ADA standards as shown on Figure 6.13.</li> </ol>	Quadrant 3 adds the most traffic to these locations and therefore should be responsible for implementation.
Bolsa Avenue/Edwards Street (D)	<ol style="list-style-type: none"> <li>1. Modify traffic signal to include improved communications system for ITS implementation.</li> <li>2. Upgrade intersection to be compliant with current ADA standards as shown on Figure 6.14.</li> </ol>	
Bolsa Avenue/West Drive (E)	<ol style="list-style-type: none"> <li>1. Modify traffic signal to include improved communications system for ITS implementation.</li> <li>2. Upgrade intersection to be compliant with current ADA standards as shown on Figure 6.15.</li> </ol>	
<b>Quadrant 4</b>		
Bolsa Avenue/Victoria Lane (F)	<ol style="list-style-type: none"> <li>1. Installation of a traffic diverter that will discourage traffic from intruding into the neighborhood as shown on Figure 6.16.</li> <li>2. Modify traffic signal to include improved communications system for ITS implementation.</li> <li>3. Upgrade intersection to be compliant with current ADA standards.</li> </ol>	Quadrant 4 adds the most traffic to these locations and therefore should be responsible for implementation.
Bolsa Avenue/East Drive (G)	<ol style="list-style-type: none"> <li>1. Modify traffic signal to include improved communications system for ITS implementation.</li> <li>2. Upgrade intersection to be compliant with current ADA standards as shown on Figure 6.17.</li> </ol>	

**Figure 6.11: Traffic Calming Improvements – Royal Oak Drive/Edwards Street (A)**



**Figure 6.12: Traffic Calming Improvements – Edwards Street/ Proposed Driveway (B)**



**Figure 6.13: Traffic Calming Improvements – Edwards Street/ Mar Vista Drive (C)**



**Figure 6.14: Traffic Calming Improvements – Bolsa Avenue/ Edwards Street (D)**



**Figure 6.15: Traffic Calming Improvements – Bolsa Avenue/ West Drive (E)**



**Figure 6.16: Traffic Calming Improvements – Bolsa Avenue/ Victoria Lane (F)**



**Figure 6.17: Traffic Calming Improvements – Bolsa Avenue/ East Drive (G)**



## NEIGHBORHOOD TRAFFIC CALMING PROGRAM

In addition to the traffic calming and accessibility measures recommended in the previous section, this Specific Plan also recommends that establishment of a neighborhood traffic calming program be established by the City of Westminster and funded by the developer(s). This program would utilize the TIRE Index (Traffic Intrusion on Residential Index) to identify each development's potential to increase traffic in adjacent neighborhoods and would guide recommendations for traffic calming on those neighborhood streets. The City will evaluate the TIRE index before completion of any development within the area to establish whether the development triggers the implementation of any additional traffic calming improvements into the adjacent neighborhoods. Please note that, as an alternative, the City could create a site-wide fee that the developers would be required to contribute towards developing traffic calming features in the adjacent neighborhoods.

Since the adjacent neighborhoods are in the City of Huntington Beach, the City of Westminster will work collaboratively with that City and its neighborhood residents to install traffic calming measures.



*Bulb-out at crosswalks with high-visibility continental striping improves pedestrian safety by limiting exposure to vehicles.*



# 6.6 BICYCLE AND PEDESTRIAN CIRCULATION AND ACCESS

## 6.6.1 BICYCLE FACILITIES

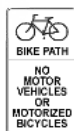
Bicycle facilities consist of Class I, Class II, Class III, and Class IV facilities as described in the following sections:

**Class I Bikeway (Bike Path)** - Provides a separated corridor that is not served by streets and highways and is away from the influence of parallel streets. Class I bikeways are for non-vehicle use only with opportunities for direct access and recreational benefits, right-of-way for the exclusive use of bicycles and pedestrians, and cross flow conflicts are minimized. There are currently no existing Class I bicycle facilities near the project site. However, the City is planning for a future Class I bicycle facility along the Navy Trail (proposed Westminster Nature Activity Trail) that would utilize the existing rail corridor to connect Seal Beach into Westminster and ultimately connect to the City’s planned Class IV bikeway along Hoover Street. Some of the Westminster Nature Activity Trail’s improvements are contained in the WMSP boundaries and include internal connectivity directly to the trail. Additionally, the City plans to provide a Class I bikeway from the current terminus of Hoover Street south (under I-405) to McFadden Avenue and provide a connection to Golden West College. This will ultimately



Graded Shoulders Recommended

**CLASS I - Multi-Use Path**  
Provides a completely separated right-of-way for exclusive use of bicycles and pedestrians with crossflow minimized.

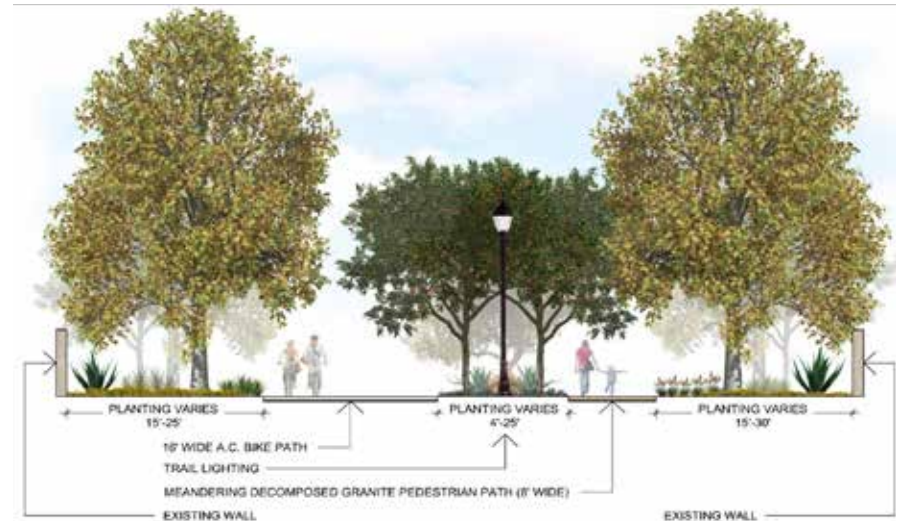


MUTCD R44A (CA)

### EXISTING



### PROPOSED



The graphic above represents a conceptual illustration of potential Class I Bikeway improvements along the proposed Westminster Nature Activity Trail (Former Navy Rail Corridor).



Mendez Historic Trail and Green Street Bikeway (Hoover Street, Phase 2)



Garden Grove Boulevard Complete Street (Garden Grove Boulevard Segment)

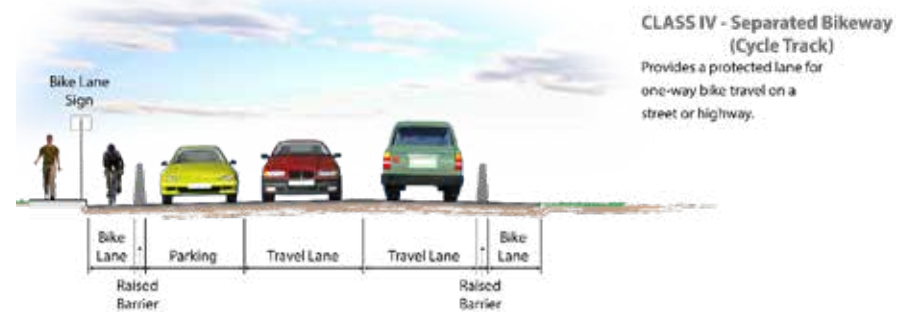
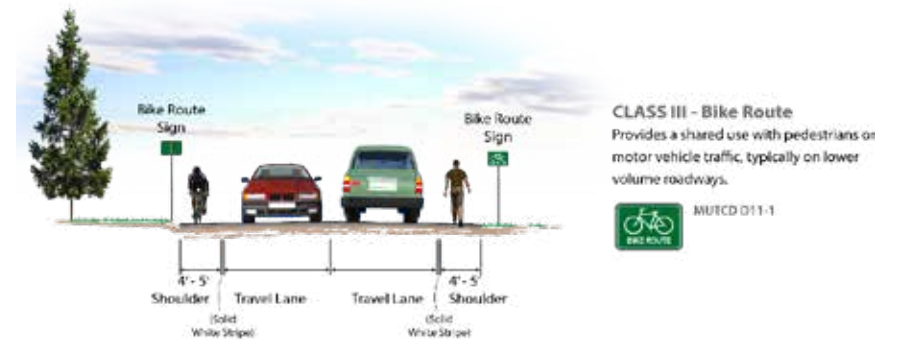
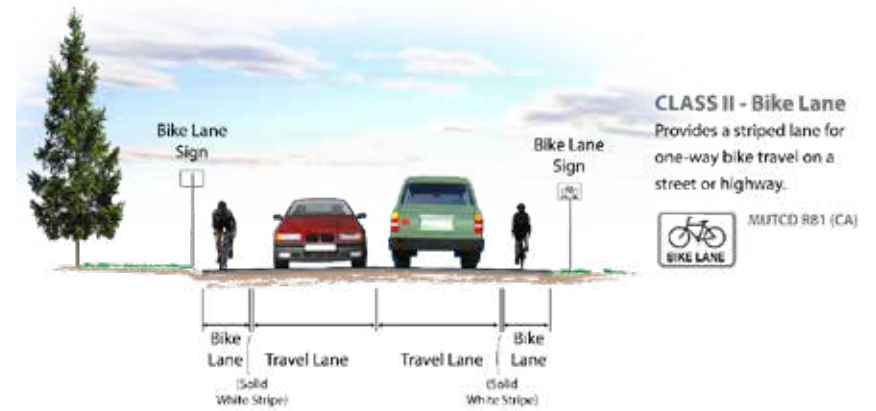
TOP and BOTTOM photos, respectively:  
 The grant-funded Mendez Historic Freedom Trail and Green Street Bikeway (Hoover Street, Phase 2), and Garden Grove Boulevard Complete Street (Garden Grove Boulevard Segment) projects are other Class IV bikeway improvements recently approved in Westminster that can serve as examples of the type of bike infrastructure desired along the Westminster Nature Activity Trail on the northern boundary of the WMSP.

provide accessibility for bicycles from the WMSP site to Golden West College to be accommodated completely through Class I and Class IV bicycle facilities.

**Class II Bikeway (Bike Lanes)** - Provides a delineated right-of-way assigned to bicyclists to enable more predictable movements, and accommodate bicyclists through corridors. Existing Class II bikeways exist in the study area along Edwards Street.

**Class III Bikeway (Bike Route)** – Provides a shared facility (shared by bikes and vehicles) that serves either continuity to other bicycle facilities or designates preferred routes through high demand corridors. Class III facilities are denoted through signage or through installation of sharrows along the roadway. Currently, there are no designated Class III facilities near the project site.

**Class IV Separated Bikeway (Cycle Track)** - Provides delineated right-of-way assigned to bicyclists that have a physical separation between them and a vehicle. This separation can include parked vehicles, bollards, curbs, or any other physical device that provides this separation. This “new” bicycle classification was defined in AB 1193, which amended the streets and highways code to allow for this treatment, and directed Caltrans to update Chapter 1000 of the Highway Design Manual to include this facility. Class IV bikeways are not planned in the WMSP project area but are planned along Hoover Street east of the project site, which will be accessible in the future via the proposed Westminster Nature Activity Trail.



## 6.6.2 INTERNAL PEDESTRIAN AND BICYCLE CONNECTIVITY

In addition to bicycle facilities, pedestrian facilities must be accommodated to provide accessibility to the future projects on the WMSP site. Pedestrian facilities include sidewalks and mixed-use paths providing this accessibility. As noted above, it is important to consider the Pedestrian Zone in the assemblage of a complete street to ensure that the facility is designed to accommodate the adjacent land uses at the site (e.g. uses with high pedestrian activity, such as a main street, should have wider pedestrian facilities and those should complement the Frontage Zone to develop a complete street that is context sensitive).

Figure 6.18, *Bikeway and Pedestrian Improvements* identifies existing and proposed pedestrian and bikeway linkages that are required for the project. New development must demonstrate that it provides internal connectivity to the parks and open spaces within the WMSP and a linkage to the Westminster Nature Activity Trail.

However, the WMSP envisions improved connectivity within the site; particularly along the Primary Internal Circulation street and on future internal roadway connections within the site. Additionally, future development within the site shall consider pedestrian access to both the Westminster Nature Activity Trail, internal community areas, and destinations within the site to ensure safe and convenient access to these areas.

### THE RELATIONSHIP BETWEEN BLOCK LENGTHS AND CIRCULATION

To further enhance pedestrian accessibility within the project site, it is recommended that future development integrate use of shorter block lengths (e.g. internal blocks of 400-600 feet in length, consistent with Chapter 5, *Development and Design Standards*) in future design concepts to increase intersection density and improve walkability. Shorter block lengths allow for more direct pedestrian accessibility, pedestrian oriented environments (with buildings oriented toward the street) and reduced out of direction travel for pedestrians.



Figure 6.18: Bikeway and Pedestrian Improvements



## 6.7 TRANSIT

In the study area, OCTA currently operates Route 64, providing access to the WMSP site from the Huntington Beach area along Bolsa Avenue (Bolsa-Victoria stop) and Edwards Street (Edwards-Mar Vista). The Specific Plan does not propose any changes to the existing transit routes provided by OCTA. However, when a concentration of new uses occurs in the WMSP Area, the WMSP site could benefit from the implementation of a “circulator” which is a privately funded shuttle system that would provide accessibility to the study area.

Circulators are generally funded by local business districts (and sometimes hotels) or homeowner associations to provide accessibility to key destinations with frequent service in the area. If a circulator were proposed as new development occurs, it is recommended that it link areas such as WMSP, local schools (such as Golden West College), and other key destinations in the area. Additionally, the WMSP recognizes that transportation is changing and that future innovative mobility options (such as microtransit) will be considered. Improving transit accessibility can also help to reduce local trips and could assist in reducing the amount of parking needed in the study area.

Although the WMSP plan accommodates and supports this potential circulator, it will need to be locally funded and, as such, the details related to frequency, vehicle type and routing will need to be developed by those implementing the circulator.

Bus stops currently exist along Edwards Street and Bolsa Avenue. One stop is located within the WMSP project site (close to the intersection of Edwards Street and Mar Vista Drive). Since OCTA manages the bus system in Orange County, property owners in the WMSP Area shall coordinate with OCTA to improve service to the site and stops at the project site.



*The Orange County Transportation Authority (OCTA) has launched an on-demand microtransit program to help better match public transit services with how commuters want to travel. The one-year pilot, called OC Flex, will allow riders to take an eight-person shuttle between two zones – one in Huntington Beach and the northern City of Westminster and the other in three cities: Mission Viejo, Aliso Viejo, and Laguna Niguel.*

## 6.8 PARKING

### 6.8.1 STANDARD PARKING

Currently, the WMSP site includes 5,818 parking spaces provided in surface parking lots. Rarely are these spaces completely utilized during the day under existing conditions; however, these spaces can be highly used during holiday seasons when the mall is busy. This context is important to understand and monitor as long as the site is overparked. In that instance, the available excess spaces can be utilized to park new development. Alternatively, underutilized spaces can be removed to facilitate new development on the site.

It is envisioned that parking associated with future development within WMSP will be provided by a combination of private surface parking lots, future parking structures, and on-street parking spaces. The City of Westminster's Municipal Code shall guide required parking for land uses in the WMSP Area, unless the property owners submit consideration for reduced parking standards that are approved by the City Traffic Engineer. For mixed-use sites, shared parking strategies (described below) shall guide parking requirements in the study area.

The WMSP plan does recognize that fully autonomous vehicles may be available in the foreseeable future. Autonomous vehicles, especially if they are provided through a subscription-based service, will decrease the ultimate demand for parking in the study area. As such, any parking structure that is developed in the WMSP Area should provide basic amenities to reuse some of the building area when parking demand is reduced. This would include providing basic electricity, sewer and water within the parking structure.

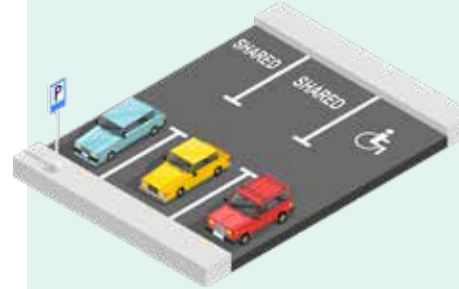
Any modifications from the City's standard parking requirements are allowable under this Specific Plan with completion of a parking study. The City shall select the consultant from its on-call list of qualified consultants.

### THE DIFFERENCE BETWEEN SHARED PARKING and OCCUPANCY PARKING STUDIES

**Shared Parking Studies** evaluate the use of a parking space to serve two or more individual land uses without conflict or encroachment.

The following conditions encourage shared parking:

- » Variations in the accumulation of vehicles by hour, by day or by season at the individual land uses
- » Relationships among the land uses that result in visiting multiple uses on the same auto trip



**Occupancy Parking Studies** evaluate the existing parking demands at the site.

**Shared parking studies** evaluate how a parking facility will function from a land use perspective, whereas **occupancy parking studies** focus more on how an existing parking facility is currently functioning.

## 6.8.2 SHARED PARKING

This Specific Plan recognizes that different uses peak in their parking demands at different times during the day as residents, employees and customers utilize retail establishments within the WMSP Area as outlined in Section 9.2.5, *Shared Parking and Parking Management*.

Shared parking identifies the temporal demand (e.g. the parking utilization by time of day) and assumes that spaces can be shared by multiple users and can occur and be implemented in different parts of the Specific Plan. Using this technique, mixed-use development will be allowed to reduce their parking requirements and ensure that the sites are not over-parked.

Shared parking tests of potential development within the Specific Plan Area indicated that a shared parking reduction (potentially up to 15%) can be achieved based on maximum assumed buildout of the plan. Actual parking activity should be reviewed as developments are implemented and the parking requirements should be reviewed to make sure that parking is “right-sized” on the site. If residential uses implement permit or restricted parking, then those spaces cannot be included in the shared parking pool utilized in the analysis.

This Specific Plan requires that:

1. New development will provide parking supply consistent with the Westminster Municipal Code unless property owners can demonstrate that reduced standards are feasible and obtain approval by the City Traffic Engineer to vary from the standards proposed in the WMC.
2. Requests for shared parking and/or requests for reduced parking ratios under the Specific Plan or any other provision of law, shall be reviewed in accordance with the provisions of Section 9.2.5, *Shared Parking and Parking Management*, of the WMSP, and, as a component of the project application, shall require a shared parking agreement signed by the owner or owners of land on which parking would expressly or foreseeably be utilized as determined by the City Traffic Engineer.

3. If a property owner or developer wants to request a shared parking credit for mixed-use development, they can complete a shared parking assessment and submit it to the City Traffic Engineer for potential reductions.
4. If a property owner or developer wants to utilize available underutilized parking at the Mall site, they can conduct a weekday and weekend parking survey to identify underutilized parking to satisfy their needs; provided that those spaces are within an 800 foot radius from the proposed development.

## RESERVED PARKING

Property owners may reserve parking spaces for specific users, such as employees, residents or customers. A space reserved for specific users cannot be used by other parkers and cannot be shared or included in the shared parking inventory.

## PARKING MANAGEMENT DISTRICTS

As an alternative to a shared parking approach, property owners may form a parking management district to provide and manage the on-site parking, under this Specific Plan. Parking management districts can be formed and organized to meet a variety of needs and services, but the ultimate goal of the district would be to allow the application of other parking management techniques to be utilized, such as in-lieu parking fees or unbundled parking. Any parking management district created shall monitor parking demand consistency throughout the year, adjust strategies to appropriately manage the district, include the City as a stakeholder in the management district and provide information to the City regularly to verify that parking on the site is appropriately maintained and implemented.

*Parking management features that help track the availability of parking spaces can make traffic flow more efficient, minimize unnecessary trips and reduce conflicts between pedestrians and vehicles within parking structures.*





### 6.8.3 PARKING MANAGEMENT STRATEGIES FOR THE SPECIFIC PLAN AREA

The WMSP also includes recommendations for the City to consider in managing parking effectively and facilitating a walkable “park-once” environment. Parking needs to accommodate numerous users. Priority, however, should be given to future residents, visitors and patrons who come to the area to live, shop, dine and recreate.

The recommendations consider and leverage the density and diversity of land uses in the WMSP. They prioritize and value short term visitor and patron access as a key element to make the Specific Plan Area vibrant and economically vital. Recommendations are for the short term (1 to 2 years), medium term (2-5 years), and long term (over 5 years).

#### SHORT TERM (1-2 YEARS)

**Adopt a Parking Occupancy Target.** The parking occupancy target should be set to emphasize efficient use of resources and ensure parking availability for employees, residents, and visitors to the Specific Plan Area. The parking supply should be managed to achieve approximately 85 percent utilization to optimize usage of this public resource.

**Monitor Parking Supply.** Parking management requires a commitment and ongoing monitoring, and evaluation. Parking utilization surveys should be conducted periodically by the City Traffic Engineer or the City’s consultant to evaluate utilization and ensure that parking availability is maintained and funded by the property owners.

**Promote Shared Parking for New Developments.** Parking districts are particularly well-suited to locations with multiple property owners in a compact, pedestrian-oriented setting such as the WMSP site. The property owners should consider locating uses with different peak parking characteristics in close proximity to each other to maximize shared parking potential (such as locating office space near entertainment space or office space near residential).

**Promote Unbundled Parking in New Developments.** Unbundled parking is the practice of selling or leasing parking spaces separately, rather than automatically including the parking spaces with the purchase or lease of the commercial or residential use. Unbundled parking manages parking demand by having residents only pay for the parking spaces they actually need. This is an efficient and fair practice, since occupants are not forced to pay for parking they do not need and can adjust their parking needs over time.

**Signage.** Clear signage shall be provided that is compliant with the California Manual on Uniform Traffic Control Devices (MUTCD) and identifies parking areas and structures. Real-time parking information should be required for any new parking structures in the Specific Plan Area. Access to parking structures should be designed as a pleasant walking experience with wide and well-lit sidewalks and pedestrian-scaled wayfinding signage.

#### MEDIUM TERM (2-5 YEARS)

**Implement Time Limits.** Projects in the WMSP should manage parking to promote high turnover at the most desirable locations, especially curbside spaces along the internal Main Street area. Time limits increase turnover and convenience to users, promoting economic vitality. Time limits would also discourage long-term parking of employees in the most desirable areas with the best accessibility to businesses. Time limits should be standardized to the extent feasible throughout the WMSP Area.

**Promote Valet Parking Services and Designate Passenger Loading Areas.** Technologies like ride-share services and microtransit help reduce parking demand. Such services, however, increase demand for pick-up and drop-off areas. The WMSP Area will experience development of restaurant and entertainment uses, among others, that are heavily utilized by ride-hailing services, which will likely result in a reduction in parking demand and increased demand for passenger loading areas.

Valet services would maximize parking use associated with restaurant and entertainment uses and are a strategy to control the demand for parking.

**Develop a Parking Management Plan.** If events are anticipated to occur at the site, the event coordinator shall develop a parking management plan and submit it to the City Traffic Engineer for approval prior to the event. The management plan will provide strategies to reduce parking demand for the event or accommodate parking demand on or off-site during the event. Section 6.8.5, *Special Event Parking*, contains additional requirements related to event parking.

### LONG TERM (5+ YEARS)

**Update Parking Supply Ratios.** Parking requirements may be reduced for future developments if parking occupancy surveys confirm lower utilization levels. The property owners should work with the City Traffic Engineer on evaluating existing parking supply and developing new parking supply ratios.

**Promote Transportation Demand Management (TDM).** Land use intensification and improvement of transit services will improve conditions to make TDM measures effective. Property owners can reduce parking demand by providing residents and workers in the Specific Plan Area with transit benefits, access to car-share vehicles, bike parking, and other TDM strategies that would curb demand for parking. This would decrease parking demand, allowing for a reduction in parking ratios and greater increase in density in the Specific Plan Area.

**Consider Development of a Parking Management District.** As discussed above, the district could manage and implement parking solutions throughout the site.

## 6.8.4 PARKING INTRUSION INTO ADJACENT NEIGHBORHOODS

During the public outreach workshops, residents identified concerns that, in the past, they had issues with visitors to Westminster Mall parking in their neighborhoods, especially during peak Mall use times, such as the holidays when parking is in high demand. As development occurs in the WMSP over time, the City Traffic Engineer, with the assistance of a consultant funded by the property owners, will monitor parking spillover into the neighborhoods.

If the monitoring by the City's consultant, who is funded by the property owners, demonstrates parking spillover is impacting the neighborhoods, the City Traffic Engineer can require the development of a parking management plan, such as a neighborhood parking permit program that will be paid for by development in the WMSP. Since the neighborhoods are located in Huntington Beach (outside of the City of Westminster's jurisdiction), it may be necessary to work with the City of Huntington Beach to form a parking management district or some type of joint powers to effectively manage parking if the need arises.

## 6.8.5 SPECIAL EVENT PARKING

Special events, such as Farmers Markets and concerts, may be held at the WMSP. A special event parking plan should be developed to provide adequate parking solutions and prevent event attendees from parking in adjacent neighborhoods. The City Traffic Engineer can require that the property owners hosting the event conduct a parking study to identify the potential parking impact the event has on adjacent neighborhoods and to demonstrate they will implement parking solutions that reduce parking congestion within the site.

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# Chapter 7

## **Design Guidelines**





## 7.1 INTRODUCTION

This chapter contains guidelines describing additional requirements that the City asks architects and developers to satisfy. Guidelines must be addressed for all development in the WMSP. Terms are defined in the Action Expectations callout box.

This chapter is broken into two parts – community and site design and building design. Community and site design provide guidelines for open space, edges, public art, and access. Additionally, this chapter provides a set of guidelines to facilitate quality building design. Considerations include massing, facades and frontages, architectural details, and corner treatments. These guidelines articulate principles that result in multi-modal streets and vibrant places for people to visit, shop, and live that reinforce the vision for the area as a pedestrian-friendly, economic, and residential hub.

Walkable, mixed-use neighborhoods encourage shoppers, residents, and employees to walk between retailers and other uses and drive less. The implementation of sustainable design practices provides additional environmental and quality of life benefits. Therefore, sustainable design principals are integrated into the guidelines and principals and should be followed to the greatest extent possible. The City will coordinate all development proposals with the Police Department and the Orange County Fire Authority to ensure that proposed projects incorporate Crime Prevention through Environmental Design principles in design and layout of the project to reduce criminal opportunity and calls for service.

### Action Expectations

The guidelines in this Specific Plan define action expectations to successfully implement the plan. The following terms clarify the level of commitment intended and reflect the expectation or desired outcome. The application of a term to a particular policy action is a deliberate application of its definition.

*Should.* This type of policy will be followed in most cases, and exceptions or degrees of implementation are acceptable with valid reasons. Similar action words: may.

*Allow.* Permits someone else's initiative and supports it unless there is a very good reason not to. Similar action words: permit.

## 7.2 COMMUNITY AND SITE DESIGN

New development should contribute to the character of the built environment and be a “good civic neighbor” providing public open space and paths through the site that respond outwards to the community, rather than creating an inward facing development.

### 7.2.1 SITE ACCESS

#### SITE ACCESS

Site access will be strongly influenced by its connection to the Primary Internal Circulation Street (Chapter 6, *Mobility*). New projects should be designed with internal circulation that encourages walking and bicycling between properties and complementary uses, while providing convenient access for residents and visitors who will arrive by car. In some cases, site access may be designed specifically for pedestrians and bicyclists so they do not have to interact with vehicles, for example, along a paseo or a promenade.



*Larger parkways create greater separation between pedestrians and the vehicular zone.*



*Varying materials help delineate passive spaces from walkways.*

1. Vehicular access to each site should be designed to minimize conflicts between pedestrians, cyclists, automobiles, and service vehicles. Sight lines, pedestrian walkways, and lighting are factors to consider in developing a site plan.
2. The on-site pedestrian circulation system should be directly connected to off-site pedestrian access, linking nearby neighborhoods, commercial projects, and open space.
3. New streets, paseos, and walkways should connect to other similar paths and provide access to open spaces where public access is already allowed or planned.
4. The number of site access points for vehicles should be minimized, and shall be consistent with the provisions identified in Chapter 6, *Mobility*. Curb cuts should be located and scaled to minimize pedestrian and vehicular conflicts, and reduce impacts to traffic flow on primary streets.
5. Internal site access points should be located as far from intersections as reasonable to allow adequate stacking room.



6. Crosswalks should be designed to heighten driver awareness (raised, special paving, color, and/or texture) in addition to the placement of warning signage.
7. Drop-off and pick-up zones should be located along the curb or within parking facilities to promote sidewalk/street wall continuity and reduce conflicts with pedestrians. Hotel lobby and medical facilities drop-off areas shall be located within the project site.



*Parking cutouts along streets slow traffic and allow for pick-up and drop-off zones.*



*Parking structure with an integrated building facade and formal entry creates interest and uniformity with surrounding buildings.*

## 7.2.2 PARKING

New development in the Plan Area should follow a park-once-and-walk strategy, where parking, site design, and pedestrian connectivity make it easy to make multiple stops without having to get back into a car. Parking should be easy to find, but its presence should not dominate the landscape and should not diminish the quality of the pedestrian environment and the community's character. Well-designed parking lots and structures can accommodate the needs of vehicular traffic while minimizing any negative effects on the human experience.

1. Vehicle parking should be distributed such that all commercial properties are within walking distance of parking.
2. Short-term parking may be provided on internal streets, given street design, to promote traffic calming and provide convenient short-term parking opportunities for ground floor retail (see the Internal Main Street section in Chapter 6, *Mobility*).
3. Parking locations should be clearly signed and easy for visitors to understand and identify where parking is available.
4. New parking should include electric charging stations for electric automobiles.
5. Convenient and safe pedestrian connections should be provided within structured and surface parking. These connections should link into the site's pedestrian network.
6. Parking should be separated from pedestrian sidewalks and pathways with landscaping and other design elements.
7. Accessible, secure and well-signed bicycle parking should be provided at convenient and visible locations throughout the development.

## SURFACE PARKING

8. Surface parking lots on corner sites should not be placed adjacent to the street edge.
9. Large projects should break up surface parking areas into a series of smaller connected areas to create visual interest and reduce “heat island” effects.
10. Parking areas should be separated from a building with both a raised pedestrian sidewalk and a landscape strip.
11. Pervious or open grid paving should be used whenever possible to reduce the negative effects of stormwater runoff and to facilitate groundwater recharge.

## PARKING STRUCTURES

12. In mixed-use developments, shared entrances for both retail and residential uses are permitted.
13. Parking structures should incorporate usage technology to assist visitors and minimize time spent searching for available parking.

## RESIDENTIAL PARKING

14. Residential products that are smaller in scale (townhome, fourplex, etc.) should have rows of garages accessible by alleys and hidden from view.

## 7.2.3 BUILDING PLACEMENT AND ORIENTATION

Building placement and orientation affect the way people experience a developed site. Placement and orientation also impact the building’s relationship with local conditions such as view, wind, and sun movements.

1. Buildings should be oriented to the street and sidewalk, encourage activity at ground level, provide entrances from the sidewalk, and provide views into other portions of the site.
2. Buildings should be positioned to enhance the sense of arrival and place.
3. Buildings should be grouped to create a functional and visual relationship between them, and clustered when feasible to maximize use of surface/ground level areas for other uses such as parks, plazas and paseos.
4. Buildings should be oriented for energy efficiency – to capture daylighting, minimize heat gain, take advantage of prevailing breezes for natural ventilation.
5. Building placement and massing should limit shading on adjacent rooftop open spaces.



*Buildings with front doors facing the street and sidewalk encourage activity.*

## 7.2.4 EDGES

1. Access points to the WMSP should be accented by monument signage, ornamental landscaping, public art, landscaped medians, or enhanced paving (colored or textured).
2. Perimeter landscaping along Bolsa Avenue and Edwards Street should permit views into the Plan Area, allowing visibility of buildings, signs, or key open space features.
3. Perimeter landscaping adjacent to the I-405 should be used to screen commercial and residential uses on the site from freeway noise and promote a healthy environment.

## 7.2.5 LANDSCAPING

Landscaping creates enjoyable places for meeting, and creates visual consistency throughout the project. Parkway planting, parking lot planting, internal roadway planting, and components of the site's overall landscape plan are addressed in Chapter 5, *Development and Design Standards*.

### LANDSCAPING

1. The selected plant species, design and placement of landscaping should provide for natural surveillance of pedestrian areas and should avoid the creation of hiding places.
2. Landscape elements, including architectural materials and plants, should contribute to the distinct character of an open space. These should provide a rich and coordinated pallet of scale, texture, and color.
3. Vines and potted plants should be used to provide texture and color on facades, to break up the massing of blank walls, and to accentuate entries, plazas, and paseos.

4. Landscaping located directly below the eaves or at a rain gutter outlet should be sturdy and be able to tolerate heavy sheet flow and periodic saturation.
5. Seasonal shading from trees and shrubs on southern and western facades should be used when developing planting schemes for courtyards and required setback areas. Deciduous trees will provide solar control during summer and winter while providing color, flowers, and other desired effects.
6. Lawn areas should be planted to serve a functional purpose. Sod should be used for turf installation. Drought tolerant grasses should be used for lawn areas.

### Landscape Definitions

**Hardscape:** Hardscape is the solid, hard elements in landscape design. Some examples of hardscape are decks, pools, berms, patios, and arbors.

**Softscape:** Softscape is the living, animated part of a landscape. It doesn't have that long-term or permanent quality. It includes soil, plants, shrubs, trees, flowers, vegetables, vines, and turf.

## 7.2.6 OPEN SPACE

### PUBLIC AND PRIVATE COMMON OPEN SPACE

Open spaces should be integrated into the overall design and architectural character of the project. They should be thoughtfully planned and not be a result of “left over” areas between buildings. Buildings clustered together should coordinate their public open space with one another to provide spaces that are centrally located and serve multiple uses. Open spaces shall include a visual focal feature or overall aesthetic in design that coordinates buildings, signs, landscaping, and outdoor furniture, public art, and amenities to create a pleasant pedestrian environment.

1. Public outdoor spaces should be part of an interconnected pedestrian and bicycle system throughout the development and should be connected to adjacent land uses and multi-modal paths.
2. Paseos and pedestrian walkways should be provided between buildings, public open spaces, and parking areas and should be visually emphasized through the use of landscaping, lighting, distinctive paving, and open structures, such as trellises.
3. Public open space shall be oriented to maximize the visual and physical link from public sidewalks and pedestrian corridors.
4. Open spaces are encouraged to include an element of surprise – such as unusual surfaces, forms, or elements.
5. Public open spaces should be surrounded by attractively designed buildings and landscape elements, as well as uses that promote street-level activity.
6. Public open space may be configured as a paseo, courtyard, plaza, or forecourt.
7. Open spaces should provide both shaded and sunlit areas during different times of the day. Shade is provided to reduce heat island effects and promote human comfort. Shade can be provided by trees, shading structures, trellises, awnings, canopies, or umbrellas integrated into the building or above open spaces.
8. Open spaces should be designed for day and nighttime use and include sufficient lighting which will be dependent upon surrounding uses and the location within the site.



*Utilizing different landscape material creates spaces with individual characteristics specific to the site.*

9. Wherever possible in residential and non-residential uses, permeable surfaces, such as paver blocks, lattice blocks, or grasscrete should be used. The use of pervious paving materials encourages pedestrian traffic and facilitates groundwater recharge.
10. Light colored paving should be incorporated near buildings to reduce the amount of heat radiating onto buildings and people.
11. Paving treatments should be used to provide clear identification of pedestrian access points and walkways, entry drives, prominent intersections, and pedestrian crossings.
  - » Appropriate treatments include: Scored patterns in smooth or rock salt finish concrete; Rectangular granite or concrete block pavers in irregular sizes; Natural stone pavers set in mortar.
  - » Tile or metal inlays should be used to create visual interest, as well as to provide public or functional art, such as directional markers.
  - » Durable, smooth, non-slip, even surfaces should be used in well-traveled areas.

## PRIVATE OPEN SPACE

Private open spaces such as porches, rooftops, green roofs, and balconies provide personal spaces to relax, play, and exercise.

12. Porches, verandas, balconies, patios, and decks should be oriented toward open spaces and streets to increase visibility of these areas and enhance their safety.
13. Common private open spaces, including upper level and rooftop open spaces, should include landscaping.
14. Common private open space should be conveniently located for the intended users.

15. Green roofs, or eco-roofs, are encouraged because they are aesthetically pleasing, reduce stormwater runoff, reduce energy consumption, and are spaces for community gardens.
16. Residential balconies shall be large enough to be occupied. See requirements for minimum sizes in Chapter 5, *Development and Design Standards*.

## STREETSCAPE AMENITIES

17. Open spaces should include well-designed seating of different varieties including movable and fixed seating. In courtyards and plazas, movable seats are encouraged for flexible use of the space.
18. Seating and other amenities should be incorporated into sculptural forms and public art as part of the overall landscape design.
19. Seating should be provided in both sunny and shaded locations.

### Open Space Types

**Paseo:** A paseo is a linear open space for only pedestrian or emergency access located between buildings or blocks. Paseos are often distinguished from streets using special paving and pedestrian-scaled lighting as well as the incorporation of pedestrian amenities such as at-grade landscaping and/or planters, ample seating, and public art.

**Courtyard:** A courtyard is an open space that is a combination of hardscape and softscape framed by a building on at least three sides.

**Plaza:** A plaza is an open space that is a combination of hardscape and softscape framed by a building on at least two sides.

**Forecourt:** A forecourt is an open area in front of a building that functions as an inviting, spatial, transitional element between the street and building environment.

## 7.2.7 GREEN STREET DESIGNS

Roads present many opportunities for green infrastructure application. One principle of green infrastructure involves reducing and treating stormwater close to its source. Urban transportation rights-of-way integrated with green techniques are often called “green streets.” Green streets provide a source control for a main contributor of stormwater runoff and pollutant load. In addition, green infrastructure approaches complement street facility upgrades, street aesthetic improvements, and urban tree canopy efforts that also make use of the right-of-way and allow it to achieve multiple goals and benefits.

The proposed Primary Internal Circulation Street, Internal Main Street, and Internal Residential streets shall be designed to be green streets. Green streets can incorporate a wide variety of design elements including street trees, permeable pavements, bioretention, and swales. Although the design and appearance of green streets will vary, the functional goals are the same: provide source control of stormwater, limit its transport and pollutant conveyance to the collection system, restore predevelopment hydrology to the extent possible, and provide environmentally enhanced roads. Successful application of green techniques will encourage soil and vegetation contact and infiltration and retention of stormwater (Source: California Regional Water Quality Control Board Order No. R8-2009-0030).



*Permeable paving and landscaped bulb-outs help control water drainage around the site.*



*Highlighted and integrated bike lane in the street promotes alternate modes of transportation.*

## 7.2.8 PUBLIC ART AND LANDMARKS

### PUBLIC ART

Public artwork can serve many purposes including increasing visual interest, providing landmarks and meeting places, anchoring a public space, or reflecting community values. Standards for public art are addressed in Chapter 5.2.26, *Public Art*.

1. All forms of original visual art are encouraged, including, but not limited to:
  - » Painting of all media, such as portable and permanently affixed works such as murals;
  - » Sculpture, which may be in the round, bas-relief, high relief, mobile, fountain, kinetic, electronic, architectural, etc. in any material or combination of materials; and
  - » Other visual media including, but not limited to prints, drawings, stained glass, artistic lighting, mosaics, photography, clay, wood, metals, paving, plant materials, plastics, or other durable and weather-resistant materials.
2. Artwork siting and its visibility are important design considerations. The artwork should be easily visible to the general public and be located in an area specifically designated on the approved building plans. Appropriate locations may include entryways, greenbelts, pathways and building exteriors. A wide range of styles, materials and types of artworks is encouraged to assure a balanced and interesting collection.
3. Exterior artwork(s) should be adequately lit to be clearly visible from sidewalks during evening hours. Interior artworks should be adequately lit during all hours of public access.
4. To provide diversity in artwork and opportunity among artists, generally not more than five pieces by the same artist are permitted.
5. Artwork should be an integral part of the landscaping and/or architecture of the building.



*Painting streets activates the ground plane and delineates spaces.*



*Artwork acts as wayfinding throughout the site.*

## 7.2.9 LIGHTING

Lighting design should help reinforce the character of WMSP area, enhance pedestrian and vehicular safety, and highlight the building design, public art, and landscape features. Well-designed lighting figures establish the character of the area and add to the attractiveness of the site.

1. Light fixtures should be architecturally compatible with the main structure or theme of the development. Light fixtures should be decorative and enhance the character of the area.
2. Low-contrast lighting, low-voltage fixtures, and energy-efficient bulbs, such as light emitting diode (LED) bulbs should be used for outdoor lighting.
3. Automatic timers should be programmed to maximize personal safety at night while conserving energy. They should be reset seasonally to match the flux of dusk and dawn.
4. Color temperature and light levels should be designed to create a quality experience and promote safety. The quality of light, level of light, as measured in foot-candles, and type of bulb or source should be carefully considered.
5. Uplighting of building elements and trees should use the lowest wattage possible to minimize impacts to the night sky. Light sources for wall washing and tree lighting should be hidden.
6. Where appropriate, downward pocket lighting should be incorporated into walls, stairs, or bollards.
7. Landscape lighting should be used to accent walkways and entries and/or seating areas and specimen plants.
8. Exterior lighting should be designed and located so as not to project off-site or into adjacent or on-site residential areas. Exposed bulbs should not be used. Cut-off lighting is preferred.
9. Parking areas should be designed using many small-scale lights versus fewer, excessively tall or bright lights.
10. Solar Powered fixtures are encouraged for all lighting when it does not conflict with security concerns.



*Lighting that engages the overhead and ground plane activates spaces at night.*

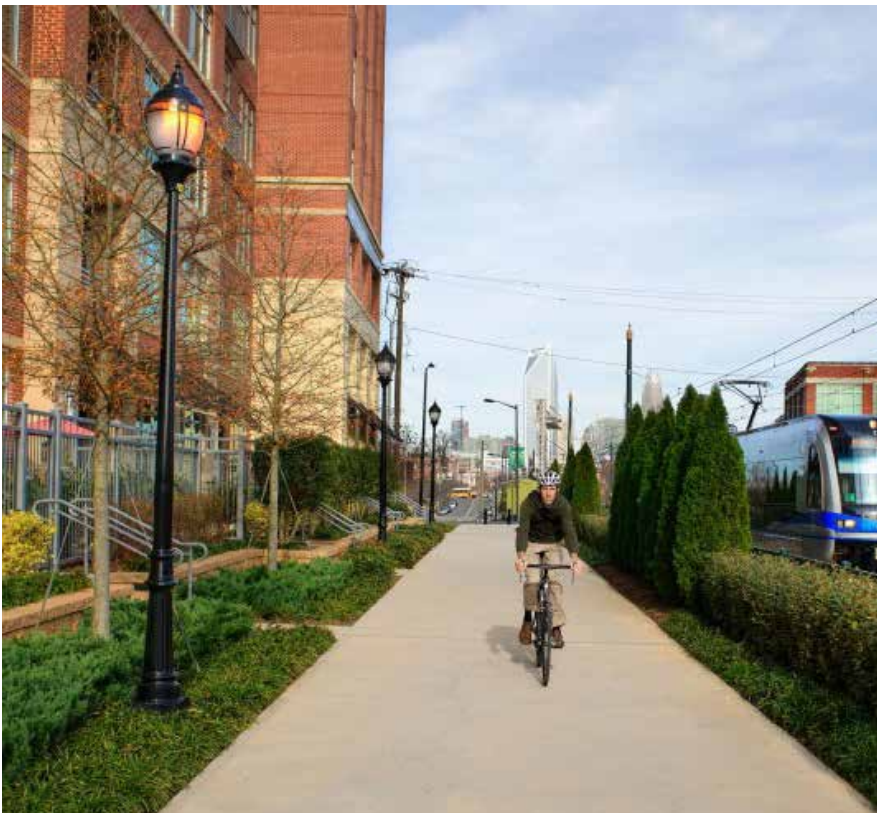


*Area lighting in public spaces on the first floor aids visibility and overhead string lights on the second floor creates a passive private area.*



## 7.2.10 FENCES, WALLS, AND SCREENING

1. Long walls shall be made more attractive and visually interesting through the incorporation of surface articulation, pilasters, and view fencing where appropriate.
2. A combination of elements should be used for screening including solid masonry walls, berms, and landscaping.
3. Guardrails throughout the project should complement the architectural style of the buildings in closest proximity.



*Planted edge creates a visually softer barrier from the transit while the low fence behind the hedge adds a structural barrier.*



*Private dining area with a modular wall can conform to the area needed.*



*Low planted fence allows for views out when standing and blocks views when sitting for privacy.*

## 7.2.11 SERVICE AREAS, UTILITIES, STORAGE, AND TRASH ENCLOSURES

Landscaping and architecturally designed screening as identified in Chapter 5.2.9 shall be used to screen trash enclosures, parking areas, storage areas, loading areas, and public utilities.

1. Any outdoor equipment, whether on a roof, side of a structure, or the ground, should be appropriately screened from view. The method of screening should be architecturally integrated with the adjacent structure in terms of materials, color, shape, and size.
2. Utility service areas, such as electrical panels, should be placed within enclosures that are architecturally integrated into the building design.
3. Access for fire apparatus should be part of the planning process so as not to disrupt the visual integrity of a project.
4. Wherever feasible utilities should be installed underground.

### TRASH AND RECYCLING ENCLOSURES

5. Service, maintenance, storage, and trash areas should be located in discreet places to the extent feasible while still allowing convenient access for each tenant and screened with landscaping from adjacent public rights-of-way, public plazas, pedestrian corridors and building fronts.
6. Trash, recycling, and compost enclosures should be complementary. Screening landscaping is encouraged.
7. Trash, recycling, and compost enclosures located in parking lots should be separated from adjacent parking stalls by minimum three-foot wide planters with low-growing plant materials.
8. Half of the trash and recycling area should be dedicated to recycling containers.

9. Trash, recycling, and compost bins should be conveniently accessible for collection and maintenance and should not block access drives during loading operations.
10. Waste collection programs should consider the implementation of compactor systems where appropriate.



*Trash utilities softened/disguised by planting and overhead structure.*



*Using materials and design complementary of site disguises utilities.*

## 7.3 GENERAL BUILDING DESIGN

Building design guidelines are intended to provide a framework for the design of buildings and to ensure attention is given to placemaking throughout the Plan Area. The guidelines promote high-quality building design that focuses on a vibrant visual experience at street level.

These guidelines do not dictate the use of a specific architectural style or styles within WMSP but encourage thoughtful building design that integrates architectural design across multiples scales. A new building should represent a single architectural vision that massing, materials, and details adhere to. When multiple buildings are developed, each building should be complementary, but differentiated in appearance through materials, architecture, and design.

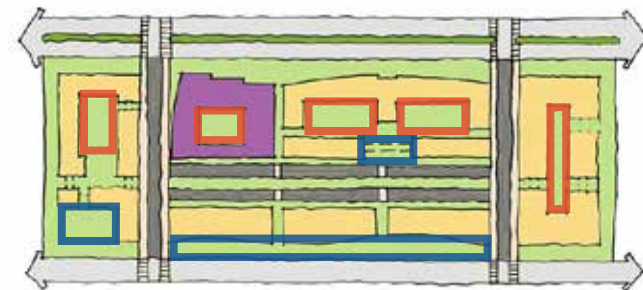
### 7.3.1 BUILDING FORM AND MASSING

Building massing refers to how the development program is shaped into a structure that gives a building its overall form. Massing addresses wall plane location, wall heights, and roof levels. The form and massing of a building can be used to frame public spaces, provide architectural interest, and create a variety of experiences.

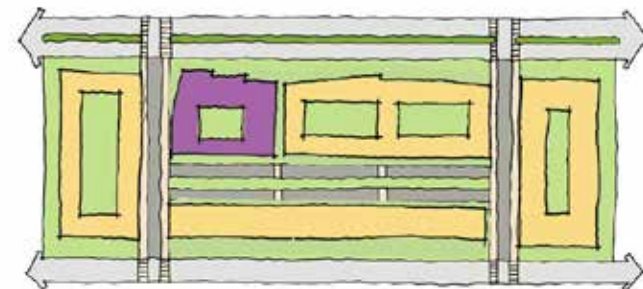
1. Buildings should be divided into distinct massing elements that create interest in the vertical and horizontal plane. This can be achieved by varying the wall plane by projecting or recessing elements, varying the wall height, and varying the roof plane.
2. Large projects should be separated by facade modulations that make the building appear as a series of different buildings.
3. Narrower building floor plates are encouraged instead of long buildings with no breaks in the building to allow for increased natural light and air.
4. Massing should be coordinated with adjoining and adjacent structures to create a cohesive spatial relationship and to frame pedestrian plazas and common open space.

5. Buildings greater than three stories should be made less imposing by stepping back each floor height from the street level.
6. Vertical elements, such as towers, may be used to accent horizontal massing and provide visual interest.
7. The residential portion of buildings should be configured and oriented to afford a sense of privacy and create small-scale open spaces.

*Buildings greater than three stories should be made less imposing by stepping back from the street level.*



**Red Space: Private Spaces**      **Blue Space: Public Spaces**  
*Building variation for massing and articulation creates public and private spaces in the site.*



*Lack of building variation for massing and articulation eliminates opportunities for more public spaces in the site.*

### 7.3.2 ROOFS

1. A variety of roof planes, ridge heights, materials, and detailing appropriate to the architectural character of the building should be used.
2. Height and roofline juxtapositions between adjoining buildings should be used to establish a variety of interesting structures and block faces.
3. Substantial roof overhangs should be used, whenever feasible to provide shade and refuge.
4. Rooftop gardens and decks are discussed in Section 5.2.15, *Open Space Requirements*.
5. Roofs should reduce heat island effects by using solar-reflective surfaces or vegetated green roofing.

### 7.3.3 FAÇADES AND ARTICULATION

1. Surface detailing should not serve as a substitute for distinctive massing.
2. Street level articulation should be human-scaled.
3. The use of standardized “corporate” or “franchise” architectural styles is strongly discouraged.
4. Building rhythm is the repetitive use of a group of visual elements to create a unified appearance of a single building. Rhythm should be expressed by integrating color, material, articulation, massing, or architectural elements such as columns and pilasters as part of the overarching architectural scheme.



*Alternating roof line and colors visually break a larger building.*



*Varying facade setbacks create opportunities for outdoor seating as well as additional buffers for pedestrians.*

### 7.3.4 FRONTAGES, BUILDING ENTRANCES, AND GROUND FLOOR TREATMENTS

The façade and ground floor of a building are the most visible components seen by pedestrians, bicyclists, and motorists who travel alongside within eye-level view. How the primary mass of the building “meets the street” is what humans experience most intimately when on the sidewalk and often becomes the biggest contributor to neighborhood character. Views of a building from a greater distance will also capture the façade and ground floor; the details will be less noticeable but nonetheless important as people approach and enter a building.

1. Active street frontages should be created by facing articulated entrances with carefully arranged doors, windows, awnings or other architectural treatments onto plazas, paseos, and streets.
2. Ground floor uses and amenities that enrich the pedestrian environment are encouraged. This may include outdoor dining and cafes, porches, patios, public seating, public art, and landscaping.

#### ENTRIES AND WINDOWS

3. Front doors and entrances should be clearly defined and recognizable from streets or walkways
4. The residential portion of mixed-use buildings should provide secured entryways for residents separate from the non-residential uses.
5. Residential buildings should emphasize pedestrian access to open space, walkways, and the public realm.
6. Windows facing existing and planned residential buildings should respect the privacy of adjacent residential structures.
7. Windows should be placed to promote natural surveillance of public and common private areas.



## 7.3.5 ARCHITECTURAL DETAILS

### DESIGN ELEMENTS AND ARCHITECTURAL DETAILS

1. Building scale should be reduced through the proper use of window patterns, structural bays, roof overhangs, wall materials, awnings, figures, and other details.
2. Architectural details that relate to human scale, such as arches, trellises, or awnings, should be used on lower-level walls.
3. The relationship between the height, mass, and thickness of a column should be appropriate to the weight of the overhead structure that the column supports.
4. The height, width, and depth of arches and other architectural details should emphasize the building's strength and balance.

### AWNINGS AND PROJECTIONS

Pedestrian-scale architectural elements, such as awnings, large windows, and architectural projections should be applied to buildings in a cohesive manner that supports the overall architectural vision for the project.

5. Passive shading elements that are integrated into the architecture of the building, such as brise soleil, overhangs, and awnings, are encouraged.



*Private communal outdoor spaces provide added space for residents.*

### MATERIALS AND COLORS

6. The finish, texture, and color of materials should be compatible with the proposed architectural character of the building.
7. Materials and colors should be used to emphasize entrance lobbies, massing changes, and different uses or tenants as appropriate.
8. Construction details should be authentic and applied with consistency. Faux architecture is strongly discouraged. Façade elements constructed of foam or foam molding are prohibited on the ground floor of buildings and should be avoided overall.
9. Alternatives to stucco are preferred. When stucco is used it should be applied with a smooth finish. Stucco seams should be used to create visual interest for the building's façade and form.



*Colors and materials highlight different building forms.*

**Figure 7.1: Building Form and Design Elements**



The illustration above depicts how buildings can incorporate a variety of design elements to create an attractive streetscape. These components should be applied to commercial, residential, and mixed-use buildings.

To ensure projects meet a consistent quality of design across the entire WMSP a minimum of three elements to be incorporated from each of the three categories: Building Massing, Design Features, and Pedestrian Features.



1. Combine vertical building features with other elements to create focal points and identity.



2. Divide buildings into distinct massing elements.



3. Vary exterior wall planes in depth and/or direction.



4. Design elements on building facades create depth and shadow patterns.





5. Apply pedestrian-scale architectural elements to buildings.



6. Exterior lighting to be located on all walkways and alcoves.



7. Awnings and canopies on buildings add architectural interest.



8. The arrangement of windows and doors on various elevations of the building design are to be implemented.



9. Storefronts to include a minimum of 60 percent glass. Windows may be large but should not extend from floor to ceiling.



10. Utilize wall-mounted lights to minimize use of freestanding light standards.



11. Signs should match the proportion and scale of building elements within the facade.



12. Orient storefronts and major building entries toward streets and active pedestrian areas.



13. Storefronts to convey an open, inviting appearance through use of windows, doors, and entrance features.



14. Trees, shrubs, and ground cover to be used in a variety of sizes.



15. Use accent planting around entries and key activity hubs.



16. Direct drainage to permeable areas such as landscaped planters.

### 7.3.6 SUSTAINABILITY

All new buildings shall be built with solar-ready electrical systems/ hardware and provided with adequate surface area for these systems.

### 7.3.7 REUSE OF EXISTING MALL STRUCTURES

1. If mechanical equipment is added to existing buildings, it should be screened in such a way as to match the architectural style and materials of the building without giving the appearance of being added on.
2. If existing buildings remain, improvements to existing facades are encouraged. Quality architectural elements that may already exist on the building should be preserved.
3. Renovations to existing corner buildings with blank walls should include the addition of architectural and massing detail to emphasize the corner's significance.



*Maximizing the use of a non-use area for solar panels.*



*Modern design elements in an older building creating appealing visual contrast.*

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# Chapter 8

# **Public Services and Infrastructure**





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## 8.1 PUBLIC SERVICES

The following describes existing public services and infrastructure that currently serve the site.

### POLICE PROTECTION

Police protection is provided by the City of Westminster Police Department (WPD), from the Department's headquarters located at the City's Civic Center.

### FIRE PROTECTION AND EMERGENCY SERVICES

Orange County Fire Authority (OCFA) provides fire protection and emergency services to the City of Westminster. Three OCFA fire stations provide protection to Westminster. Fire Stations #64 and #65 are the nearest stations to the Specific Plan area, located less than one mile from the site, at 7351 Westminster Boulevard and 6061 Hefley Street in the City of Westminster, respectively.

### SCHOOL

Public education in the City of Westminster is provided by the Westminster School District, Garden Grove Unified School District, Ocean View School District, and the Huntington Beach Union High School District. The project site is served by Westminster School District and Huntington Beach Union High School District.

### SOLID WASTE

Solid waste collection service is provided to the Specific Plan area by the Midway City Sanitation District (MCSD). Pursuant to Assembly Bill (AB) 939, the State of California requires all municipalities to divert 50 percent of their solid waste from landfill disposal through source reduction, recycling, and composting. Pursuant to AB 1826 effective January 1, 2019, businesses that generate four cubic yards or more

of commercial solid waste per week shall arrange for organic waste recycling services. In 1992, the City of Westminster completed its Source Reduction and Recycling Element (SRRE) to comply with these requirements.

Since that time, AB 341 increased this goal to 75 percent by 2020. These mandates apply to all commercial and multi-family residential uses within the Specific Plan area. New development within the Specific Plan area is required to provide convenient recycling within its trash collection areas in accordance with CalGreen. Construction waste shall also be diverted subject to the requirements of CalGreen.

## 8.2 UTILITIES

The following describes existing utilities that currently serve the site.

### COMMUNICATION

A range of communication services by national and local service providers are available to the project site. Currently, telephone and cable services are located underground along the project site's frontage on both Bolsa Avenue and Edwards Street.

### GAS AND ELECTRICITY

Gas and electric service in the City of Westminster is provided by SoCalGas and Southern California Edison, respectively. Existing utilities are located underground along the site's frontage on both Bolsa Avenue and Edwards Street.

## 8.3 INFRASTRUCTURE

The WMSP allows for an increase of development within the Specific Plan boundary, which will increase the demand of potable water and sewer flows over existing conditions while largely maintaining existing runoff conditions. A technical study addressing hydrology, sewer and

wastewater infrastructure, water distribution infrastructure, and water quality prepared for the project analyzed the proposed land use changes within the Specific Plan Area. The study analyzed how these changes may impact the existing infrastructure that lies within or immediately downstream from the project (See the Environmental Impact Report for the full report).

No deficiencies under existing conditions were noted for sewer, water, and storm drain infrastructure. For those areas where the land use changes may impact the existing infrastructure, project-specific analyses will be required during final design to evaluate individual storm drain, water and sewer facilities related to the individual projects.

Based on the existing built out condition and the proposed land use changes under the WMSP, including the implementation of low impact development features, no substantial additional sources of pollutants or significant increases in runoff for the 85th percentile storm event are anticipated. Individual projects will adequately reduce project related impacts to hydrology and water quality to a level less than significant through incorporation of site design/ Low Impact Development (LID) features and infiltration/biotreatment Best Management Practices (BMPs), as required under the MS4 Permit and local LID requirements.

### 8.3.1 HYDROLOGY

The WMSP site resides within the Anaheim Bay – Huntington Harbour Watershed within the regional Santa Ana River Watershed (Figure 8.2, *Anaheim Bay – Huntington Harbour Watershed*). The Anaheim Bay-Huntington Harbour Watershed is located in northern Orange County, approximately 25 miles south of Los Angeles and 85 miles north of San Diego. The Anaheim Bay-Huntington Harbor Watershed is composed of a number of channels, none of which is a dominant river for the watershed, with each draining a substantial portion of the watershed. The Specific Plan area drains to the Westminster Channel (OCFCD C04)

through City and County storm drain lines and confluences with Bolsa Chica Channel (OCFCD C02). The Westminster Channel is concrete-lined and serves entirely urbanized subwatershed.

## EXISTING CONDITIONS

### *Drainage Facilities*

The WMSP area is served by three primary flood control and drainage systems.

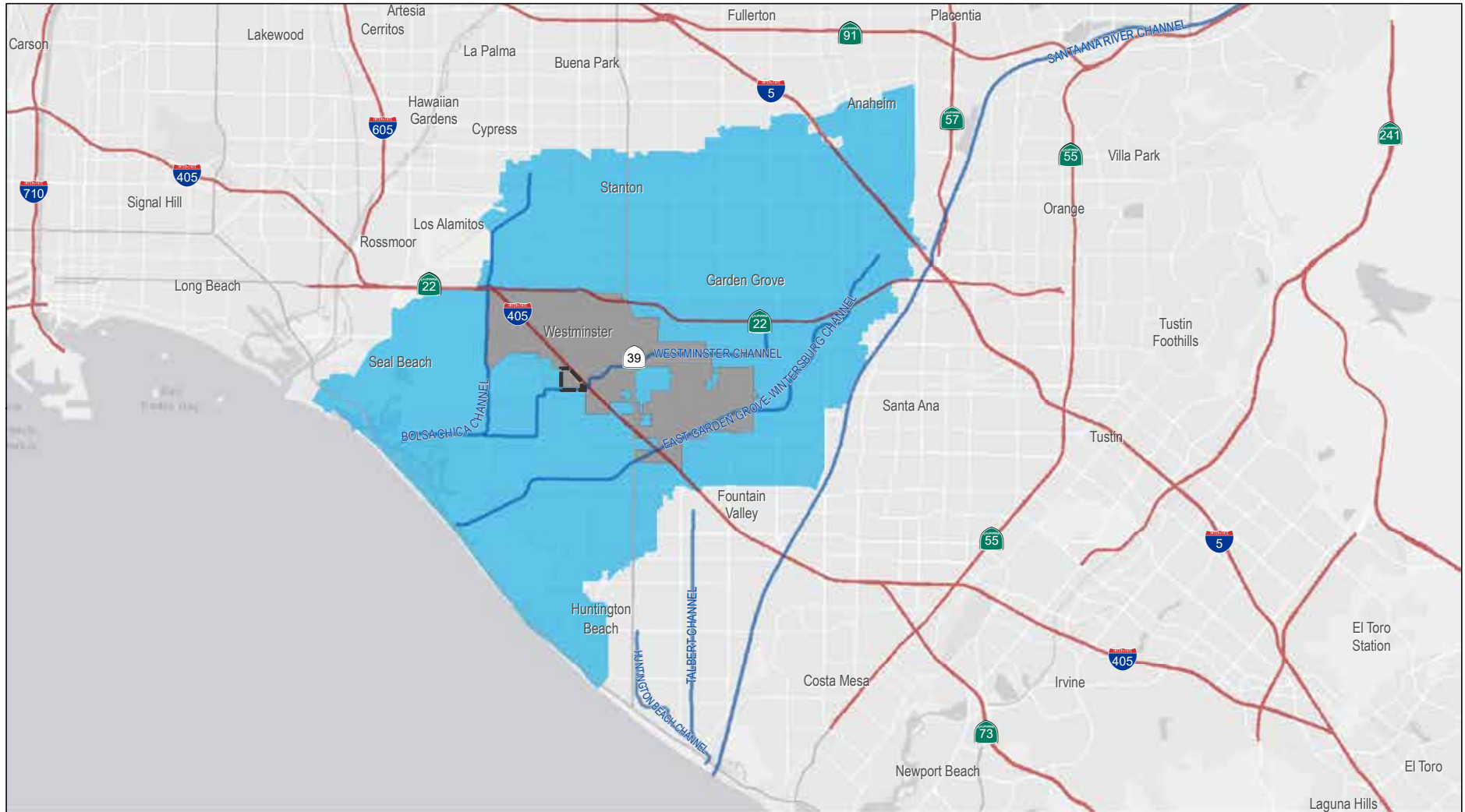
Private Storm Drain lines ranging from 10” to 30” in diameter currently serve the WMSP site. Flows drain from the northeast of the site to the southwest and connect to both City and Orange County Flood Control District (OCFCD) infrastructure.

The City operates and maintains the adjacent storm drain system including catch basins and a pipeline that runs along Edwards Street that ranges from 63” to 66” in diameter as it runs downstream.

OCFCD operates and maintains the Westminster Channel which runs along Bolsa Avenue to the south of the WMSP area.

Runoff from the WMSP area generally sheet flows across impervious surfaces, prior to draining to on-site storm drain infrastructure through grate inlets and catch basins. Under existing conditions, the WMSP area is estimated to be approximately 90% impervious, using the OC hydrology manual criteria. Flows drain to the Westminster Channel either through City infrastructure or directly from the site to the channel. Flows ultimately drain to Anaheim Bay, Huntington Harbor and the Pacific Ocean. See Figure 8.3, *Existing Storm Drain Systems*, for existing storm drain facilities within and surrounding the WMSP boundary.

# Figure 8.1: Anaheim Bay - Huntington Harbour Watershed



**Anaheim Bay-Huntington Harbor Watershed**  
*Westminster Mall Specific Plan*

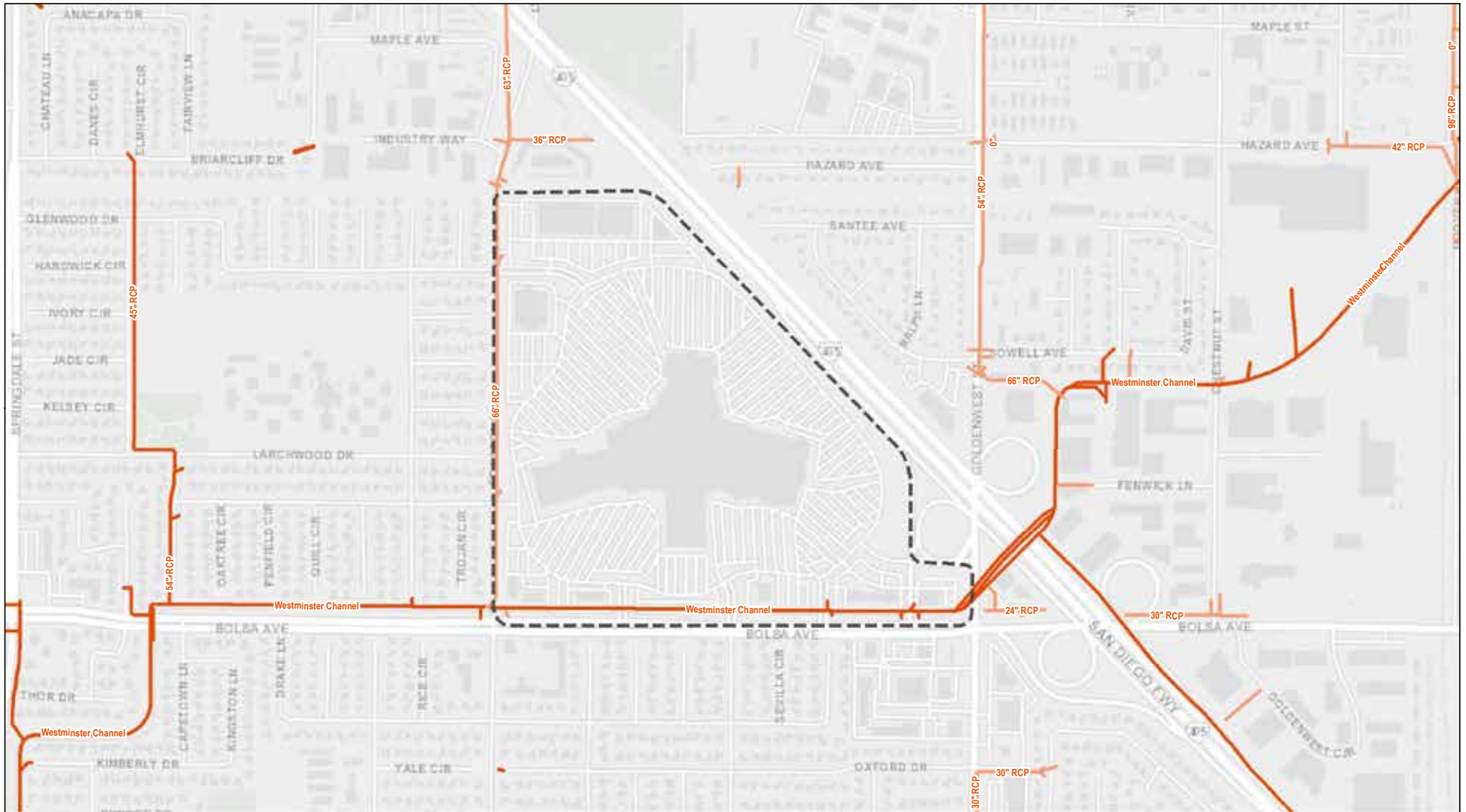
**Figure 7-1**  
 2/14/2020

Specific Plan Boundary    
 City of Westminster    
 Anaheim Bay-Huntington Harbour Watershed

   
 10,000 5,000 0 10,000 Feet

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# Figure 8.2: Existing Storm Drain Systems



**Existing Storm Drain Systems**  
*Westminister Mall Specific Plan*

Specific Plan Boundary
 — City Storm Drain Lines
 — OCFD Storm Drain Lines

**Figure 7-2**  
 4/9/2020

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### *Floodplain Mapping*

The WMSP area is split between two Flood Insurance Rate Maps (FIRMs), see Figure 8.3., *FEMA Flood Zones*. The northerly 8-acres is covered under FIRM map No. 06059C0119J and is designated as flood zone “X” (shaded). The balance of the WMSP area is covered under FIRM map No. 06059C0232J and is designated with two flood zones. The most southerly 270 feet, adjacent to the north side of Bolsa Avenue, is designated as flood zone “A” and is a special flood hazard area subject to inundation by the 1% annual chance flood. The portion north of the flood zone “A” is designated as flood zone “X” (shaded). Flood zone “X” is defined by FEMA as those areas subject to a 0.2% chance of flooding; areas of 1% annual chance of flooding with average depths of less than 1-foot or with drainage areas less than 1-square mile; and areas protected by levees from a 1% annual chance flood. Flood zone “A” is a special flood hazard area and has no base flood elevation determined. See Figure 8.3, *FEMA Flood Zones*, for flood zones within and adjacent to the Specific Plan area.

### *2004 City of Westminster Master Plan of Drainage Update*

The 2004 Stormwater Master Plan includes hydrologic modeling for the City of Westminster storm drain lines to determine existing capacity. The analysis returned a number of deficient segments in need of improvement and areas susceptible to localized flooding. However, none of these areas were within the boundary or immediate vicinity of the Specific Plan area and would not have any impact on the existing or proposed conditions of the WMSP.

Deficient segments and areas in need of improvement are added to the City’s long-term capital improvement list. Any deficient segments found within the WMSP area in the future will be added to the City’s improvement list or will be improved as part of redevelopment activities and agreements associated with the Specific Plan.

## PROPOSED BUILDOUT CONDITIONS

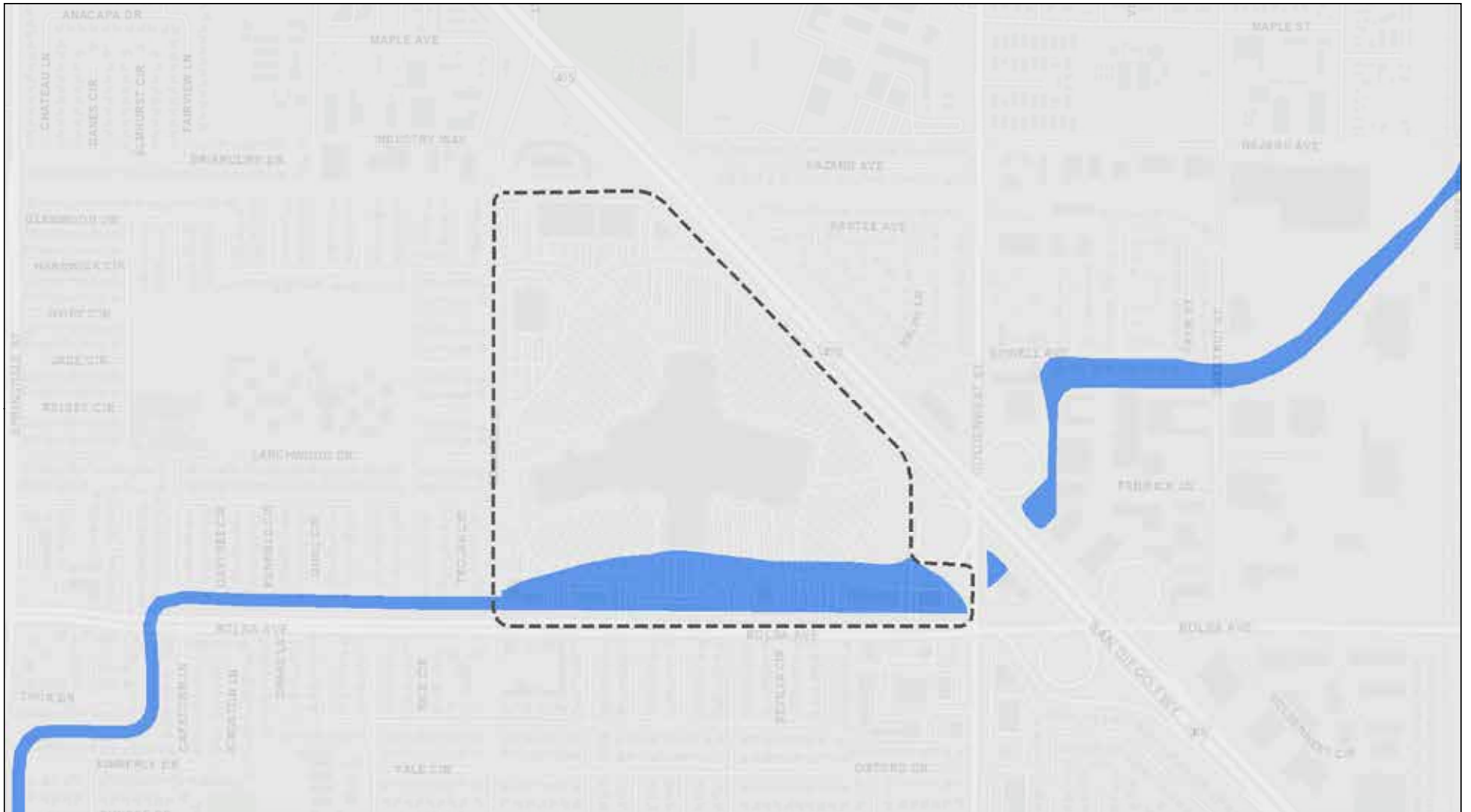
### *Drainage Conditions*

The increase of up to 3,000 dwelling units allowed under the WMSP, coupled with a decrease of 160,000 square feet of commercial/retail area, will result in a decrease of impervious cover from 90% to 80-85%. Therefore, it is anticipated that runoff from the WMSP area will ultimately decrease in the proposed condition as compared to the existing condition and that on-site detention for flood control will not be required. This will be confirmed with the City and OCFCD during the final design phases of the Specific Plan.

All new storm drains will be appropriately located and sized to convey flows respective to their tributary area for the design storm required by City and County requirements. Infrastructure will connect to either the 66” City of Westminster line on Edwards Avenue or the Westminster Channel, and discharge to Anaheim Bay-Huntington Harbour as under existing conditions. Any noted deficiencies as a result of infrastructure improvements will be corrected on an as-needed basis.



**Figure 8.3: FEMA Flood Zones**



**FEMA Flood Zones**

Westminster Mall Specific Plan



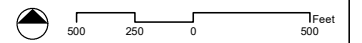
Specific Plan Boundary

**FEMA Flood Zones**

- A
- X

**Figure 7-3**

2/12/2020



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## 8.3.2 SEWER AND WASTEWATER INFRASTRUCTURE

The purpose of the sewer and wastewater evaluation is to determine if the existing sewer system can accommodate the proposed land use changes at the Specific Plan level. The analysis is based on information provided by the City of Westminster, Midway City Sanitation District (MCSD) and Orange County Sanitation District (OCSD).

### EXISTING CONDITIONS

#### *Sewer System and Facilities*

The sewer infrastructure system in the City is primarily maintained by MCSD. MCSD sewer infrastructure ultimately ties into OCSD regional trunk lines for wastewater treatment. OCSD sewer trunk lines range from 15"-95" throughout the City of Westminster. Under existing conditions, the WMSP area is served by a series of private 12" to 15" vitrified clay pipes (VCP) with 6" laterals. These flow from on-site buildings towards the north and west, before connecting with a 15" MCSD line. After connecting through MCSD infrastructure, flows ultimately tie to the 66" OCSD Miller-Holder trunk line. Flows within the Miller-Holder trunk line drain to OCSD Reclamation Plant No. 2 for treatment. However, flows can be diverted to either Reclamation Plant No. 1 or Reclamation Plant No. 2 based on available plant capacities and modeled inflows. Figure 8.5, *Existing Sewer Systems*, shows the existing sewer infrastructure surrounding the Specific Plan Area.

#### *Sewer Flows*

For each land use within the WMSP boundary, footprints of the existing development (i.e. retail and restaurant) were utilized along with their corresponding flow/generation factors to develop existing condition

flow rates. Under the existing conditions, average daily sewer flows are estimated at 0.07 million gallons per day (MGD) for the Specific Plan area.

#### *Sewer Capacity Assessment*

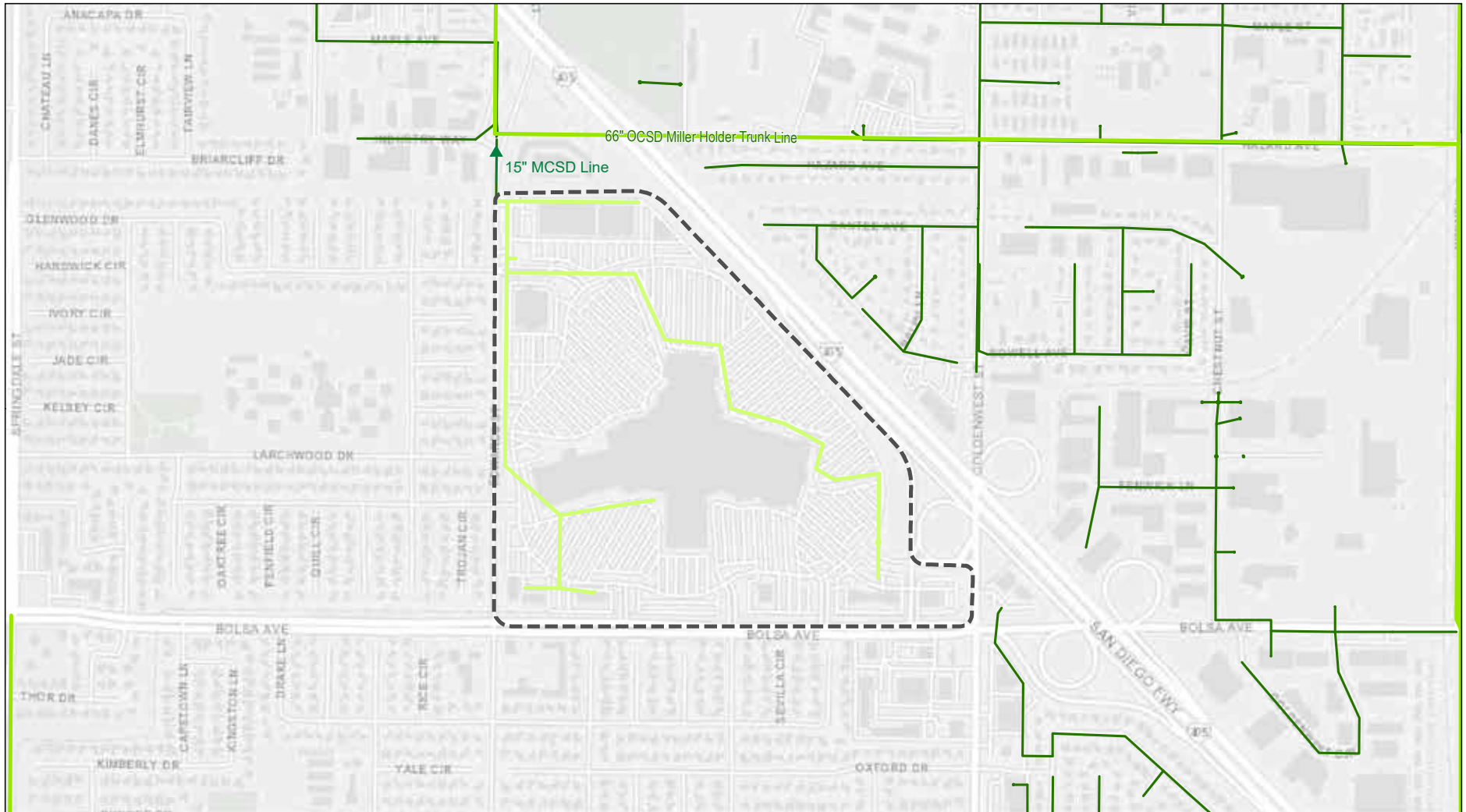
The MCSD Sewer System Master Plan (April 2015) calculated flow rates from a hydraulic model to determine the adequacy of the existing backbone sewer system to convey existing and ultimate sewer loadings. The sewer system currently has a capacity of 18 MGD and was operating at 5 MGD, signifying sufficient capacity for existing land uses and future development. MCSD staff confirmed there were no deficiencies with the existing 15" MCSD sewer line that connects the Specific Plan sewer flows to the OCSD Miller-Holder trunk line.

### PROPOSED BUILDOUT CONDITIONS

#### *Proposed Wastewater System Flows*

Under the proposed condition, sewer flows will increase significantly due to the increase in multi-family residential and commercial/restaurant land uses. Using the same commercial flow methodology as the existing conditions, residential sewer flows were calculated using residential water demand factors multiplied by a 0.95 sewer flow factor. Full implementation of the land use changes has the potential to increase sewer flows by 0.49 MGD within the Specific Plan area. The increased sewer flows will connect through an existing 15" MCSD line before connecting to the OCSD Miller-Holder trunk line north of the Specific Plan boundary. The proposed increases in sewer flows could also impact OCSD sewer trunk lines downstream of the Specific Plan boundary. Figure 8.6, *Proposed Sewer Demands*, highlights areas that will experience increased sewer flows from land use changes.

**Figure 8.4: Existing Sewer Systems**



**Existing Sewer Systems**  
 Westminister Mall Specific Plan

Specific Plan Boundary    
  MCSD Sewer Lines    
  OCSD    
  Private Sewer Lines (6"-15")

**Figure 7-4**  
 2/14/2020

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### *Proposed Sewer/Wastewater System*

Per the MCSD Sewer System Master Plan (April 2015), there are no MCSD segments identified as having capacity issues within or adjacent to the Specific Plan area. The proposed increase of 0.49 MGD will not exceed the available regional capacity of MCSD's system.

Under proposed WMSP buildout, all on-site sewer infrastructure is anticipated to be demolished with new infrastructure sized for the proposed land uses. Proposed sewer infrastructure will continue to tie into the existing 15" MCSD line.

While the 15" line is currently in good condition with no capacity issues noted, a project-specific sewer study will be required to model impacts from the proposed project buildout to determine if any new deficiencies will occur. In the case of noted deficiencies or potential deficiencies, MCSD has processes in place to work with developers of the WMSP to upsize the deficient segments. MCSD utilizes development fees for new connections and proposed flow increases to improve existing low capacity sewer lines and upsize existing lines. While development fees come through MCSD, the MCSD works with OCSO to finalize fees for new sewer connections. Any future development in the Specific Plan area will be required to pay additional fees associated with the increase in wastewater flows.

Although OCSO has no deficient lines serving the Specific Plan area, it utilizes development fees to cover associated costs with providing any incremental expansions in service or infrastructure as a result of new development that increases the quantity or flow rate of wastewater discharge. Potential impacts to OCSO facilities and associated OCSO review requirements and connection fees will need to be analyzed on a project-by-project basis.

## **8.3.3 WATER DISTRIBUTION INFRASTRUCTURE**

### **EXISTING CONDITIONS**

#### *Water System*

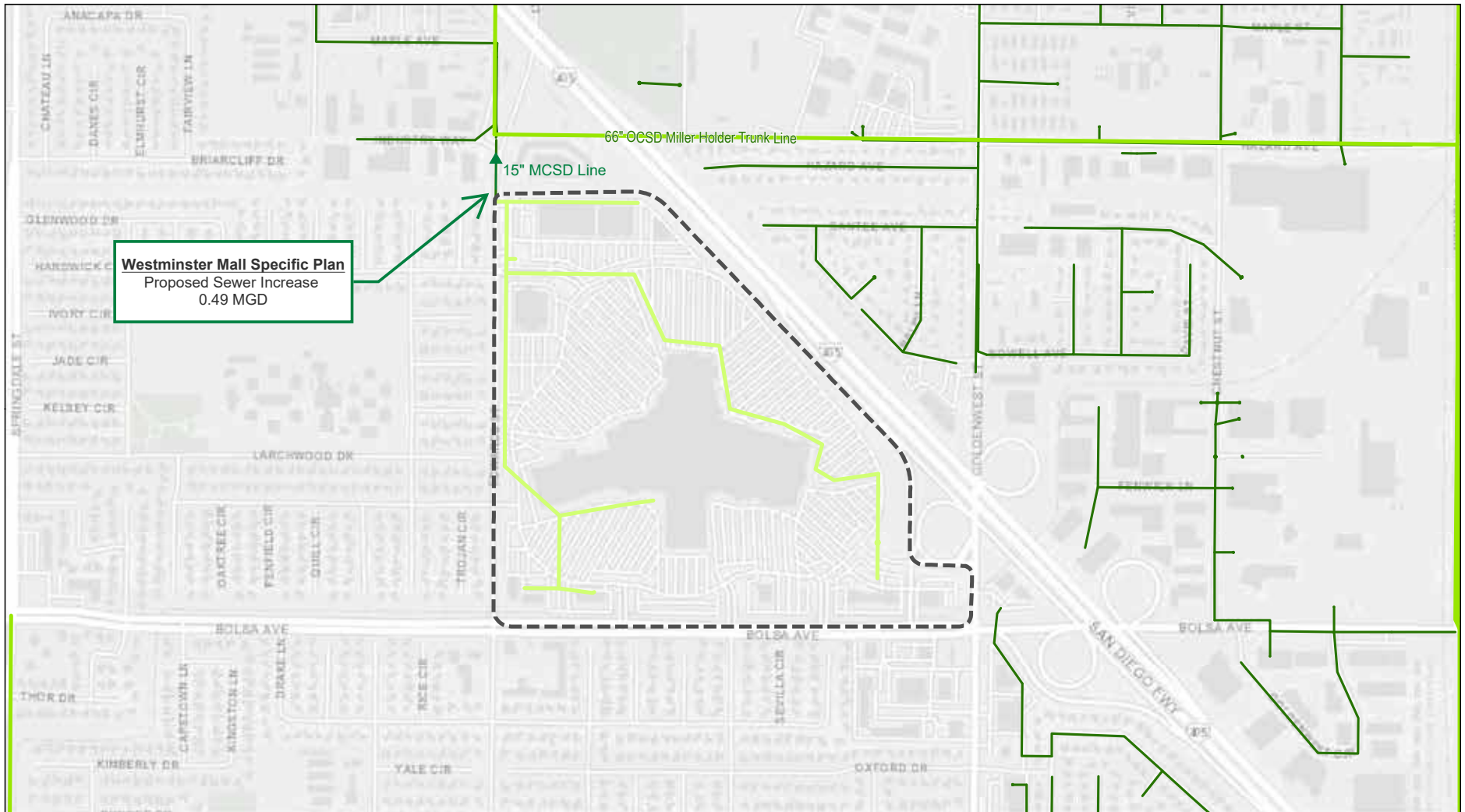
The City of Westminster is the water service provider for the Specific Plan area. Westminster receives water from two main sources: imported water from Metropolitan Water District (MWD) and groundwater pumped and treated from city-owned wells. The City has a 16 MGD reservoir facility that was constructed in 2003 and consists of two 8 MGD storage tanks, four booster pumps and a 2,800-gallon per minute (gpm) well. Approximately 75% of the City's water supply is from ten city-owned groundwater wells that are located within the City's boundaries.

Per inspection of the 1973 as-built plans for the Westminster Mall site, the WMSP area is currently served by a single loop water distribution network. A 12" ductile iron pipe (DIP) serves the north, east, and south side tied to a 10" DIP on the west side, completing the loop. On-site water systems are fed by a 12" City line on Bolsa Avenue. Figure 8.7, *Existing Water Systems*, shows locations of the existing water facilities and existing water systems serving the WMSP area.

#### *Water Demand*

Water demand factors were supplied from the 2019 City of Westminster Water Master Plan Update (WMPU). Water usage for landscaping was estimated using the Estimated Total Water Use (ETWU) methodology. Under the existing conditions, average daily demands are estimated at 0.07 MGD.

**Figure 8.5: Proposed Sewer Demands**



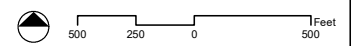
**Proposed Sewer Demands**

Westminster Mall Specific Plan

- Specific Plan Boundary
- MCSD Sewer Lines
- OCSD
- Private Sewer Lines (6"-15")

**Figure 7-5**

2/14/2020



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## *Water System Analysis*

The 2019 Water Master Plan Update serves as the guiding document that outlines the capacity and improvements required for the City's water distribution facilities to meet the needs of current and future development, through project buildout conditions. The WMPU identified that no pressure improvements were required throughout the City, and that all distribution lines had adequate pressure. Approximately 20,063 linear feet of water main replacements were identified in the WMPU, to be reviewed and addressed on a case-by-case basis. None of these limited pipelines were located in the vicinity of the WMSP.

In addition to identifying improvement areas throughout the City's water system, the WMPU identified existing high demand water users and planned areas of demand increase per the City's General Plan buildout scenarios. Among existing individual water users, the Westminster Mall area was ranked second highest in the City. Proposed Westminster Mall area buildout demands anticipated an additional 0.40 MGD over existing subarea conditions.

The 2020 City of Westminster Urban Water Management Plan (UWMP) was released in June 2021 and estimates water supplies and demand for 25 years into the future. The 2020 UWMP concluded there are sufficient water supplies to meet water demands in an ultimate buildout scenario through 2045 based on data produced in 2015 by the Center for Demographic Research at California State University, Fullerton.

## **PROPOSED BUILDOUT CONDITIONS**

### *Water Demand*

Under the Specific Plan, water demands will increase significantly due to the increase in multi-family residential, commercial, and mixed land uses. Full implementation of the land use changes has the potential to increase water demand by 0.52 MGD (581 acre-feet per year (AFY))

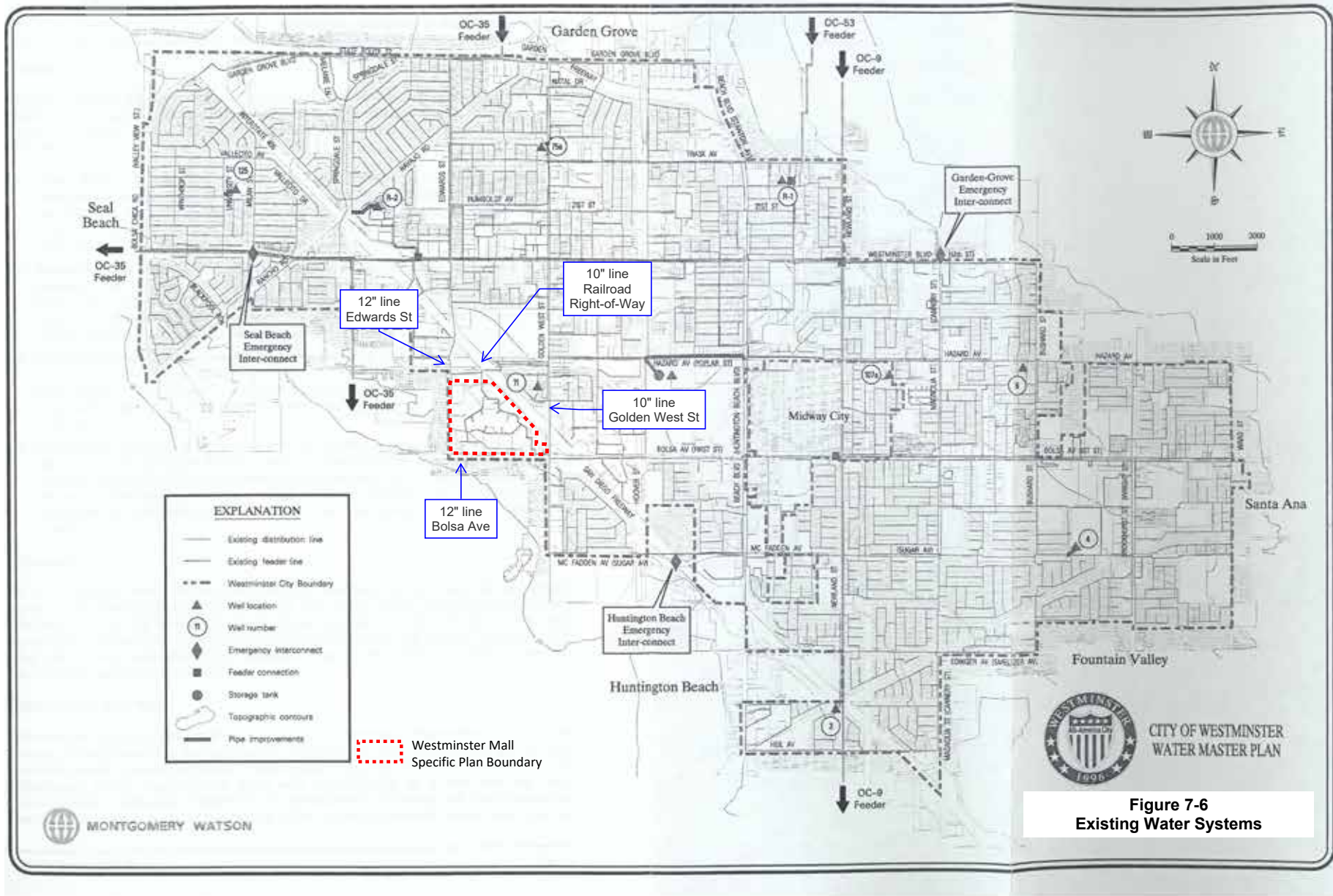
within the Specific Plan area (Figure 7.7, *Proposed Water Demand Increases*). Similar to the increases in sewer flows, water demands will primarily come from the additional 3,000 dwelling units and 142,000 square feet of proposed restaurant space.

The anticipated increases in water demands are greater than the estimated increases of 0.4 MGD for the WMSP area reported in the 2019 WMPU. This difference can be attributed to the additional dwelling units proposed under the WMSP as compared to what was anticipated during the City's general planning efforts. In addition, conservative demand factors were utilized for this water demand analysis.

### *Water Supply*

A Water Supply Assessment (WSA) was prepared to assess the water demand and supply conditions with implementation of the proposed project (see Environmental Impact Report for full report). According to the City's UWMP, the City has adequate supplies to serve 100% of its customers during a normal, single dry year, and multiple dry year demand through 2045 with projected population increases and accompanying increases in water demand. However, the City's General Plan was updated in 2016 and included land uses for the project as well as other redevelopment areas within the City that were not included in the 2020 UWMP. Therefore, projected water demand for the City at buildout in 2045, including the proposed project, was estimated based on the 2016 General Plan Update land use designations. Implementation of the proposed project, as well as buildout of the other redevelopment areas within the City, will not obstruct the City's ability to meet water demands of its customers in normal, single dry, and multiple dry years. In addition, redevelopment within the City would occur over a period of 20 to 30 years and therefore the incremental increases in water demand would not overtax the City's ability to supply water to all of its customers.

Figure 8.6: Existing Water Systems



The WSA concludes that the City will have sufficient water supplies available during normal, single dry, and multiple dry years through the year 2040 to meet all projected water demands associated with its existing and future customers, including the proposed project. In the unlikely event of a water shortage, implementation of the City's Water Conservation Program and water efficiency strategies would ensure that sufficient water supplies were available to serve its customers, including the project and existing and future users.

### *Water System*

The WMSP is not located in an area with any deficient segments or improvement projects, per the 2019 WMPU. However, per electronic correspondence with City staff dated January 22, 2020, all existing waterlines within the WMSP area will most likely require relocation and upsizing, based on demand scenarios. In addition, the existing 10" and 12" water lines located within the railroad right-of-way to north of the WMSP area will require relocation/upsizing.

Once the proposed land use is finalized for the Specific Plan, the City will run their hydraulic model to determine any deficiencies within City water lines serving the property. In the case where there are deficiencies identified, the developer will be required to upgrade the City infrastructure to serve the Specific Plan land uses. The City will work closely with the developer to design and approve of any upgrades.

## **8.3.4 WATER QUALITY**

### **REGULATORY FRAMEWORK**

#### *Basin Plan*

The Santa Ana Regional Water Quality Control Board's (RWQCB) Basin Plan, which covers the WMSP area, provides all relevant information necessary to carry out federal mandates for the antidegradation policy, 303(d) listings of impaired waters, and related Total Maximum

Daily Loads (TMDLs), and provides information relative to National Pollutant Discharge Elimination System (NPDES) and Waste Discharge Requirement (WDR) permit limits.

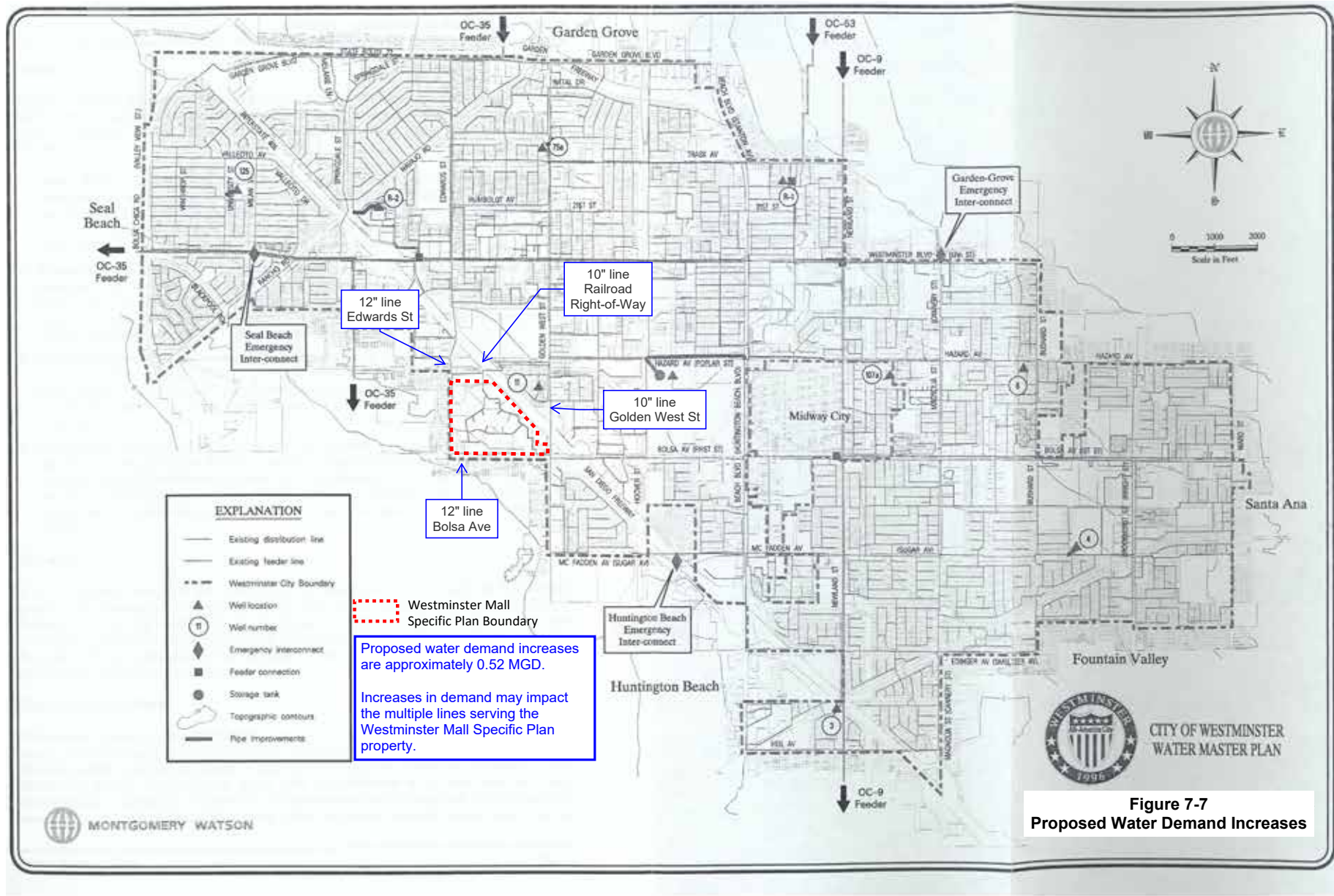
#### *Total Maximum Daily Loads (TMDLs)*

Under Section 303(d) of the Clean Water Act (CWA), states are required to identify water bodies that do not meet their water quality standards. Once a water body has been listed as impaired on the 303(d) list, a TMDL for the constituent of concern (pollutant) must be developed for that water body. A TMDL is an estimate of the daily load of pollutants that a water body may receive from point sources, non-point sources, and natural background conditions (including an appropriate margin of safety), without exceeding its water quality standard. Those facilities and activities that are discharging into a water body, collectively, must not exceed the TMDL. In general terms, Municipal Separate Storm Sewer Systems (MS4) and other dischargers within each watershed are collectively responsible for meeting the required reductions and other TMDL requirements by the assigned deadline.

#### *County of Orange MS4 Permit, Drainage Area Management Plan (DAMP) and Local Implementation Plans (LIP)*

In May 2009, the Santa Ana RWQCB reissued the North Orange County MS4 Storm Water Permit as WDR Order R8-2009-0030 (NPDES Permit No. CAS618030) to the County of Orange, the incorporated cities of Orange County, and the Orange County Flood Control District within the Santa Ana Region. Pursuant to this "Fourth-Term" MS4 Permit, the Co-permittees were required to update and implement the Drainage Area Management Plan (DAMP) for its jurisdiction, as well as Local Implementation Plans (LIPs), which describe the Co-permittees' urban runoff management programs for their local jurisdictions.

**Figure 8.7: Proposed Water Demand Increases**



Under the City's LID, land development policies pertaining to hydromodification and low impact development (LID) are regulated for new developments and significant redevelopment projects. LID Best Management Practices (BMPs) try to offset losses of natural hydrologic processes by introducing structural and nonstructural design components that restore these water quality functions into the project's land plan. These land development requirements are detailed in the County-wide Model Water Quality Management Plan (WQMP) and Technical Guidance Document (TGD), approved in May 2011, which cities have incorporated into their discretionary approval processes for new development and redevelopment projects.

The LID hierarchy requires new developments and redevelopments to implement BMPs under the LID hierarchy as described in the TGD. New projects and redevelopments within the City will follow the set hierarchy of BMP selection, and more runoff throughout the City will be effectively treated as development occurs.

### *General Construction Permit and Storm Water Pollution Prevention Plans (SWPPPs)*

The Construction General Permit (CGP), Order No. 2012-0006-DWQ, NPDES Permit No. CAS000002, last updated by SWRCB in July 2012, regulates storm water and non-storm water discharges associated with construction activities disturbing at least one acre of soil or construction activity that results in land surface disturbances of less than one acre but is part of a larger development or sale of at least one acre of soil. Construction sites that qualify must submit a Notice of Intent (NOI) to gain permit coverage or otherwise be in violation of the CWA and California Water Code.

The CGP requires the development and implementation of a Storm Water Pollution Prevention Plan (SWPPP) for each individual construction project greater than or equal to one acre of disturbed soil area (regardless of the site's Risk Level). The SWPPP must list BMPs

that the discharger will use to control sediment and other pollutants in storm water and non-storm water runoff; the BMPs must meet the Best Available Technology (BAT) and Best Conventional Pollutant Control Technology (BCT) performance standards. Additionally, the SWPPP must contain a visual monitoring inspection program; a chemical monitoring program for sediment and other "non-visible" pollutants to be implemented based on the Risk Level of the site, as well as inspection, reporting, training and record-keeping requirements. Section XVI of the CGP describes the elements that must be contained in a SWPPP. Any proposed projects (new development or redevelopment) greater or equal to one acre within the WMSP area will be subject to the CGP and SWPPP requirements.

## EXISTING GROUNDWATER CONDITIONS

### *Regional Groundwater Conditions*

The Specific Plan area lies within the Orange County Groundwater Basin (OC Basin). The OC Basin is the source of approximately 60 to 70 percent of the water supply for 2.3 million people. OCWD is responsible for managing the OC Basin. To maintain groundwater quality, OCWD conducts an extensive monitoring program that serves to manage the OC Basin's groundwater production, control groundwater contamination, and comply with all required laws and regulations. A network of nearly 700 wells provides OCWD a source for samples, which are tested for a variety of purposes. OCWD collects 600 to 1,700 samples each month to monitor Basin water quality. These samples are collected and tested according to approved federal and state procedures as well as industry-recognized quality assurance and control protocols.

The OC Basin also has prescribed beneficial uses and water quality objectives as stated in the Santa Ana RWQCB Basin Plan.

### *Local Groundwater Conditions*

The WMSP area is located on an area of shallow groundwater, approximately 5 to 10 feet below ground surface per the Orange County Technical Guidance document. Despite the presence of Type B soils and a lack of groundwater contamination within the immediate vicinity, infiltration for the project site is likely infeasible due to shallow groundwater. Site-specific borings and percolation testing will be required as a part of project-level analysis.

## **CONSTRUCTION ACTIVITIES**

Throughout construction, buildout, and operation of the WMSP, BMPs will be employed in order to protect local water resources and limit impacts from stormwater pollution. During construction, BMPs will aim to reduce sediment mobilization and prevent sediment from entering the storm drain system. Post-construction BMPs will be designed to reduce pollutants from ongoing, longer term operations based on site-specific analysis.

Clearing, grading, excavation and construction activities associated with the proposed project may impact water quality due to sheet flow erosion of exposed soils and subsequent deposition of particulates in local drainage conveyance facilities. Grading activities, in particular, lead to exposed areas of loose soil, as well as sediment stockpiles, that are susceptible to uncontrolled sheet flow. Although erosion occurs naturally in the environment, primarily from weathering by water and wind action, improperly managed construction activities can lead to substantially accelerated rates of erosion that are considered detrimental to the environment.

### *Construction General Permit*

Prior to the issuance of grading permits, the project applicants shall provide evidence that the development of the projects one acre or greater of soil disturbance shall comply with the most current Construction General Permit (CGP) and associated local National Pollutant Discharge Elimination System (NPDES) regulations to ensure that the potential for soil erosion is minimized on a project-by-project basis.

### *Construction Best Management Practices (BMPs)*

The CGP requires that a construction SWPPP must be prepared and implemented at all construction projects with one acre or greater of soil disturbance and revised as necessary as administrative or physical conditions change. The SWPPP must be made available for review upon request and shall describe construction BMPs that address pollutant source reduction, and provide measures/controls necessary to mitigate potential pollutant sources. These include, but are not limited to: erosion controls, sediment controls, tracking controls, non-storm water management, materials and waste management, and good housekeeping practice.

Prior to commencement of construction activities within the Specific Plan area, the project-specific SWPPPs will be prepared in accordance with the site-specific sediment risk analyses based on the grading plans, with erosion and sediment controls proposed for each phase of construction for the individual project. The phases of construction will define the maximum amount of soil disturbed, the appropriately sized sediment basins and other control measures to accommodate all active soil disturbance areas and the appropriate monitoring and sampling plans.



## POST-CONSTRUCTION ACTIVITIES

With the proposed land use changes, development resulting from the WMSP may result in long-term impacts to the quality of storm water and urban runoff, subsequently impacting downstream water quality. Developments similar to the proposed project can potentially create new sources for runoff contamination through changing land uses. As a consequence, the Specific Plan may have the potential to increase the post-construction pollutant loadings of certain constituent pollutants associated with the proposed land uses and their associated features, such as landscaping, roadways and residential uses.

To help prevent long-term impacts associated with land use changes, and in accordance with the requirements of the City of Westminster LIP and consistency with OC DAMP and Fourth-Term MS4 permit, new development and significant redevelopment projects must incorporate LID/site design and source control BMPs to address post-construction storm water runoff management. In addition, projects that are identified as priority projects are required to implement site design/LID and source control BMPs applicable to their specific priority project categories, as well as implement treatment control BMPs where necessary. Selection of LID and additional treatment control BMPs is based on the pollutants of concern for the specific project site and the BMP's ability to effectively treat those pollutants, in consideration of site conditions and constraints. Further, priority projects must develop a project-specific Water Quality Management Plan (WQMP) that describes the menu of BMPs chosen for the project, as well as operation and maintenance requirements for all structural and any treatment control BMPs.

Since the Specific Plan area does not include a specific or detailed development plan, and the Specific Plan will likely consist of multiple phases of development, a project-specific WQMP will not be developed for the Specific Plan at this time. Future project-specific WQMPs, preliminary and/or final, will be prepared consistent with the prevailing

terms and conditions of the City's LIP, OC DAMP, and Model WQMP at the time of project application. Moreover, LID and water quality treatment solutions prescribed in project-specific WQMPs shall be designed to support or enhance the regional BMPs and efforts implemented as part of City-wide efforts to improve water quality.

In accordance with the Fourth-Term MS4 Permit for North Orange County, the LID BMPs shall be sized and designed to ensure on-site retention of the volume of runoff produced from a 24-hour 85th percentile storm event, as determined from the County of Orange's 85th Percentile Precipitation Map. The 2011 Model WQMP and its companion Technical Guidance Document provides design criteria, hydrologic methods and calculations for combining use of infiltration, retention, and biofiltration BMPs to meet the required design capture volume.

In accordance with the MS4 Permit and Technical Guidance Document, the use of LID features will be consistent with the prescribed hierarchy of treatment provided in the permit: infiltration, evapotranspiration, harvest/reuse and biotreatment. As the WMSP project site is located in an area of shallow groundwater, infiltration will likely be infeasible. However, site-specific studies will be conducted during final design to confirm feasibility. If infiltration is confirmed infeasible, harvest and reuse of stormwater will undergo a feasibility analysis as part of project-level planning. If infiltration, harvest and reuse are found to be infeasible, biofiltration will be used to treat on-site runoff.



# Chapter 9

# **Administration and Implementation**





CAFFI PRIMO

## 9.1 GENERAL ADMINISTRATION

### 9.1.1 AUTHORITY

The City of Westminster initiated and prepared the Westminster Mall Specific Plan pursuant to the provisions of California Government Code, Title 7, Division 1, Chapter 3, Article 8 (Sections 65450 through 65457). The law allows the preparation of specific plans as required for the implementation of the general plan. Specific plans act as an implementing mechanism between the General Plan and individual development proposals.

The WMSP is the regulatory document guiding land use and development within the boundaries identified in the Specific Plan. Upon adoption by ordinance, the WMSP will serve as zoning for the properties involved. It establishes the necessary plans, development standards, regulations, infrastructure requirements, design guidelines, and implementation programs on which subsequent project-related development activities are to be based. The implementation of all projects within the WMSP are subject to the provisions of this section and Chapter 17.565 of the Westminster Municipal Code. It is intended that local public works projects, design review plans, detailed site plans, grading and building permits, or any other action requiring ministerial or discretionary approval applicable to this area will be consistent with the WMSP.

### 9.1.2 INTERPRETATION, CONFLICT, AND SEVERABILITY

#### INTERPRETATION

In case of uncertainty or ambiguity to the meaning or intent of any provision of the WMSP, the Community Development Director has the authority to interpret the intent of the provision. Interpretations may be referred to the Planning Commission for consideration and action at the Community Development Director's discretion. All interpretations made by the Community Development Director may be appealed to the Planning Commission in accordance with the appeal procedures in the City of Westminster Municipal Code (WMC).

#### CONFLICT

In the event of a conflict between the provisions of the WMSP and the provisions in the WMC, the Specific Plan shall prevail. For permitted uses, the provisions of Table 4.1, *Permitted Land Uses* shall apply, and if Table 4.1 is silent on or does not list the use, it is prohibited. The Community Development Director is authorized to provide administrative determinations and interpretations regarding uses, development and design standards and design guidelines in the Specific Plan. Such administrative determinations must be in writing and may be appealed in accordance with the provisions of the WMC. For any other topical issue not addressed or otherwise specified in the WMSP, the Community Development Director has the responsibility and authority to make an interpretation based upon the intent of the provisions of the Specific Plan. The most appropriate or closely matching code section and land use type or procedure will be determined. In summary:

**Permitted Uses:** Where the Specific Plan is silent, the use is prohibited (if it's not listed in Table 4.1, it is not permitted).

**Development Standards and other Topical Issues:** In cases where this Specific Plan is silent, the Community Development Director will make an interpretation based on the intent of the provisions of the Specific Plan.

**Definitions:** The WMSP provides a Glossary in Appendix A to clearly define how particular items are defined in this Specific Plan. In some cases, items are defined differently than in the WMC, and the definition associated with the Specific Plan will take precedence. Where the WMSP is silent on a definition, the definitions listed in the WMC shall be used.

#### SEVERABILITY

If any chapter, subsection, sentence, clause, or phrase of the WMSP, or future amendments or additions hereto, is for any reason held to be invalid or unconstitutional by the decision of any court, such a decision shall not affect the validity of the remaining portions of the WMSP.

## 9.2 REVIEW AND APPROVAL PROCESS

The intent of section 9.2 is to streamline subsequent project approvals by the appropriate City staff. Table 9.1 shows the approval level for anticipated development within the WMSP.

### 9.2.1 PRELIMINARY PLAN AND PRE-APPLICATION MEETING

For the City to effectively administer the WMSP, expeditiously review development applications, ensure project coordination, and provide timely information regarding improvements, status of the trip budget and other aspects of the WMSP, prior to submitting a development application for new construction, applicants are required to participate in a two part pre-application process:

**Preliminary Plan Review** – requires project-level comments, including WMSP consistency, from various City departments and agencies. A preliminary plan application can be obtained from the Planning Division.

**Pre-Application Meeting** - requires coordination and screening of application submittal materials prior to the time an application is formally submitted and/or receive guidance on necessary next steps. Application materials may be submitted to the City in advance of a scheduled meeting with City staff, though a project-level review will not be performed.

### 9.2.2 APPROVAL AUTHORITY

The responsibilities of the Community Development Director shall include administering, interpreting, and enforcing all requirements and standards of the WMSP, including the acceptance and processing of all land use permit applications.

The Community Development Director may approve or conditionally approve applications that meet the requirements of the WMSP consistent with the approval type and level as well as the entitlement processes identified in Tables 4.1, *Permitted Land Uses*, and 9.1, *Approval Type and Level*. The Community Development Director holds final approval authority for and enforcement of building permits, certificates of occupancy, sign permits, and temporary use permits. Decisions of the Community Development Director may be appealed to the Planning Commission (WMC Section 17.640.010). At the Community Development Director's discretion, any development request may be taken directly to the Planning Commission for their input or action.

The Planning Commission may recommend approval, conditional approval, or denial of some projects, and consider the appeal of the actions of the Community Development Director. The Commission may also make recommendations to the City Council regarding changes to the WMSP.

The City Council may approve, conditionally approve, or deny any project within the WMSP brought before them.

For all other actions not listed in Table 4.1 or Table 9.1 (i.e., tentative tract map, etc.), refer to the WMC.

### 9.2.3 CONSISTENCY WITH VISION AND OBJECTIVES

All applications for development within the WMSP must demonstrate that they are consistent with and advance the ideas set forth in the Westminster Mall Vision and Objectives outlined in Section 1.3. Project applications requiring entitlements shall include a narrative illustrating the project's compliance with the concepts outlined in each objective.

**Table 9.1: Approval Type And Level**

Action	Comm. Dev. Director <sup>1</sup>	Planning Commission	City Council
Interpretation of Specific Plan Requirements	X		
Tenant Improvements (Internal Only)	X		
Master Sign Program (Section 5.2.23)	X		
Project Sign Program (Section 5.2.23)	X		
Signs Consistent with Master Sign Program	X		
Modification of Retail Overlay Boundary (Section 9.2.4(B)5 and Section 9.4)	X		
Small additions (Under 2,500 sq. ft.)	X		
Outdoor seating (See Table 4.1; Eating and Drinking Establishments)	X		
Change in Occupancy (Between Permitted Uses)	X		
Façade Improvements (Fronting, abutting, or adjacent to Bolsa/Edwards/Freeway)		X	
Façade Improvements (Not fronting, abutting, or adjacent to Bolsa/Edwards/ Freeway)	X		
Temporary Outdoor Events	X		
Carts and Kiosks	X		
New construction (Fronting, abutting, or adjacent to Bolsa/Edwards/Freeway)		X	
New construction (Not fronting, abutting, or adjacent to Bolsa/Edwards/Freeway)	X		
Request for a Reduction in Parking or Shared Parking (see Sections 6.8.2 and 9.2.5)	City Traffic Engineer		
Change in Occupancy (To Conditional Use )		X	
Expansion of Non-Conforming Use		X	
Administrative Adjustments to the Specific Plan	X		
Condominium Conversion			X

Note:

<sup>1</sup> Community Development Director may advance any Director-level decision to the Planning Commission

## 9.2.4 ADMINISTRATIVE ADJUSTMENTS AND AMENDMENTS

Approval of the WMSP indicates acceptance by the City Council of a general framework for development within the WMSP boundaries. Part of that framework establishes specific development standards that constitute the zoning regulations for the WMSP. It is anticipated that certain modifications to the Specific Plan text, exhibits, and/or project may be necessary during the development of the project. Amendments may be requested at any time pursuant to Section 65453(a) of the Government Code. Depending upon the nature of the request, environmental analysis may be required, pursuant to the CEQA Guidelines. All modifications to the WMSP shall occur in accordance with the amendment process described below:

### (A) MINOR ADMINISTRATIVE ADJUSTMENTS

Minor Administrative Adjustments may be granted to development projects requesting minor adjustments or variations from the plans and standards of the WMSP that are consistent with the vision of the WMSP, are in substantial conformance with the intent of the Specific Plan provisions, and do not result in additional potentially significant environmental impacts as defined by CEQA. A minor administrative adjustment may be granted up to three times for a project, but only once for a specific type of request per project. For projects that require more than three administrative adjustments, Planning Commission review and approval is required

A request that exceeds the limitations identified below shall require the filing of an application for a variance or major amendment of the WMSP. Minor Administrative Adjustments shall be reviewed by the Community Development Director for final determination pursuant to Chapter 17.555 of the WMC and may be approved, conditionally approved, or denied without a formally noticed public hearing unless it is appealed in compliance with Chapter 17.640 of the WMC.

The following may be considered for Minor Administrative Adjustments:

- » Minor changes to the design of the roadway cross-sections and location of vehicle ingress and egress, provided that the streets and drives have adequate capacity to handle the anticipated volumes of traffic and the design changes are supported by the City's Traffic Engineer.
- » Minor adjustments that are 10% or less of the development standards or regulations as provided in Chapter 5 of the WMSP.
- » Adjustments to the alignment, location and sizing of utilities and facilities that comply with the applicable plans and standards of the agency responsible for such utilities and facilities.
- » Adjustments to the conceptual phasing program, provided that public infrastructure improvements and services necessary to serve the development are available or will be provided concurrently with the development.
- » Revisions to graphics and text that do not significantly change the intent of the WMSP.
- » The addition of permitted uses that do not change or exceed the maximum development intensities permitted by the WMSP and are consistent with the vision of the WMSP, as deemed appropriate by the Community Development Director.
- » Any modifications deemed by the Community Development Director to be minor modifications that are in keeping with the intent of the WMSP and are in conformance with the City's General Plan.

The Community Development Director shall record the determination in writing with findings on which the determination is based. The Minor Administrative Adjustment may be approved, with or without conditions, after making all the following findings:

1. Approval of the Administrative Adjustment and minor WMSP Modifications would not alter the mixed-use development vision, maximum density/intensity, circulation pattern, or conditions of approval as set forth in the WMSP, Development Agreement, or other related approvals.



2. Approval of the Administrative Adjustment and minor WMSP Modifications would not be detrimental to the public health, interest, safety, or general welfare and would not be detrimental or injurious to property or improvements in the vicinity and in the same zoning district.
3. The project is consistent with the City's General Plan and complies with all other applicable provisions of the WMSP.

## (B) MAJOR AMENDMENTS

Major Amendments to the Specific Plan are modifications or amendments that change the intent, provisions, or development standards of the WMSP. Major amendments shall constitute the following:

### *SPECIFIC PLAN*

- » Amendments to the Specific Plan that would result in an amendment to the City's adopted General Plan.

### *DEVELOPMENT PROJECTS*

- » Amendments to an approved Development Agreement.
- » Changes to uses and development intensities that exceed the Maximum Development Intensities permitted by the Specific Plan.
- » Any changes in development intensity that would result in substantial changes or potentially significant impacts not considered by the EIR.
- » Any amendment that changes the retail preservation policy requirement of 600,000 square feet is considered a major amendment.
- » Any amendment that changes the acreage of open space below 9.5 acres is considered a major amendment.

The City Council, property owner, or project applicant shall have the authority to initiate the process to amend the WMSP at any time. An amendment to the City's General Plan shall not be required unless it is determined by the City of Westminster that the proposed WMSP amendment would substantively affect the General Plan goals, objectives, policies, or programs for the WMSP.

## (C) VARIANCES

A variance is a discretionary entitlement that may waive or relax the development standards of this Specific Plan. Variances to the WMSP shall be considered consistent with Section 15.555 *Administrative Adjustments and Variances* of the City of Westminster Municipal Code.

## 9.2.5 SHARED PARKING AND PARKING MANAGEMENT

Shared parking and parking management may be employed within the WMSP to provide parking for all land uses. However, there is no requirement for property owners to enter into shared parking agreements. In accordance with Table 9.1, *Approval Type and Level*, all new shared parking agreements must be reviewed by the City Traffic Engineer. Covenants, Conditions, and Restrictions (CC&R's) for separate projects in the WMSP Area must address the shared nature of parking and it is recognized that more than one set of CC&R's may be prepared for the overall site. Mechanisms shall be instituted in the residential CC&R's that clearly define shared parking relationships and how the parking will be managed throughout the project.

Parking management will entail a combination of providing physical spaces for parking and managing those spaces so that they are properly allocated to and used by residents, visitors, and businesses. Parking management can help prevent situations such as permanent residents using guest parking spaces instead of the enclosed parking spaces allocated to them.

## 9.3 WMSP TRIP BUDGET

A design goal of the WMSP is to ensure that new vehicle trips can be accommodated within the right of way of Edwards Street and Bolsa Avenue. A Vehicle Trip Tracking System will be created and maintained by the City that will establish a vehicle trip budget. All new projects will be evaluated to ensure that the new vehicle trips remain within the budget.

The trip budget system will be accomplished through a two step process:

1. A Trip Budget will be established using information from the WMSP environmental document that will identify the number of new trips that are assumed to be generated by the project.
2. A Trip Generation Estimate will be established for each subsequent development action that will estimate the number of new trips generated by that project and how much of the WMSP Trip Budget is utilized by the development action. This approach will ensure that, as the WMSP redevelops, the total number of trips on the site are consistent with what was used in the EIR

Prior to submittal of any development application, the applicant shall obtain the trip budget tracking worksheet from the City to estimate PM Net New Peak Hour Trips to ensure there is remaining capacity for approval of new uses or a change of use. It is required that an applicant use the City budget tracking worksheet to estimate trips prior to completion of a site-specific traffic study if required.

### 9.3.1 WMSP TRIP BUDGET CAPACITY

The WMSP Trip Budget Tracking System will be used to regulate the total allowable residential and non-residential uses generated by new development, modification to existing development, or changes of use that are permitted in the WMSP.

The WMSP Trip Budget establishes a baseline capacity of PM Peak Hour Trips for the Specific Plan and controls the amount of development

on the project site. The EIR identifies the maximum number of trips allowed for the site which is based upon the maximum 3,000 dwelling units and 1.2 million square feet of non-residential assumed for the project and as identified in Chapter 3, *Land Use and Community Structure*.

The purpose of the tracking system is to allow some flexibility in the types of uses that can be developed in the project area (without an amendment to the Specific Plan) so long as the overall development proposed for all projects combined stays within the total Daily and PM Peak Hour Trips assumed for the Specific Plan. Of specific interest to the City is ensuring that the overall trips are within the budget of trips covered in the environmental document (covering Transportation, GHG, Air Quality, Noise, and Energy sections) and maintaining the goal of not exceeding the available capacity of the adjacent transportation network.

The City Traffic Engineer will review proposed projects for their compliance with the following:

1. Consistency with the uses permitted in Table 4.1, *Permitted Uses of the WMSP*;
2. The ability to demonstrate, through the preparation of a Trip Generation Estimate, that there are enough trips remaining in the WMSP Trip Budget to serve the remaining project elements and that the overall WMSP Trip Budget will not be exceeded; and
3. The ability to demonstrate consistency with the allocation of PM Peak Hour Trips calculated as part of the Baseline Trip Budget (unless consistent with Trip Transfer provisions).

Existing and Net Trips associated with each proposed project shall be provided to the City to ensure enough trip capacity remains to accommodate projects proposed later in the implementation of the Specific Plan. Existing and Net Trips must also be provided at the site access points to track how trips from the Specific Plan access the site and verify that it is consistent with the EIR assumptions (this also will provide information to identify if one area of the site is developed at a

higher trip intensity than what was assumed in the EIR). The following outlines this process:

1. Estimation of existing trips generated by the site (derived using trip generation and allocation estimates on a project-by-project basis or by collecting traffic counts at the project driveways).
2. Total number of new PM Peak Hour Trips generated by the project consistent with the methods used to evaluate the Specific Plan Area trip generation.
3. Description of any Transportation Demand Management (TDM) measures that have been applied to the calculations.
4. Overview of on-site circulation plan (driveways, drive aisles, and any other measures that illustrate internal circulation or trip capture).
5. Projects that either exceed the Trip Budget, or require modification to the WMSP, shall provide substantial evidence as part of the application to demonstrate that the proposed new trips, and associated cumulative buildout of the WMSP, do not exceed the capacity of Edwards Street and Bolsa Avenue through additional assessment.

### TRIP GENERATION AND CHANGES OF USE

Changes of use in a tenant space or area that is already constructed shall also require completion of a Trip Generation Estimate and an update to the WMSP Budget Tracking System as part of any building permit issuance and must be approved prior to issuance of the Certificate of Occupancy for the use.

### 9.3.2 DETERMINING THE BASELINE NUMBER OF TRIPS ALLOCATED TO A SITE OR PROJECT (BASELINE TRIP BUDGET)

Since the WMSP EIR is based on Net New PM Peak Hour Trips, the City's WMSP Trip Budget Tracking System also includes estimates for existing trips to determine the total amount of development that is permitted on a site. The Budget Tracking System can utilize either these trip estimates, updated existing trip generation at the site using more current trip generation data (described in the trip generation estimate), and/or utilize driveway counts for estimating existing trips on the system, whichever is greater. Total trips by area can be estimated by adding the existing trips from each site to the Net Trips. Using these estimates, a total of 54,490 Daily and 3,562 Gross PM Peak Hour Trips are available for use within the WMSP.

#### DETAILED TRIP CALCULATION

After an applicant estimates the baseline number of trips that are available for use on their site, they will be required to prepare a Trip Generation Estimate that will be incorporated into the trip budget tracker that illustrates trips generated by each use proposed on a site. Trips shall be calculated by multiplying the total number of units, hotel rooms, or square footage for each nonresidential use by unique trip generation multipliers to determine the total number of daily and PM Peak Hour Trips proposed with each project. Trip generation should be developed using the same methodology and trip rates used for the Specific Plan EIR or similar trip generation reference approved by the City Traffic Engineer.

Trips associated with each project or change of use will be subtracted or added to the total number of trips assigned to the mixed-use areas in the WMSP Trip Budget Tracker.

If the detailed Trip Calculation for the proposed project results in the use of fewer trips than the number allocated to a site using the baseline number Trips, the remaining trip allocation shall remain available for reallocation and distribution across future development in any other areas within the Specific Plan at the discretion of the City.

### DETAILED TRIP ASSIGNMENT

In addition to the trip generation tracking system discussed above, each development shall provide a trip assignment to the access points of the project site. This will be added or subtracted to the existing volumes at that location (using either traffic counts at the site driveways or through a project-by-project tracking system) to ensure that the project is consistent with the access and distribution assumptions in the Specific Plan EIR.

### EXCLUSIONS

Parking structures, mechanical rooms, hallways (not including lobbies), and elevators shall not be included in the calculations for trip budgeting and tracking.

### 9.3.3 TRIP BUDGET TRACKING - ADMINISTRATION

It is the responsibility of the City of Westminster to maintain the official WMSP Trip Budget Tracking System. Approval of the number of trips assigned to each project is required by the City Traffic Engineer prior to issuance of any building permit for new units, hotel rooms, or nonresidential square footage (including parks).

Since traffic trip generation rates may vary over time, the formulas and totals used in the WMSP Trip Budget Tracking System may be updated periodically by the City. Any changes to trip generation rates or Trips shall be documented in the WMSP Trip Budget Tracking System worksheet. Internalization of trips due to project design (internal trip capture) or TDM strategies applied shall be incorporated into the analysis using best practice methodologies.

The City may review the trip budget information periodically and make updates as needed (as it monitors the effectiveness of the TDM program, for example). In instances where the area generates fewer trips than assumed in the CEQA document, the trip budget tracking shall be updated to reflect available trips due to new or refined information or the effectiveness of the TDM program.

## 9.4 RETAIL PRESERVATION POLICY

The Retail Preservation Policy (RPP) will ensure the WMSP includes substantial retail offerings, either by maintaining existing or providing new non-residential space. In particular, the RPP implements the City goal of maintaining a minimum of 600,000 square feet of retail space in the Specific Plan Area, which constitutes approximately half of the pre-existing Westminster Mall building space (and a lower proportion of all pre-existing retail space in the WMSP Area).

### APPLICATION

To support the placemaking goals of the WMSP, a portion of the Specific Plan Area is designated as a Retail Overlay Area, as shown in Figure 3.12. The Retail Overlay Area is designed to accommodate the bulk of WMSP’s visitor offerings and as such focuses on the construction of new or preservation of existing retail or hotel space. The RPP prohibits residential or office projects in the Retail Overlay Area unless they are incorporated into the second story and above of a vertical mixed-use building. To maintain flexibility over time, the boundaries of the RPP may be modified if an “equivalent” amount of retail or hotel land or buildings are designated in a location that is contiguous to the pre-existing Retail Overlay Area.

Through the build out of the WMSP Area, the City’s Community Development Department will maintain a map of all retail and hotel projects as construction is completed or the building is permanently preserved. These sites will remain subject to the RPP unless a WMSP amendment is applied for and processed through the City’s standard entitlement process for specific plans. Any retail or hotel space that is developed or permanently preserved in conjunction with the RPP must also comply with the Land Use and Design Guidelines articulated elsewhere in the WMSP. For example, eligible retail space must be in areas of the Plan that support or allow retail development.

When retail or hotel projects are constructed, or existing buildings are preserved for retail purposes, the amount of Gross Leasable Area in the new or permanently preserved buildings will be subtracted from the 600,000 square feet of retail space sought by build-out of the WMSP. Once 600,000 square feet of retail or hotel space have been built or permanently preserved within the WMSP, residential and office only projects will be allowed in the Retail Overlay Area. A summary of the RPP land use scenarios and requirements is provided in Table 9.2.

### EQUITABLE REIMBURSEMENT OBLIGATION

To support the implementation and an equitable application of the RPP, the City will initiate and administer a “Equitable Reimbursement Obligation” (EROP) program. This program will create a payment mechanism that applies to all WMSP residential property owners/developers to ensure they contribute to the RPP, even if their property is not located within the Retail Overlay Area. The goal will be to ensure that any financial burden to property owners who develop or preserve retail or hotel uses will be shared among all WMSP property owners/developers that choose to develop their sites with residential projects, and not just those property owners located within the Retail Overlay Area. The Retail Preservation Policy equitable adjustment component of the Multi-Owner Equitable Reimbursement Obligation Program does not apply to owners/developers of office or medical office projects.

The specific structure, procedures, and methodology to implement the Equitable Reimbursement Obligation program will be developed after approval of the WMSP (see *Funding Policies and Implementation Actions: Immediate Action 3.1* in Section 9.8.3). It is expected that the City will collect required reimbursement payments on new residential development at set milestones (maps or permits issued) and then pass the revenue through to owners eligible for reimbursements (i.e., those that develop and/or permanently preserve retail or hotel space). The reimbursement obligation will be roughly equal to the difference in the surplus development value between retail and residential land uses (recognizing that the comparable value of these land uses may

**Table 9.2: Retail Preservation Policy Land Use Scenarios**

Scenarios	Requirements
Developer seeks to build or permanently preserve retail or hotel inside the Retail Overlay Area	Allowed if consistent with WMSP Land Use and Design Guidelines. Applicant eligible for credit through the Equitable Reimbursement Obligation Program to be developed pursuant to the RPP.
Developer seeks to build or permanently preserve retail or hotel outside Retail Overlay Area	Allowed if consistent with WMSP Land Use Requirements, Development Standards, Placemaking Concepts, and Design Guidelines. Applicant eligible for credit through Equitable Reimbursement Obligation Program
Developer seeks to build residential only outside Retail Overlay Area	Allowed upon payment of applicable “Equitable Reimbursement Obligation” (see description below)
Developer seeks to build vertical mixed-use retail with residential or office above first floor inside the Retail Overlay Area.	Residential and/or office is allowed on the second floor or above only. Ground floor uses within the Retail Overlay Area shall be bona fide retail establishments covering at least 75 percent of the street frontage and with a minimum depth of 45 feet.
Developer seeks to build residential only inside Retail Overlay Area	Not allowed unless the Retail Overlay Area is modified as described below.
Developer seeks to build residential only on a site contiguous to the Retail Overlay Area, and the project area overlaps into the pre-existing Retail Overlay Area	<p>Not allowed unless Community Development Director approves a modification to the Retail Overlay Area boundary (see Table 8-1 for required planning approvals). All modifications to the Retail Overlay Area must be contiguous to the pre-existing boundary and meet one of the following criteria:</p> <p>(1) Contain the same amount of land area as the proposed portion of the residential only project that intrudes into the pre-existing Retail Overlay Area,</p> <p>or</p> <p>(2) Accommodates an amount of retail or hotel space that is “equivalent” to the proposed portion of the residential only project that intrudes into the pre-existing Retail Overlay Area (see equivalency factors defined herein). Under this scenario the City will issue a Certificate of Occupancy (COO) for the residential project only after:</p> <p>(a) issuing COO for an “equivalent” amount of new retail or hotel space in the modified Retail Overlay Area;</p> <p>or</p> <p>(b) an “equivalent” amount of existing retail space is permanently preserved by the property owner.</p>
Developer seeks to build non-residential buildings, other than retail or hotel, inside Retail Overlay Area	Not allowed

change over time). Property owners or developers who construct (or permanently preserve) both retail / hotel and build residential will get a credit or offset against their Reimbursement Obligation based on the equivalency factors defined herein.

Prior to the City's establishment of a Multi-Owner Equitable Reimbursement Obligation program (EROP) that is final, binding, and no longer subject to administrative appeal or legal challenge, any application for a project outside of the Retail Overlay area shall have a condition of approval imposed on land use entitlements that Developer will join the EROP and pay the project's fair share contributions prior to issuance of a Certificate of Occupancy on the first building in the project

## DEFINITIONS

For the purposes of implementing the RPP, the following definitions will apply:

1. **Retail Space:** Retail space is defined as any building area designed to accommodate business operators that sell consumer goods and services directly to the public; including, but not limited to, general retail, restaurants, and entertainment venues. The space will be open to the public during business hours on a "walk-in" basis, although membership and/or reservations may be allowed. The term "Retail Space" shall also apply to the variant "retail" and "retail square footage" wherever such terms are used in the WMSP. Ground-floor elements of permissible second-floor uses within the Retail Overlay Area (e.g., residential lobbies) maybe allowed, but will not be included as bona fide retail space (or contribute to meeting the retail preservation targets described herein).
2. **Hotel Accommodations:** Hotel space is defined as any building area (including without limitation lobby and amenity space) that provides for visitor overnight accommodations, except for short-term rental lodging within residential units. The term "Hotel Accommodations" shall also apply to the variant "hotel space."
3. **Gross Retail Leasable Area:** The retail preservation requirement will be calculated based on the gross leasable building square footage for the retail portion of a building. For the purposes of this calculation the gross retail leasable area will be no more than 120 percent of the net leasable area. Net leasable area refers to floor space that can be rented directly to tenants and usually excludes common areas such as lobbies, hallways, and/or building utility or storage areas. The gross retail leasable area will also exclude portions of a building devoted to land uses other than retail or hotel (e.g., it excludes the residential or office component of a mixed-use building). The term Gross Retail Leasable Area shall also apply to the variant "Gross Leasable Area" wherever such term is used in Section 9.4 of the WMSP.
4. **Land Use Equivalency Factors:** The RPP establishes land use equivalency factors that equate residential to retail and/or hotel development. The equivalency factors are designed to ensure an adequate amount of retail or hotel space is provided along with residential development. The equivalency factors are defined as follows:
  - a. **Retail to Residential:** Each residential unit will be deemed equivalent to 200 gross square feet of retail. This ratio is based on the target of 600,000 square feet of retail at full build-out of the WMSP which allows up to 3,000 housing units (600,000 square feet divided by 3,000 units).
  - b. **Hotel to Retail:** Each hotel room will be deemed equivalent to 1,000 gross retail square feet. Any retail space integrated into a hotel project, such as a restaurant or gift shop, will be calculated and credited as qualifying retail space.
  - c. **Residential to Hotel:** Each residential unit will be deemed equivalent to 0.2 hotel rooms. This ratio is mathematically derived from the relationship between hotel and retail space.

## 9.5 ENVIRONMENTAL CLEARANCE

The EIR for the WMSP has been prepared as a Program EIR (PEIR), as defined by Section 15168 of the CEQA Guidelines. All subsequent development projects that are within the scope of this PEIR may be approved following a streamlined environmental review process, as guided by the provisions of CEQA. This approach is consistent with the tiering provision in California Public Resources Code Section 21083.3 and CEQA Guidelines Section 15183 for “Projects Consistent with a Community Plan, General Plan, or Zoning.” This tiering opportunity is only available for plans (e.g., specific plan) for which an EIR has been prepared.

Note that tiering under these provisions will require City review and project documentation to substantiate that a development project does not result in any new potentially significant environmental impacts. Such review could be documented in the form of one or more technical reports, a staff analysis comparing the project to the assumptions in the PEIR, or possibly an Initial Study to ensure topic-by-topic review and substantiation.

An assessment of the appropriate level of environmental review will be conducted by the City at the time of application submittal. This review will either determine that a project is within the purview of the EIR or if additional technical studies are required to determine the appropriate level of CEQA review. The extent of documentation will depend on the anticipated environmental impacts of the proposed project. If consistency with the WMSP and EIR has been substantiated, and the review shows that the project would not result in new significant environmental impacts, no further environmental review is required.

Projects may also be exempt from further environmental analysis under CEQA pursuant to other sections of CEQA (e.g., exemptions for residential infill projects, statutory exemptions, or categorical exemptions).

The project site is within a Southern California Association of Governments (SCAG) High Quality Transit Area (HQTA) which also

qualifies future projects for a limited environmental review process. CEQA Guidelines Section 15183.3 allows eligible projects to streamline the environmental review process by limiting the topics subject to review at the project level. Public Resources Code Sections 21099 and 21155.4 also limit review of environmental topics and exempt certain types of projects.

The type of CEQA review needed for each project will be determined by the City staff during their review of the type of project or development proposed.

## 9.6 SPECIFIC PLAN COST RECOVERY

Pursuant to California Government Code Section 65456, after adopting a specific plan, a city may impose a specific plan fee upon persons seeking governmental approvals which are required to be consistent with the specific plan including changes to projects, occupancies, modifications, additions, and provisions beyond the original development. The fees shall be established so that, in the aggregate, they defray but do not exceed the cost of preparation, adoption, and administration of the specific plan. The fee charged shall be a prorated amount in accordance with the applicant’s relative benefit derived from the specific plan. In some instances, applications within the WMSP will require the use of outside consultant services for activities such as transportation services, trip budget or parking analyses, and affordable housing specialties. The applicant shall bear the full cost of services and technical analysis. Deposit amounts will be procured by the City.

The City may, at some point in the future, put mechanisms in place to recover costs for the preparation and implementation of the WMSP. This may be done by collecting appropriate fees as development occurs in the project area, or by cost-sharing with individual property owners or developers.



## 9.7 IMPLEMENTATION

Implementation of the WMSP will require communication between the City and property owners to allow for redevelopment of existing land uses. Because the parcels within the project boundary are owned by different property owners, it is likely that development will occur in phases. To ensure consistency with the objectives of the WMSP, the following actions must be taken to facilitate implementation of the Specific Plan objectives:

1. Facilitate ongoing collaboration between the City and private property interests to implement the concepts and objectives of the WMSP.
2. Encourage property assembly between multiple property owners to facilitate larger-scale development and building footprints which are less likely to be achieved if parcels are developed independently.
3. Establish a trip budget tracking system to ensure that maximum trip count is observed as the WMSP builds out.
4. Ensure that all development within the WMSP complies with the mitigation measures specified in the Environmental Impact Report, or as approved by the City.

### 9.7.1 PUBLIC AND PRIVATE IMPROVEMENTS

On- and off-site improvements are intended to increase the value of a property and to provide public realm improvements as described in this Plan. They can occur within the parcel boundaries or within the right-of-way adjacent to the property.

Public and private improvements constructed within the WMSP will be maintained through a combination of public and private entities, as described below. It is anticipated that roadways, landscaping, and other

improvements within the WMSP will remain under private ownership, therefore these improvements will be privately maintained.

There may be advantages to the City owning and maintaining certain backbone infrastructure roadways, utilities or other types of improvements in the plan area. The City may consider this public ownership approach if necessary to achieve benefits from efficient financing and coordination of the development of the backbone infrastructure.

City streets within the WMSP (Bolsa Avenue and Edwards Street) will be maintained by the City.

- » Landscape, lighting, and other street improvements within public streets shall be installed by the developer(s) and maintained through a landscape and lighting district or some other special maintenance district established by the City.
- » Private infrastructure improvements such as water, sewer, and storm drain facilities shall be installed by the developer(s) and maintained by the property owner.
- » Off-site infrastructure improvements such as water, sewer, and storm drain facilities shall be constructed by the developer(s) and maintained by the designated utility provider.

The City will require applicants to install or consent to on- or off-site improvements through a development agreement or as a condition of approval, on subject property or in the right-of-way adjacent to the property bounded by the centerline of the street.

## 9.8 FUNDING

This section describes the general approaches and policies for funding both the one time and ongoing costs of providing the public infrastructure, improvements and services needed to implement the Specific Plan. It is designed to ensure that new development within the Plan Area “pays its own way” and does not put a strain on the City’s limited financial resources.

While the need to improve and expand public infrastructure and services to and within the Plan area will evolve over time, it is important to ensure that adequate funding is secured prior to development. Moreover, given that the City does not have the financial wherewithal to subsidize or finance needed improvements and public services, the funding resources will need to be generated by the property owners, developers, and/or tenants within the Plan Area.

### 9.8.1 OVERVIEW OF FUNDING REQUIREMENTS

As noted, the Plan Area will be required to finance or provide in one manner or another the full costs of the improvements and services sought and/or needed to accommodate the development and land uses therein. While the specific nature and scope of the facilities and services needed may change over the life of the Plan (depending on the pace of development, technology, and other factors), the Specific Plan and other City sources provide baseline information on the applicable standards and related requirements.

Chapter 8, *Public Services and Infrastructure*, describes the type and level of improvements and services sought. The Environmental Impact Report also identifies a number of mitigation measures necessary to address various Plan impacts. Additionally, the City Park and Recreation Facilities Master Plan and Development Impact Fee Program describe the baseline service standards and facilities that will be applicable

to new development under the Specific Plan<sup>1</sup>. Finally, Chapter 5, *Development and Design Standards*, describes the on-site affordable housing goals and policies applicable to the Specific Plan.

Table 9.3, *Preliminary Project-Wide Infrastructure Cost Categories*, provides an initial and preliminary description of the primary infrastructure improvements necessary to support full development of the Specific Plan at buildout. Project applicants will need to work with the City staff to develop final cost estimates for the items in Table 9.3, and any other necessary public infrastructure, as a basis for cost allocation and financing, as described further below. It is important to note the cost categories are provided for illustrative purposes only and based on readily available information. Additionally, the items illustrated in Table 9.3 are not all-inclusive. Other financial obligations include property specific improvements (e.g., parking and affordable housing), City development impact fees and permits (as well as other districts such as school fees), streetscape and right-of-way obligations and improvements, among others.

Table 9.3 also excludes the cost of ongoing public services needed to serve the land uses in the Specific Plan, such as public safety and the maintenance of public spaces and right-of-way. While some of these costs will be covered through the expansion of existing City General Fund revenues, others are expected to require additional funding, particularly for required levels of service that exceed the City-wide norm. Property owners will be responsible for maintaining all park and open space facilities and internal streets in the Specific Plan area unless alternative funding agreements and resources have been established and secured. Financing mechanisms for such services are further described in the subsequent section.

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<sup>1</sup>As required by State law, a Citywide Development Impact Fee program should be updated periodically during the life of the Plan to ensure the facilities included (and associated costs) correspond to updated land use buildout, City capital facility and infrastructure needs, updated cost estimates, and other factors. Typically, these updates are conducted in five-year increments, although they can occur more or less frequently at the discretion of the City.

Future analysis of the items identified in Table 9.3, *Preliminary Project-Wide Infrastructure Cost Categories*, should also include the real cost or imputed value of land dedications and permanent easements associated with public infrastructure (e.g., roads and parks) within the Plan Area. Actual land costs will depend on a variety of factors, including location, timing, entitlement status, and condition, among others, and will likely to be dedicated by local property owners/developers. Nevertheless, it will be important to explicitly account for the real or imputed value/cost of land as a basis for creating a fair and equitable cost allocation among Specific Plan property owners/developers.

Actual capital facilities, public improvements, and public services will be phased over time, with costs likely to change based on a variety of factors (e.g., technology, evolving needs, market considerations, inflation, etc.). Individual development phases will stand alone functionally and aesthetically, including the provision of adequate parking if subsequent phases are not developed. For example, the first phase of development will be accompanied by traffic, utility, and TDM-related improvements, among others. (e.g., realignment and reconstruction of internal streets, frontage curb line, sidewalk, and median modifications, and configuration of a publicly accessible park open space). It will also include adequate open space and affordable housing opportunities, consistent with the standards and policies described in previous chapters.

The phasing of the infrastructure improvements depicted in Table 9.3 must ensure that any new development stands alone aesthetically and functionally, consistent with Specific Plan policies. For example, as further described in Chapter 5, the park and open space requirements include a minimum of nine and a half acres. The location and boundaries of these parks may shift as the as the buildout process unfolds so long as alternative sites of equivalent size are identified and secured. New development applicants may choose to complete more improvements than required for a given amount of development in order to realize construction/cost efficiencies, to market future phase development sites, to enhance the attractiveness of existing facilities, or for other reasons.

**Table 9.3: Preliminary Project-Wide Infrastructure Cost Categories**

Category	Amount for Specific Plan
New Roadway Improvements <sup>2</sup>	
<i>Major Internal Streets (60 ft. Width)</i>	1,400 Linear Ft. (LF)
<i>Minor Internal Streets (50 ft. Width)</i>	2,200 Linear Ft. (LF)
Subtotal	3,600 Linear Ft. (LF)
Off-site Intersection Improvement	5 Intersections
On-site Public Parks	9.5 Acres
Water Supply	Phased replacement of all on-site water conveyance
Wastewater	Phased replacement of all on-site water conveyance
Stormwater	Phased replacement of all on-site water conveyance

*Notes: Developers are responsible for right of way improvements to street frontage on existing roads. Some of these may remain in private/public ownership and are not yet determined.*

## 9.8.2 OVERVIEW OF FUNDING TOOLS AND RESOURCES

It is expected the public improvement and service costs outlined above will be funded utilizing a variety of funding mechanisms and resources. The appropriate and most effective funding mechanism will vary based on the nature of the improvement or service being delivered. Other factors relevant to these decisions include the timing (e.g., when the funds are needed), level and frequency of costs (e.g., ongoing versus one-time costs), who are the primary beneficiaries and responsible parties, and funding availability, among others.

While some degree of flexibility will be required given the uncertainty associated with securing various funding sources, the following tools and mechanisms have been identified as particularly applicable for implementation of the Plan. However, it is recognized that during the buildout of the Specific Plan, other funding sources may be identified as particularly useful or effective, and may be utilized, as appropriate. The tools and mechanisms below are presented, not as an exhaustive list, but as a preferred set given the circumstances and information available during preparation of the Specific Plan. The funding mechanisms can be used separately or in combination.

### AREA DEVELOPMENT IMPACT FEES

The City could approve an area development impact fee payable by new development to cover the cost of public infrastructure and other amenities (they cannot be used for public services or maintenance). The fee program may be enacted by the City Council through the adoption of an ordinance at a public hearing. This action would not require a public vote or landowner approval. The fee levels must be based on a “rational nexus”, or a demonstrated relationship between the amount and impact or demand attributable to the development paying the fee. This ensures an equitable allocation of infrastructure costs among property owners/developers with the WMSP and can also be used to fund “fair share” of facilities needs outside the Plan Area. Fees are usually payable at the

time of the issuance of the building permit, although other options are available (e.g., certificate of occupancy).

In the case of the Specific Plan, an area impact fee would supplement the existing Citywide development impact fees but focus more narrowly on infrastructure improvements that primarily serve Westminster Mall and potentially adjacent areas. Proceeds may be used to reimburse property owners who pay up-front costs for facilities or infrastructure needed to facilitate development on their property. Developers can also be given the option to finance the fees through a Community Facilities District (a CFD, as described in a subsequent section) or, conversely, pay fees in-lieu of participating in a CFD.

A potential deficiency of impact fees is that they are typically collected over time as development occurs. To the extent that funding is needed up-front for a particular facility, fees may not generate sufficient revenue. Additionally, programmed or expected development that does not occur when expected, or never occurs, exacerbates the initial problem by producing less funding than anticipated when the fee amount was set.

An area impact fee is particularly well suited for park and open space improvements since they can be phased in over time (these improvements are not needed prior to vertical development). An area impact fee can also facilitate a relatively equitable, “pay as you go” approach through a system of credits and reimbursements whereby property owners/developers who dedicate are compensated by those who do not.

## MELLO-ROOS COMMUNITY FACILITIES DISTRICTS (CFD)

The City or property owners could seek to form a Mello-Roos Community Facilities District (CFD) that would be authorized to levy a special tax for public facilities and services. A CFD may be initiated by the legislative body or by property owner petition and must be approved by a two-thirds majority of either property owners or registered voters (if there are more than 12 registered voters living in the area).<sup>2</sup>

A key advantage of CFDs is that they can be used to cover the cost of certain public services as well as infrastructure. The tool is particularly useful for any required up-front improvements such as utility upgrades and transportation improvements since the revenues can be used to secure tax-exempt bond proceeds. However, the facilities must be publicly owned. It could also be used to purchase and improve park land, for example to take advantage of an early site acquisition opportunity at a price below values likely to materialize after successful project development has occurred.

**Table 9.4: Potential Funding Sources and Uses (Preliminary)**

Cost Item <sup>1</sup>	Funding Mechanism						
	Existing City Fees, Charges, Etc.	Area Development Fees	Developer Dedications / Exactions	Development Agreements	Public Facilities CFD <sup>2</sup>	Maintenance and Services CFD	BID / PBID
Transportation Facilities	√	√	√	√	√		
Water Supply	√	√	√	√	√		
Wastewater	√	√	√	√	√		
Stormwater	√	√	√	√	√		
Park and Recreation Facilities	√	√	√	√	√		
Landscape and ROW Maintenance				√		√	√
Public Safety Services				√		√	√
<p><i>Notes:</i></p> <p><i>1. There may be advantages to the City owning and maintaining certain backbone infrastructure roadways, utilities or other types of improvements in the plan area. The City may consider this public ownership approach if necessary to achieve benefits from efficient financing and coordination of the development of the backbone infrastructure.</i></p> <p><i>2. Because a Mello-Roos CFD requires public dedication, it may not be appropriate for certain facilities or improvements within the Specific Plan.</i></p>							

<sup>2</sup> The boundaries of a CFD can be formed to only include “willing” property owners, with annexation requirements triggered by development. An alternative (and usually more optimal) approach is to structure the rate and method of taxation in a way that’s amenable to all parties (e.g., tax is only triggered upon development, not on existing uses). While landscape and lighting maintenance districts (LLMDs) are also used for this purpose, Mello-Roos CFDs have become increasingly popular given the higher level of flexibility related to scope of services, geography, the tax rate and method, and other considerations.

CFD special taxes are collected annually with property taxes and can be prepaid based on provisions specified in the tax formula. The special tax amount is secured by a lien against the property. While the tax rate is based on a formula that typically takes into account land use type, development intensity, and other factors (but not value), there is no requirement that it be apportioned based on the level of direct benefit each parcel receives. Among other advantages, this provision allows flexibility to alleviate cost burdens on specific classes of development (e.g., retail, affordable housing, or existing uses).

Within the Westminster Mall Specific Plan, a Mello-Roos CFD may be particularly well suited for the following facilities and services:

- » Public Facilities CFD: A public facilities CFD could be formed to finance the cost of various on and/or off-site public infrastructure and improvements needed to serve the Specific Plan. This tool could be particularly useful for any required up-front improvements such as utility upgrades and transportation improvements (e.g. traffic signals, bike and pedestrian facilities) since the revenues can be used to secure tax-exempt bond proceeds. However, CFD debt is usually less appropriate for financing up-front facilities with total costs below \$2 - \$5 million given issuance and other financing costs.
- » A public safety CFD could be formed to cover the cost of any police or fire to cover level of service costs that exceed the norm of the City as a whole.

It is important to note that a public facilities CFD can only be used to fund infrastructure that is owned or dedicated to the City. A CFD could not be used to cover the cost of roads, parks and open space, or other amenities that remain under private ownership. However, a Public Safety CFD can be approved to cover the cost of increased demand for Police and Fire services created within the Plan area regardless of ownership.

## DEVELOPER DEDICATIONS, CONTRIBUTIONS, AND EXACTIONS

Under the Subdivision Map Act, developers may be required to dedicate land or make cash payments for public facilities and infrastructure improvements required or affected by their project. Dedications are typically made for road and utility rights-of-way fronting individual properties, park sites, and land for other public facilities directly required by their projects (e.g., payments for a traffic signal).

In the case of the Specific Plan, it is anticipated that developers will be required, as a condition of approval, to dedicate the right-of-way adjacent to their properties and to make or fund necessary improvements for street frontage and utilities. To the extent that developers over-build such improvements (e.g. for economies of scale or to facilitate a continuous street design) an area wide reimbursement mechanism could be implemented. Additionally, developers may elect to provide dedications or one-time payments for other project infrastructure requirements, improvements, or mitigations in lieu of participation in one or several of the financing mechanisms identified herein. For example, developers may want to “buy their way out” of required participation in an area CFD. In such cases, the City will determine and approve the terms and conditions associated with such dedications or contributions.

## DEVELOPMENT AGREEMENTS

The City could enter into a Development Agreement (DA) with one or a combination of Westminster Mall property owners / developers that covers the terms and conditions under which a particular project may proceed.

As a legally binding contract, a DA would differ from ordinary rezoning decisions or other public approval because, in addition to binding developer to detailed plans and conditions, they also commit public entities to a specified course of action. In addition, DAs usually

incorporate vesting provisions that protect the project from future regulatory changes by the City. A DA and any amendments must be approved by the City Council.

A DA provides developers with assurances that the land use entitlements for a project will not be changed in the future, during the specified term of the DA. In return for these public considerations and assurances, the developer makes financial commitments beyond those that could be justified through typical subdivision ordinance dedications and city planning entitlement exactions and/or impact fees. The DA provides specific information on the one-time and ongoing financial obligations that will be required by all participants.

While a DA is usually a two-party agreement, multiple property owners can form a Limited Liability Corporation (LLC) as a basis for establishing critical financial obligations and related commitments both internally and/or with the local land use authority (e.g. the City). By way of example, a DA between the City and LLC covering the entire Westminster Mall ownership (or a subset of owners) could address overarching financial and entitlement related issues of mutual interest.

A DA may also be applicable to very large Specific Plan projects that include attributes that deviate a number of ways from the initial Plan (e.g., more density, different land uses). In this case, a DA could be used as a basis for securing desired infrastructure, public amenities or other community benefits, in exchange for additional entitlements above the allowable baseline.

### PRIVATE AGREEMENTS AND FUNDING

Private developers/property owners within the WMSP may establish internal agreements and financing structures to deliver the public infrastructure and amenities consistent with Plan requirements, designations and standards. Of course, the City would still have oversight of design and related requirements as a condition of project approval and would likely seek dedication of the land improvements as public right-of-way or permanent easement. However, since the City is

generally not a party to these agreements, they do not require Council approval.

Private party agreements and financial arrangements can take many forms and cover a variety of circumstances and improvements. In particular, they can be structured to ensure financial obligations “run with the land” through deed provisions and other property recordings. Such mechanisms would enable developers to set up internal reimbursement agreements that specify the terms and conditions of payments between parties, addressing the “over-sizing” of certain facilities, for example. Land exchanges, owner partnerships, and other contractual arrangements or transactions can also facilitate the funding of public improvements absent direct involvement by the City.

Privately initiated funding arrangements offer a variety of benefits, including more efficient decision making and project execution, lower development costs and reduced need for City involvement or approval. However, they can also be difficult to establish on a broad scale for area-wide financing, an important consideration within the Specific Plan area given the number of separate ownership groups. Finally, private financing may come with higher financial return thresholds relative to a tax exempt CFD bonds, for example.

### PROPERTY BUSINESS IMPROVEMENT DISTRICT (BID/PBID)

BIDs or PBIDs can provide funding for both facilities and services sought by property owners and/or tenants within a particular district. BIDs and PBIDs are typically organized and funded by property owners or tenants and can be dissolved by the same group. Consequently, they are most appropriate for services or facilities that are over and above those sought or required by the City. Common improvements and services funded by BIDs include streetscape enhancements, “clean and safe” services (e.g. private security), special events and marketing activities, and other local enhancements sought by tenants or property owners.

### 9.8.3 SUMMARY OF POTENTIAL FUNDING SOURCES AND USES

Table 9.4, *Potential Funding Sources and Uses*, presents a matrix linking the infrastructure and public service costs within the Specific Plan to the most applicable funding sources described above. As shown, there are multiple options for funding many of the development costs. In addition, funding sources can be combined and / or used to replace others (e.g. “buy-down”) over time. The most appropriate funding source(s) will depend on the unique circumstances that apply to individual projects and may evolve over time. In addition, the approach will be guided by the funding policies and implementation actions described in the following sections.

## 9.9 FUNDING POLICIES AND IMPLEMENTATION ACTIONS

The following policies and actions will govern the funding of private and public improvements, infrastructure, and public facilities for the Specific Plan. The policies, provided first, are intended to guide the subsequent actions including requisite studies or findings; formation and adoption of financing entities and mechanisms; and other approaches, approvals and programs. The Immediate Actions correspond to the Short-Term period, as described in the subsequent WMSP Phasing section.

**Funding P1. The land uses within the Specific Plan shall pay the full costs of capital facilities, infrastructure improvements, maintenance and public services, and other requirements needed to support and serve the Plan Area and mitigate the impact of development on other parts of the City.**

**Immediate Action 1.1: Establish Initial Phasing Assumptions.** Prior to the approval of planning entitlements for individual development projects on privately owned properties, the City will prepare, at the applicants’ expense, a cost analysis that identifies all public

infrastructure and service requirements needed to serve their project and their respective costs. The infrastructure and service cost estimates included in this report will be based on the best information available and will be updated periodically during various phases of the Specific Plan development process. This cost analysis should account for project-specific circumstances, changing market conditions, and be updated as more refined facility and cost data become available.<sup>4</sup>

**Immediate Action 1.2: Establish Area Development Impact Fee Program.** The City will establish an Area Development Impact fee to address the applicable cost categories identified in Table 9.3 to fund the expansion of infrastructure and public facilities necessary to serve the area. An Area Development Impact Fee Program will be adopted by the City Council and paid at the issuance of building permits for any new building space within the Plan Area.<sup>5</sup> The fee will be updated periodically to account for changes in infrastructure needs and costs.. The fee program will specify city policies and procedures for credits, reimbursements, and waivers, if any.

At the request of property owners and/or developers, the City may consider other financing tools and sources to provide for needed capital improvements, including, without limitation, participation in public facilities CFDs, direct financial contributions and dedications, and/or other mechanisms described in this Chapter. These alternative mechanisms may replace and / or supplement Area Impact fees, subject to City Council approval. Development shall also be required to pay existing city-wide impact fees and provide additional project or site-based dedications.

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<sup>4</sup> For projects that proceed under a Development Agreement (DA) the specific methodology for addressing cost escalation, whether the result of inflation or other factors, will be addressed therein.

<sup>5</sup> Any building permits sought prior to adoption of the Area Impact Fee Program will deposit into an escrow account controlled by the City an amount equal to a preliminary estimate of the fee amount plus 10 percent. The City will refund any overpayment once the Fee Program is finalized (conversely, applicant will be responsible for any underpayment).



**Immediate Action 1.3: Establish Affordable Housing In-Lieu Fee Program.** The City will establish an optional Affordable Housing In-Lieu Fee applicable to residential projects that opt out of the inclusionary housing requirements described in Chapter 5. The fee will be designed to fully offset the costs of developing affordable housing elsewhere in the Specific Plan area, consistent with the policies described in Chapter 5, and the revenue would be solely used to advance these projects. The in lieu fee will be applicable to projects of 30 or less. In addition, projects larger than 30 units can use the in lieu fee for up to 2 units, plus fractional units.

**Immediate Action 1.4: Establish Public Right-of-Way and Public Access Area Maintenance Agreement(s).** As a condition of project approval, the City will require that developers and/or property owners execute an Agreement with the City to fund or otherwise provide for the maintenance of all public rights-of-way and public access areas within the Plan Area. The Maintenance Agreement(s) will cover all facilities in the public right-of-way within or adjacent to a particular project, including without limitation, parks and open space, streets and sidewalks, landscaping, lighting, furniture, and median islands. The scope and level of maintenance required will meet City standards as defined in the Agreement. The City will seek coordination among Plan Area property owners, to the extent possible, in order to ensure a consistent level of maintenance across the Plan Area. For example, the City may seek property owners to form a Master Property Owner Association to fund or otherwise provide for the services defined in the Maintenance Agreement. WMSP property owners will be responsible for establishing the preferred and most appropriate funding and organizational mechanism(s) for meeting their obligations under this policy, subject to City approval.

**Immediate Action 1.5: Establish Public Safety Services Funding Agreements.** The City will seek to establish funding mechanisms and agreements to ensure that the cost of providing adequate police and fire services are funded by property owners. These costs will be calculated based on the existing City-wide service standards through

a third-party fiscal analysis conducted by the City and paid for by the developers. The City may ask property owners to form a public safety services CFD to cover the cost of Specific Plan public safety services.

**Immediate Action 1.6: Establish Sales Tax Reporting and Collection Requirements.** Sales tax revenue is an important contributor to the City's General Fund, the primary funding source for ongoing public service costs City-wide and within Specific Plan area. Accordingly, the City will establish a "Tax Localization Plan" to ensure that all commercial activity subject to sales tax that occurs within the Specific Plan area will accrue to the City, to the extent allowed by law. The Tax Localization Plan will apply to both new development as well as the ongoing operations of businesses located within the Plan Area.

**Funding P2: The Specific Plan shall expand infrastructure improvements and services in a phased manner such that adequate capacity is provided as development occurs.**

**Action 2.1: Establish and Secure Phasing Requirements.** The WMSP identifies three major phases of development as described further below. The City shall require that each major phase of development prepare a Development Phasing and Financing Plan for City review and approval. The Development Phasing and Financing Plan shall include a detailed infrastructure and public service schedule for the proposed phase that links the timing of additional infrastructure and service provisions to the level of demand created by new proposed development. The Plan shall be prepared at the expense of applicants and identify the methods, mechanisms and resources necessary to ensure all required infrastructure and public services can be adequately provided for and funded. The Plan may need to account for changing circumstances (e.g. infrastructure needs or costs), unique project specific requirements (e.g. more or less development in a specific phase than anticipated).

**Action 2.2: Identify Required Land and Facility Dedications, Easements, and Use Restrictions.** Specific Plan implementation will require land for public infrastructure (e.g. transportation, utilities park and open space) and to meet other policy goals (e.g. affordable housing). The City and property owners shall establish a process, and in some cases designate the approximate amount and preferred location, for necessary land contributions (e.g. through an irrevocable offer dedication of a public access easement or similar mechanism). Some developers may need to “oversize” or provide land and public facilities beyond what is required by a particular project to ensure various phases of development stand alone functionally and aesthetically. The timing and precise terms of land conveyance, whether through dedication, easement, deed restrictions, or other means, may vary depending on the circumstance.<sup>6</sup> However, a clear and enforceable process should be established prior to major redevelopment in order to achieve equitable cost allocation, implement various financing tools, and advance other planning prerogatives.

**Funding P3. The cost of public infrastructure, amenities envisioned for the Specific Plan (including land as well as maintenance) shall be allocated among land uses (i.e. property owners, developers, and tenants) in a manner that is fair and equitable, to the extent possible.**

**Action 3.1: Establish Equitable Reimbursement Obligation Program.** The City will establish a “Equitable Reimbursement Obligation Program” to implement the RPP and potentially support equitable cost allocation for other WMSP infrastructure and public amenities. As described in the RPP, the Equitable Reimbursement Obligation Program will ensure that any financial burden to property owners who develop or preserve retail or hotel uses will be shared among WMSP residential property owners/ developers, not just those properties located within the Retail Overlay Area. The Program will develop specific procedures and calculations for fee payment obligations from residential development to compensate property owners that develop and/or permanently preserve retail or hotel space. The Program may be expanded to ensure equitable cost sharing for other property owner obligations such as park land or ROW

for utilities or internal streets. The Retail Preservation Policy equitable adjustment component of the Multi-Owner Equitable Reimbursement Obligation Program does not apply to owners/developers of office or medical office projects; however office or medical office uses will pay their fair share of all other components included in the Program.

**Action 3.2: Advance Other Equitable Cost Sharing Measures.** Implementation of the Specific Plan should be guided by the principle of an equitable allocation of costs to ensure that no property owner / developer is disproportionately disadvantaged by the proposed land use and phasing program. For example, if it is necessary for certain property owners to dedicate property towards the creation of public access/infrastructure/facilities, they should be fairly compensated by property owners / developers who do not make such dedications.

While most of the financing mechanisms identified herein incorporate proportionality or nexus-based approaches to cost allocation, there remains some level of discretion in their structure and application. Accordingly, the City should engage and coordinate with property owners, developers, and other entities in the establishment and execution of the preferred Specific Plan financing mechanism(s).

To the extent feasible, the value of any land that is dedicated for the purposes of infrastructure, public access infrastructure, and facilities should be based on its fair market value given allowable uses. For example, a third-party appraisal would allow for a realistic determination of the value asset based on the highest and best use of the land. Of course, land that is currently not developable, due to permanent easements or environmental considerations, for example, would be assessed accordingly.

**Action 3.3: Consider Creative and Flexible Financing Solutions.** Such measures could include cost sharing agreements, construction and reimbursement agreements with developers, and “pre-payment” or “buy-down” options that increase certainty and avoid interest costs. For

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<sup>6</sup>*It is the City's preference that all land and improvements be owned and maintained by the Specific Plan property owners with the exception of water and sewer lines.*

example, the City shall facilitate mechanisms for future development to reimburse developers who oversize infrastructure or dedicate excess land, possibly as part of an area development impact fee ordinance or related credit and reimbursement agreements.

## 9.10 WMSP PHASING

Plan implementation will occur over an extended period and will be driven by a variety of factors, including demand for housing and commercial uses, construction costs, other development in the region and available financing. To reflect the incremental nature of the process and to better understand when certain improvements should or could be made, Plan implementation has been broken into three separate and discrete phases. The three phases are tied to the best understanding, at the time of Plan adoption, as to when specific triggering events should occur.

The discrete Specific Plan phases have been defined to ensure that capital facilities, public access infrastructure and facilities, and public services are planned, coordinated, and provided such that each phase can stand alone functionally and aesthetically even if subsequent phases are not developed. Each phase includes specific requirements and milestones that must be achieved before subsequent development can proceed. In particular, in each phase WMSP property owners will need to provide for a minimum level of park and open space acres, transportation improvements, and retail and/or hotel square footage to proceed with project development. In addition, ongoing activities such as maintenance, conservation, and monitoring will span the life of the Specific Plan.

Table 9.5 identifies the level of residential development included in each phase and the corresponding park and open space acres and retail or hotel development required. The phasing requirements are designed to achieve the public Open Space goals outlined in Chapter 3, *Community Structure and Land Use Plan*, while retail and hotel requirements ensure consistency with the Retail Preservation Policy defined in Section 9.4 of this chapter.

Notwithstanding anything in this or any other provision of the WMSP to the contrary, any project where the property owner has provided not less than that owner's Proportional Share of Retail/Hotel Space (as defined below) shall not be denied or delayed base on phasing requirements for the provision of permanently preserved retail space within the WMSP. A project's Proportional Share of Retail/Hotel Space shall be determined based on the following definitions and formula:

### Definitions:

- » **WMSP Retail/Hotel Preservation Target:** The minimum amount of retail square feet targeted for buildout of the specific plan, as defined in the Retail Preservation Policy (see Section 9.4)
- » **Property Owner Acreage:** The property ownership acreage held by each of the five property owner groups as determined at the date of final approval of the specific plan.
- » **Property Acreage Allocated to Office:** The property ownership acreage planned and allocated for office or medical office development.
- » **Total WMSP Area Acreage.** The total amount of land acreage covered by the WMSP (estimated at 102.82 acres)

### Formula:

Proportional Share of Retail/Hotel Space =  $WMSP\ Retail / Hotel\ Preservation\ Target * (Property\ Owner\ Acreage / (Total\ WMSP\ Area\ Acreage))$

Nothing here in shall limit the City's authority to require that roadway, utility, fire / life safety, and other infrastructure, amenities, and/or services (including retail) be provided for through the Equitable Reimbursement Program.

Specific transportation calming and entry point improvements will also be triggered in each phase. These improvements are linked to the Traffic Calming Implementation Quadrants as defined in Chapter 6, *Mobility*.

Additional information on the specific implementation steps included in each phase is provided in the following sections.

### PHASE 1. SHORT-TERM

Phase 1 covers the first 950 units of residential development and also incorporates the very-short-term measures (next 1-3 years) necessary to lay the groundwork for all new development (e.g., residential, retail, hospitality, entertainment, office and medical).

The WMSP identifies the general locations of the 3-acre Urban Park Plaza and the 2.5-acre Neighborhood Park in Figure 5.15. These two major parks can be moved on the development site to accommodate the owner’s land use planning. Also, the parks maybe relocated or shared between owners upon review and approval from the City of a relocation or sharing agreement agreed to by the property owners. Locations and final layout of the Neighborhood Park and Urban Park Plaza will be reviewed and approved by the Community Development Director.

Property owners must also designate up to 200,000 net leasable square feet of retail and or hotel space prior to the completion of this phase. In the alternative, projects within Phase 1 shall be authorized to satisfy Retail / Hotel phasing requirements through participation in Equitable Reimbursement Program. As described in Section 9.4, *Retail Preservation Policy*, existing retail space can contribute to this goal provided that zoning designations have been secured.

Phase 1 will also include completion of a number of initial implementation activities geared toward three key objectives: 1) establish the long-term funding and financing mechanisms and organizational structures that will ensure effective plan implementation; 2) start to develop the transformative placemaking aspects of the WMSP Vision that enhance the identity of the area as a distinctive destination; and 3) start to build out the most basic mobility improvements that will allow for greater connectivity within the WMSP.

**Table 9.5: WMSP Phasing Requirements**

Phase	Residential Units that have Received Permits	Cumulative Park / Open Space Areas Required (acres) <sup>1,2</sup>	Cumulative Retail or Hotel Required (sq.ft.) <sup>1,3,4</sup>
Phase 1	0 - 950	5.5	200,000
Phase 2	951 - 1,895	9.5	400,000
Phase 3	1,896 - 3,000	17	600,000

Notes.

1. Acres indicated are approximate; actual amounts in Phase 1 and Phase 2 can include a 10% deviation.

2. Represents the public park and open space acres that must be designated for the parks and open spaces required by Section 5.2.15, *Open Space Requirements*, before issuance of building permits in the subsequent phase.

3. Represents the net leasable square feet of designated retail and/or hotel building space that must be secured to the satisfaction of the CDD during the Phases (e.g. prior to approval of new development in the subsequent phase).

4. Any project which provides the project’s Proportional Share of Retail/Hotel Space shall conclusively be deemed to have fully satisfied all retail square footage requirements, limitations, and conditions applicable to such owner’s land. (See page 219 for Proportional Share of Retail/Hotel Space description).

Examples of projects that could be completed in this phase:

- » New monumentation or other landmark features at gateway entrances
- » New roadway, intersection, crossings, signal and driveway improvements along Bolsa Avenue and Edwards Street
- » Parkway landscape improvements along Bolsa Avenue and Edwards Street
- » Westminster Nature Activity Trail Improvements; demolition of some of the existing Mall buildings; construction of new buildings and internal roadways
- » Establish an Area Impact Fee for park and open space facilities
- » Establish necessary financing districts and mechanism for public right-of-way maintenance
- » Establish Public Safety Services Funding Agreements

## PHASE 2. MID-TERM

Phase 2 covers infrastructure, public facilities, and service needed to accommodate approximately 1,895 housing units (plus any existing or new commercial real estate with the Plan Area). During this phase the City will require that WMSP property owners specify and designate approximately four additional acres of public park and open space land prior to issuance of a building permit for the 1250<sup>th</sup> residential unit (for a total of 9.5 public park/open space acres). Property owners must also designate an additional 200,000 net leasable square feet of retail and/or hotel space (for a total of 400,000 square feet), consistent with the Retail Preservation Policy.

Additional examples of infrastructure and public facilities that could be completed in this phase:

- » Construction of buildings and internal roadways completed on remaining parcels

- » Public open space park construction, separate from required open space/landscaping for individual projects (3 acre, 2 acre and remaining open space areas to total 9.5 acres)
- » Landmark features/artwork/mural along 405 freeway edge
- » Affordable housing goals are met for all prior approved development or projects

## PHASE 3. LONG-TERM AND ONGOING

Phase 3 covers infrastructure, public facilities, and services needed to accommodate the remainder of WMSP buildout, including up to 3,000 housing units. During this phase, the City will require that WMSP property owners specify and designate the final 7.5 acres of public park and open space needed to achieve the goal of 17 acres. This land dedication will be required prior to issuance of a building permit for the 1,896<sup>th</sup> residential unit. Property owners must also designate an additional 200,000 net leasable square feet of retail and/or hotel space (for a total of 600,000 square feet), consistent with the Retail Preservation Policy.

Each newly constructed project, remodeled project, street improvement, or public amenity incrementally adds to realization of the WMSP Vision. The City should periodically assess this progress, then evaluate and respond to subsequent projects based on an understanding of the then-current balance of uses and infrastructure capacity. The City should ensure that priority is placed on the community vision, environmental sustainability, vehicle trip management, and the need to minimize impacts of new development on existing neighborhoods.



# Appendix A

## **Glossary**





Pacific  
CITY

GOLDGLOW RESTAURANT

VEGETARIAN



## Definitions “A.”

**Allowed Use.** A use of land identified in the Specific Plan, as a permitted or conditional use that may be established with a land use permit and, where applicable, site plan review and/or building permit approval, subject to compliance with all applicable provisions of this Specific Plan.

**Architectural Design Elements.** See Architectural Feature.

**Architectural Feature.** Non-habitable design elements on buildings which provide visual variation and/or relief (i.e., towers, gables, spires, cupolas, artificial windbreaks, and similar structures). The interior space created by such a projection may serve as usable space (i.e., counter space, bookshelves, window seat), but may not increase the floor area of the space (i.e., may not project from the floor level of a habitable space and measure less than six feet from floor-to-ceiling).

**Assembly.** A facility or space designed to accommodate an assembly of 50 or more people for an organized event such as a reception, banquet, dance, concert, conference, seminar or other similar type activity. (See also “Net Principle Assembly”)

## Definitions “B.”

**Banquet Facility.** A building or indoor space(s) that has been designed for assembly (see definition of Assembly).

**Bedroom.** Any room, alcove, loft or similar feature within a dwelling, intended for sleeping.

**Building Height.** Building height is the vertical distance from the grade of the site to the highest point or ridge of a main roof structure or parapet, measured as an imaginary plane located above and parallel to the grade. Towers and other design features are permitted to exceed the maximum building height as described in Section 5.2.11, *Building and Floor Height*.

**Business Frontage.** The portion of a building occupied by a single business tenant, and fronts on a public or private street or outdoor pedestrian corridor or walkway.

## Definitions “C.”

**California Building Code (CBC).** Title 24 of the California Code of Regulations, known as the California Building Standards Code or just “Title 24,” which contains the regulations that govern the construction of buildings in California including but not limited to Electrical (Part 3), Mechanical (Part 4), Plumbing (Part 5), Energy Part 6), Fire (Part 9), and Green Buildings Standards (Part 11). The code is updated every 3 years and the organization of parts may change.

**CalGreen.** Currently Part 11 of Title 24, California Code of Regulations, CalGreen contains standards applicable to residential and non-residential buildings throughout California. Calgreen requires design and construction practices that reduce negative environmental impacts and encouraging sustainable design to improve public health, safety and general welfare.

**Carts and Kiosks (Retail Display Cart).** Carts include rolling cart that are used to temporarily display retail items for sale. Kiosks are small open-fronted hut or cubicle structures from which retail items sold. See examples in Section 5.2.22, *Carts and Kiosks*.

**City Traffic Engineer.** The City Traffic Engineer is the City employee assigned the responsibility of overseeing the professional and technical support of the City’s traffic signal system and communications infrastructure. Additionally, the City Traffic Engineer is responsible for overseeing mobility, circulation, on/off-street parking, and the transportation planning process. The City Traffic Engineer may delegate the authority to perform any of the duties or responsibilities identified in the Specific Plan to another employee or a consultant of the City.

**Commercial Message.** A form of communication soliciting or advertising a specific product or business.

**Community Development Director.** The Community Developer Director is the department head assigned the responsibility of overseeing the Planning and Building functions of the City. The Community Development Director may delegate the authority to perform any of the duties or responsibilities identified in the Specific Plan to another employee or a consultant of the City. At the discretion of the Community Development Director, any Director-level decision may be advanced to the Planning Commission for their input or action.

**Composition.** The placement or arrangement of visual elements within a space or in a building design.

**Conditioned Space.** The area (in gross square feet) of enclosed conditioned space on all floors of a building, as measured at the floor level of the exterior surfaces of exterior walls enclosing the conditioned space. Condition space includes any enclosed space that is provided with wood heating, mechanical heating, or mechanical cooling as defined by the CBC.

#### Definitions “D.”

**Dentil.** One of a number of small, rectangular blocks resembling teeth and used as decoration under the soffit of a cornice.

**Developer.** The entity responsible for the planning, design, and construction of an applicable development project.

**Development.** Any construction activity or alteration of the landscape, its terrain contour or vegetation, including the erection or alteration of structures. New development is any construction, or alteration of an existing structure or land use, or establishment of a land use, after the effective date of this Specific Plan.

**Development Intensity.** A measure of the degree of development such as “density” for non residential uses or “floor area ratio” for non-residential uses or an increase in standards such as parking that occur on a site.

#### Definitions “E.”

**Electric Vehicle.** An automotive-type vehicle for on-road use, such as passenger automobiles, buses, trucks, vans, neighborhood electric vehicles, electric motorcycles, and the like, primarily powered by an electric motor that draws current from a rechargeable storage battery, fuel cell, photovoltaic array, or other source of electric current. Plug-in hybrid electric vehicles (PHEV) are considered electric vehicles. Or as currently defined by CalGreen.

**Electric Vehicle (EV) Charger.** Off-board charging equipment used to charge and electric vehicle.

**Electric Vehicle Charging Space (EV Space).** A space intended for the future installation of EV charging equipment and charging of electric vehicles or as currently defined by CalGreen.

**Electric Vehicle Ready (EV Ready).** See Electric Vehicle Charging Space

**Electric Vehicle Charging Station (EVCS).** One or more electric vehicle charging spaces served by electric vehicle charger(s) or other charging equipment allowing charging of electric vehicles. Electric vehicle charging stations are not considered parking spaces or as currently defined by CalGreen.

**Entrance Overhang.** A protruding structure over a doorway intended to provide protection from rain, snow or other inclement weather conditions.

## Definitions “F.”

**Floor Area.** See “Gross Floor Area.”

**Focal Point.** An element or area of interest, emphasis, or uniqueness with a composition that captures and holds the viewers attention.

## Definitions “G.”

**Graywater.** Pursuant to California Health and Safety Codes Section 17922,12, “graywater” means untreated wastewater that has not been contaminated by any toilet discharge, has not been affected by infectious, contaminated, or unhealthy bodily wastes, and does not present a threat from contamination by unhealthful processing, manufacturing or operating wastes. “Graywater” includes but is not limited to, wastewater from bathtubs, showers, bathrooms washbasins, clothes washing machines, and laundry tubs, but does not include wastewater from kitchen sinks or dishwashers.

## Definitions “H.”

No specialized terms beginning with the letter “H” are used in this specific plan.

## Definitions “I.”

**Infill Site.** A site in an urbanized area that is immediately adjacent to parcels that are developed with urban uses.

## Definitions “J.”

No specialized terms beginning with the letter “J” are used in this specific plan.

## Definitions “K.”

**Kiosk.** See Carts and Kiosks.

## Definitions “L.”

**Lane.** A private drive aisle that provides vehicular ingress, egress and internal circulation for the specific plan and is not intended to accommodate through traffic circulation.

**Laundries and Dry Cleaners.** Service establishments, no larger than 5000 SF, that engaged primarily in small volume laundry and garment services, including garment pressing, tailoring, and dry cleaning pick-up stores (for off-site dry-cleaning processing only). See “Personal Services.”

**Landmark.** See Section 7.2.8 of the Specific Plan.

**Low-Emitting and Fuel Efficient Vehicles.** 1. Zero emission vehicles (ZEV), including neighborhood electric vehicles (NEV), partial zero emission vehicle (PZEV), advanced technology PZEV (AT ZEV) or CNG fueled (original equipment manufacturer only) regulated under California Health and Safety Code Section 43800 and CCR, Title 13, Sections 1961 and 9162. 2. High-efficiency vehicles, regulated by U.S. EPA, bearing High Occupancy Vehicle (HOV) care pool lane stickers issued by the Department of Motor Vehicles.

**Low Impact Development (LID).** Control of stormwater at its source to mimic drainage services provided by an undisturbed site.

## Definitions “M”

**Master Sign Plan.** A plan showing the type, size, location, design, colors and numbers of all signs proposed within the Specific Plan Area.

**Mixed-Use Development.** A development project that combines multifamily residential land use with at least two of the following uses: hotel, retail, or assembly. Mixed-use can occur horizontally or vertically.

**Model Water Efficient Landscape Ordinance (MWELo).** A California regulation commencing with Section 490 of Chapter 2.7, Division 2, Title 23, California Code of Regulations. The MWELo regulation establishes a

structure for planning, designing, installing, maintaining, and managing water effect landscapes in new construction and rehabilitation projects. In addition to these requirements, the City of Westminster has adopted Water Efficiency Landscape Measures as Chapter 17.310 of the WMC as well as a Handbook of Water Efficiency Landscape Measures.

**Moldings.** Materials such as wood, plastic, or stone shaped for use as a decorative or architectural building feature.

### Definitions “N.”

**Neighborhood Electric Vehicle.** A motor vehicle that meets the definition of “low-speed vehicle” in Section 385.5 of the Vehicle Code.

**Net Principle Assembly.** The main gathering space exclusive of hallways, bathrooms, kitchens, and other service areas as well as any prefunction space.

### Definitions “O.”

**Open Space.** Pertaining to residential uses, means outdoor area on the ground, or on a balcony, deck, porch or terrace designed and accessible for outdoor living, recreation, pedestrian access or landscaping. Open space may include unenclosed patios with solid or lattice roofs. Open space shall not include the following: driveways, parking spaces, other surfaces designed or intended for vehicular travel, utility or service areas, any required front or street side yard and required interior side yards if the minimum dimension is less than that required for common open space (if area is designated as common open space) or private open space (if the area is designated as private open space).

- » *Common Open Space.* Open space within a residential development reserved for the exclusive use of residents of the development and their guests with a minimum dimension of ten (10) feet and 100 square feet in size.

- » *Private Open Space.* Open space adjoining and directly accessible to a dwelling unit, reserved for the exclusive use of residents of the dwelling unit and their guests with a minimum dimension of five (5) feet and 40 square feet in size.
- » *Public Open Space.* Open Space within the urban environment that are readily and freely accessible.
- » *Semi-Private Open Space.* Open Space accessible to the general public that has a certain private character to it.

**Outdoor Displays.** Any arrangement of merchandise or advertising that occurs outdoors to publicize business products and/or services.

**Overspray.** Water that is delivered beyond the planted area.

### Definitions “P.”

**Pedestrian Node.** An area that is strongly influenced by pedestrians and contains several pedestrian amenities.

**Pedestrian Scale.** The use of architectural features and site design elements that are smaller in scale and mass to be more proportional to the human body and noticeable to pedestrians such as surface textures and patterns, lighting, window and door hardware and adornments, and similar details.

**Permeable Paving.** Paving material that allow the movement of water around or through the paving material thereby permitting water to percolate through the soil below.

**Personal Care Services.** A business or occupation which provides services involving personal grooming or the care of a person’s apparel, including, but not limited to, laundry services, manicurists, tailors, shoe repair shops, tanning salons.

**Planning Area.** An area within the Specific Plan which abuts upon a street and which provides the ground area of a building or buildings, together with all open spaces as required by the Specific Plan.

**Plants, Invasive.** Invasive Plants are both indigenous and non-indigenous species with growth habit that are characteristically aggressive. Invasive plants typically have high reproductive capacity and tendency to overrun the ecosystems they inhabit.

**Plants, Native.** Native plants are plants that have adapted to a given area and are not invasive.

**Pool or Billiard Rooms.** A room, building or structure wherein the games of pool or billiards are played by the public. These games are played with cue sticks on tables with or without pockets. One or more pool or billiard tables shall constitute a pool or billiard room. Three or fewer pool tables may be a permitted accessory use but only in a bona fide restaurant (not including fast food restaurants), bar, private clubs and other commercial entertainment uses.

**Potable Water.** Water that is drinkable and meets the U.S. Environmental Protection Agency (EPA) Drinking Water Standards.

**Portico.** A structure consisting of a roof supported by columns at regular intervals, typically attached as a porch to a building.

**Primary materials.** The materials that make up the majority of the structural components, foundation and envelope of construction projects.

**Public Works Director.** The Public Works Director is the department head assigned the responsibility of overseeing the Engineering functions of the City. The Public Works Director may delegate the authority to perform any of the duties or responsibilities identified in the Specific Plan to another employee or consultant of the City.

## Definitions “Q.”

No specialized terms beginning with the letter “Q” are used in this specific plan.

## Definitions “R.”

**Rainwater.** Precipitation on any public or private parcel that has not entered an offsite drain system or channel. A flood control channel, or any other stream channel, and has not previously been put to beneficial use.

**Rainwater Catchment System.** A facility designed to capture, retain, and store rainwater flowing off a building, parking lot, or any other manmade impervious surface for subsequent onsite use. Rainwater catchment system is also known as “Rainwater Harvesting System” of “Rainwater Capture System.”

**Regulatory Agency.** The designated department or agency as specified by statute or regulation.

**Retail Frontage.** The portion of a building occupied by a single retail tenant, and fronts on a public pedestrian corridor or walkway.

**Retail Mezzanine.** A raised platform or balcony within a retail or mixed use building that serves as an intermediate floor between the ground floor and the second floor.

**Regular Event.** Any event proposed that does not require the closure of the Festival Street and does not exceed a maximum number of 650 people.

## Definitions “S.”

**Special Event.** Any event or combination of events that would result in more than a maximum number of 650 people and may or may not include the closure of the Festival Street.

## Definitions “T.”

**Temporary Event.** A single community event or celebration that occurs for a short period of time. See also Special Events.

**Tenant.** The lessee of space within the Specific Plan Area.

**Tenant-Occupants.** Building occupants who inhabit a building during its normal hours of operation as permanent occupants, such as employees, as distinguished from customers and other transient visitors.

**Theme.** The overall design elements as a common thread of the architectural planning and design that ensures consistency in the design of the community.

## Definitions “U.”

**Unbundled Parking.** A parking strategy in which parking spaces are rented or sold separately, rather than automatically included with the rent or purchase price of a residential or commercial unit. Tenants or owners can purchase only as much parking as they need and are given the opportunity to save money and space by using fewer parking stalls. Unbundled parking is more equitable and can reduce the total amount of parking required for the building.

## Definitions “V.”

**Vanpool Vehicle.** Eligible vehicles are limited to any motor vehicle, other than a motortruck or truck tractor, designed for carrying more than 10 but not more than 15 persons including the driver, which is maintained and used primarily for the non-profit work-related transportation of adults for the purposes of ridesharing.

**Visual Hierarchy.** A key component of design, visual hierarchy organizes and prioritizes design elements to convey their relative importance to guide visitors, define spaces, and create a sense of order.

## Definitions “W.”

No specialized terms beginning with the letter “W” are used in this specific plan.

## Definitions “X.”

No specialized terms beginning with the letter “X” are used in this specific plan.

## Definitions “Y.”

No specialized terms beginning with the letter “Y” are used in this specific plan.

## Definitions “Z.”

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# Appendix B

# **General Plan**

# **Consistency**





General Plan Consistency California law (Government Code §65450-§65453) allows cities to develop and administer Specific Plans as an implementation tool for their General Plan. As a requirement of state law, Specific Plans must demonstrate consistency in regulations, guidelines and programs with the goals, objectives, policies, standards, programs and uses that are established in the General Plan. After a analysis of the City of Westminster General Plan, the Westminster Mall Specific Plan (WMSP) has been found to be consistent with and supportive of the City's General Plan, as amended. A discussion of the relationship of this Specific Plan to relevant General Plan goals from the Land Use, Economic Development, Community Design, Housing, Mobility, Noise, Parks and Recreation, Infrastructure and Natural Resources, Public Health and Safety elements follows. Only goals and policies applicable to the Moran Street Specific Plan are analyzed here for consistency. Goals and policies found to be unrelated to the Specific Plan have been intentionally omitted from this discussion.

According to the City's general plan, the current land use designations of the project site is Mixed Use – Westminster Mall. While the land use classification is still appropriate, the development projections in the Specific Plan are higher than was assumed in the General Plan. Therefore, for the proposed project to be implemented, the General Plan will be amended concurrently with adoption of the Specific Plan to remove density and intensity requirements and increase the development capacity to match that of the Specific Plan (3,000 residential units, 1,200,000 square feet of non-residential uses and 425 hotel rooms).

## B.1 LAND USE ELEMENT

### GOAL LU-1: MANAGING FUTURE GROWTH

A sustainable, balanced land use pattern that accommodates residential and nonresidential growth in strategic locations in order to preserve the community's existing single-family neighborhoods.

**LU-1.1:** Sustainable Development Pattern. Provide for an overall pattern of land uses that promotes efficient development; reduces pollution, automobile dependence, greenhouse gas emissions, and the expenditure of energy and other resources; ensures compatibility between uses; enhances community livability and public health; and sustains economic vitality.

*Consistency: The mixing and intensification of uses on the Westminster Mall site helps to reduce vehicular trips which in turn minimizes automobile dependence, and greenhouse gas emissions. The inclusion of two new parks helps to improve the livability of the area and promotes a healthy, active environment and the introduction of new jobs-generating uses contributes to the City's long-term fiscal sustainability as well.*

**LU-1.2:** Complete and livable neighborhoods. Maintain a development pattern of distinct residential neighborhoods oriented around parks, schools, community meeting facilities, and other gathering spaces that are connected with neighborhood-serving businesses and walking and biking pathways.

*Consistency: The WMSP creates a new urban scale neighborhood and jobs center that integrates new park amenities, improvements to the Navy Trail (Class 1 Bike Trail), and encourages a walkable internal network of pathways that connects internal open spaces and gathering areas. It is anticipated that some residential uses in the project area will be stand-alone (distinct residential neighborhoods) and others will*

*be integrated into the mixed-use environment planned for the majority of the site, which will be linked to plazas, promenades, and pedestrian scale amenities and improvements.*

**LU-1.3:** Infill Development. Promote infill residential development that complements existing neighborhoods and surrounding areas. Infill residential development and future housing growth in Westminster is strongly encouraged in areas designated for mixed-use development in order to preserve the character of the community's existing single-family uses.

*Consistency: Notably one of the City's largest and most prominent redevelopment opportunities, the WMSP promotes an increase of intensity of development on an infill site and a mix of residential, non-residential (office, medical office, retail and entertainment) and hospitality uses integrated into a master planned activity area for the community. Since several existing retail uses on the site are no longer operational (Sears, Babies R Us), the Westminster Mall has an opportunity to re-envision and reposition the mall to respond to changing market conditions and community desires to create an activity center and gathering place within the City.*

**LU-1.5:** Cohesive and Integrated Development. Encourage the use of specific plans to provide for the cohesive and integrated development of large areas, complex or multi-parcel sites, areas with multiple property owners, and/or areas of particular importance to the community.

*Consistency: Redevelopment of the WMSP project area requires an extra level of coordination as there are five different property owners with parcels located in the WMSP. Each parcel has different characteristics such as physical constraints (topography or drainage limitations), or timing constraints (some are ready to redevelop earlier than others), and the various owners may have different or competing needs for their investment at the Mall, so coordination can become one of the City's most challenging issues to create an integrated, cohesive development*

*on the site. All property owners must work in concert to create a Master Plan for the site that has an integrated network of internal streets, gathering spaces and that are distributed evenly throughout the site and provide much needed recreational amenities for the community.*

**LU-1.6:** Preserve single-family neighborhoods. Direct growth away from established single-family residential neighborhoods onto underutilized parcels along Westminster's corridors and onto major centers of activity.

*Consistency: The WMSP proposes to create an urban village in the area between the I-405 Freeway, Bolsa Avenue and Edwards Street that is intended to serve as the City's most prominent activity center. It concentrates the mixing of uses and height to the center of the project and creates gradual transitions in building scale from the existing single-family neighborhoods to the south and west of the project area. It also requires parkway improvements along Bolsa Avenue and Edwards Street to enhance the visual appeal of the corridor and create an attractive street edge.*

**LU-1.8:** Impact Fees. Require new development to pay its proportionate share of the cost of providing and/or upgrading public facilities and services impacted by new development through impact fees.

*Consistency: The property owners will be required to pay project impact fees for schools, infrastructure and public safety personnel.*

**LU-1.9:** Special Assessment Districts. Encourage the use of special assessments as a way to address public improvements (i.e., parks, undergrounding utilities, landscape, lighting, signage, street furniture, or other public improvements) in concert with new development.

*Consistency: Property owners may also create an improvement or assessment district to fund affordable housing and parks improvements required for the project.*

## GOAL LU-2: LAND USE MIX

A mix of land uses that meets the diverse needs of Westminster's residents, offers a variety of employment opportunities, and supports the development of regional destinations.

**LU-2.1:** Places to live. Provide sites for a range of housing types, locations, and densities in a variety of neighborhood settings equipped with amenities that support a high quality of life.

**LU-2.2:** Places to work. Provide for a broad spectrum of land uses that offer job opportunities for Westminster's residents, including commercial, office, industrial, and business parks.

**LU-2.3:** Places supporting the quality of life. Provide a diversity of uses and services supporting Westminster's residents, such as facilities for civic governance and administration, public safety (police and fire), seniors and youth, community gatherings, and comparable activities.

**LU-2.4:** Centers of activity. Promote the development of distinct, well-designed, mixed-use destinations that feature residential and commercial uses, blend harmoniously with surrounding uses, are accessible from transit and walking and biking paths, and serve as focal points in the community.

**LU-2.5:** Balancing jobs and housing. Coordinate residential and nonresidential planning and development with regional, county, and other local agencies to further regional and sub-regional goals for job-housing balance.

*Consistency for policies LU 2.1-.2.5: By mixing uses on the site, the project will create a new center of activity for the City, promote walkability, help to reduce the amount of vehicle trips generated by any new development and by intensifying uses on this large site, and helps the City to minimize encroachment of more intense uses into the City's single family residential neighborhoods (focus intensification on a large site, versus a gradual, incremental expansion of higher density uses*

*that "creep" into established single family neighborhoods). The mixing of uses also contributes to the fiscal sustainability and longevity of new development in the project area (and citywide) providing new housing opportunities not currently present in the City, new jobs-generating uses and sales tax generating uses.*

**LU-2.6:** Land use change. Where opportunities for land use change arise, seek input from adjacent property owners, the surrounding neighborhood or district, and other stakeholders during the consideration process so as to determine appropriateness of inform strategies.

*Consistency: Throughout the process the City held a series of community workshops to discuss the proposed project. The workshops were held at the Mall and at Clegg Elementary School and provided the community opportunities to provide input on specific topics such as the future project design, desired amenities, building height and traffic concerns/desired roadway improvements. Online surveys were also conducted related to the project Vision and other topics.*

*There was a great turnout from the community especially for the second survey. By the time the survey closed September 17th, 2018, almost 2,000 people submitted a response and almost half provided written comments. Participants were asked to provide feedback on various topics such as amenities and preferred uses. Many responses included ideas for the city and property owners to consider for the future and potential case studies to look to as good examples.*

**LU-2.8:** Development flexibility. Allow for flexible development standards in the City's zoning code provided that the potential benefits and merit of projects can be balanced with potential impacts.

*Consistency: The Specific Plan provides setbacks and standards for the street edges (along Bolsa Avenue, Edwards Street and the I-405 Freeway) and allows for flexibility for design of roadways and buildings internal to the site to accommodate the needs of the multiple property owners and the phasing of future development.*

### **GOAL LU-3: MIXED-USE DEVELOPMENT**

Complementary well-designed activity centers that allow people to live close to shopping, employment, and public uses, contribute to a reduction in automobile use, and provide venues for engaged and enhanced pedestrian activity.

**LU-3.1:** Differentiation of mixed-use areas. Establish and maintain distinct identities for Westminster’s mixed-use areas by customizing uses, scale, form of development, and amenities.

**LU-3.2:** Use configuration. Permit a mixture of compatible land uses on a single site or within a single development project in a vertical or horizontal configuration.

**LU-3.3:** Development scale. Establish standards to ensure that a sufficient scale and footprint of any single use is achieved in mixed-use areas to establish a cohesive environment that minimizes impacts attributable to the adjacency of differing uses. This may define minimum parcel and building sizes, number of housing units, and/ or nonresidential square footage, as well as relationships and setbacks among the uses.

**LU-3.4:** Corridor development. Facilitate the redevelopment of the City’s auto-oriented commercial corridors (particularly Beach Boulevard and Westminster Boulevard) by clustering higher density, pedestrian-oriented mixed uses on larger parcels at key intersections, while re-using intervening parcels for housing mixed with neighborhood-oriented commercial services.

*Consistency for LU-3.1 through LU-3.4: By mixing uses on the site, the project will transition from an auto oriented development (typical of Malls developed in the 1960’s and 1970’s) to create a new center of activity for the City, promote walkability, help to reduce the amount of vehicle trips generated by any new development and by intensifying uses on this large site, and helps the City to minimize encroachment of more intense uses into the City’s single family residential neighborhoods (focus intensification on a large site, versus a gradual, incremental expansion of higher density uses that “creep” into established single family neighborhoods). The mixing of uses also contributes to the fiscal sustainability and longevity of new development in the project area (and citywide) providing new housing opportunities not currently present in the City, new jobs-generating uses and sales tax generating uses. The Specific Plan also identifies a maximum number of units, hotel rooms, and non-residential square footage that is allowed by the plan.*

*It concentrates the mixing of uses and building height to the center of the project and creates gradual transitions in building scale from the existing single-family neighborhoods to the south and west of the project area. It also requires parkway improvements along Bolsa Avenue and Edwards Street to enhance the visual appeal of the corridor and create an attractive street edge.*

**LU-3.8:** Implementation tools. Provide flexible development standards implemented through a specific plan or new development code standards for mixed use that ensure compatibility between allowable uses on site and with adjacent uses.

*Consistency: The WMSP provides customized development standards for the project site and contains multiple setback, building height and other development standards and design standards and guidelines to ensure a logical transition from the single family neighborhoods to the south and west.*

**LU-3.9:** Design integration. Require that residential and nonresidential portions of mixed-use buildings and sites be integrated through site and building design to ensure compatibility among uses.

*Consistency: Since the entirety of the WMSP allows for mixed use, the document includes multiple provisions to address transitions and minimize conflicts between residential and nonresidential uses (development standards and design standards and guidelines).*

**LU-3.10:** On-site amenities. Require that residential/commercial mixed-use projects provide on-site gathering spaces (plazas, courtyards, etc.) and other pedestrian-scale amenities, such as benches, fountains, and landscaping, that contribute to the living environment of residents, or contribute funds for their development within proximity to the project.

*Consistency: Parks, plazas and other open spaces are an integral component of the WMSP, promoting cultural opportunities, community interaction, physical activity and placemaking features as essential elements of any future development.*

*The Specific Plan requires the development of an Urban Plaza and a Mixed Use Neighborhood Park, and also required projects provide other open space amenities such as plazas and paseos, internal parkway treatments, improvements to the parkways along Bolsa Avenue and Edwards Street and improvements to the Navy Trail Easement (new Bike Facility).*

## GOAL LU-4: LAND USE COMPATIBILITY

Compatible residential, commercial, and industrial development that is sensitively integrated with existing development and neighborhoods that minimizes impacts on surrounding land uses.

**LU-4.1:** Development compatibility. Require that development is located and designed to ensure compatibility among land uses, addressing such elements as building orientation and setbacks; buffering; visibility and privacy; automobile and truck access; impacts of noise, lighting and glare; landscape quality; and aesthetics.

**LU-4.2:** Transition in scale. Require that the scale and massing of new development in higher-density centers and corridors provide appropriate transitions in building height and bulk that are sensitive to the physical and visual character of lower density adjoining neighborhoods.

*Consistency with LU-4.1 and LU-4.2: The WMSP provides customized development standards for the project site and contains multiple setback, building height and other development standards and design standards and guidelines to ensure a logical transition from the single family neighborhoods to the south and west.*

*Since the entirety of the WMSP allows for mixed use, the document includes multiple provisions for appropriate transitions between residential and nonresidential uses (development standards and design standards and guidelines). The WMSP also includes provides direction identifying areas where various uses are encouraged, permitted and prohibited within the Specific Plan area. For example, noise generating uses such as amplified music, are prohibited along the Bolsa Avenue and Edwards Street edges, however they are permitted internally to the site, closer to the freeway, to minimize the potential conflicts with existing single family residential neighborhoods.*

**LU-4.4:** Mitigation. Require new uses to provide buffers between existing uses where potential adverse impacts could occur, such as decorative walls, setbacks and landscaping, restricted vehicular access, parking enclosures, and lighting control.

*Consistency: The WMSP requires new parkway improvements along Bolsa Avenue and Edwards Street, and building heights are required to be lower on these edges to provide a logical transition from the adjacent single-family residents. The Mobility Section of the WMSP also requires new traffic calming improvements to minimize cut through traffic into the adjacent neighborhoods.*

**LU-4.7:** Commercial and industrial development. Require new commercial and industrial developments to clearly demonstrate that they will have no significant detrimental impacts upon the City and its residents, including, but not limited to, significant adverse traffic, noise, air pollution, and fiscal impacts.

*Consistency: No industrial uses are proposed in the WMSP at this time. The specific plan and mitigation measures in the EIR include provisions to reduce, minimize or eliminate traffic, noise and air pollution impacts through use of building materials/treatments (noise), and mixing of uses (minimizing vehicle miles traveled). It also provides provisions and strategies to ensure that there are no detrimental fiscal impacts with the construction of new projects (those will also be evaluated on a case-by-case basis as individual property owners submit for approval of their projects).*

## GOAL LU-7: NOISE

Community noise and vibration levels that balance the need for peaceful environments for sensitive land uses with the needs of local businesses and regional land uses.

**LU-7.1:** Land Use Noise Compatibility. Assess the compatibility of proposed land uses with the noise environment when preparing, revising, or reviewing development project applications.

**LU-7.2:** Noise insulation and vibration standards. Require new projects to comply with noise insulation and vibration standards of local, regional, and state building code regulations.

**LU-7.4:** Noise control. Utilize noise abatement, design techniques, and other mitigation strategies-including staggered operating hours, insulation, building setbacks, noise barriers, placement of parking, and utility areas, and building orientation-to ensure that noise levels do not exceed the limits in the Westminster Municipal Code.

**LU-7.5:** Roadway Noise. Encourage non-motorized transportation alternatives for local trips and the implementation of noise sensitivity measures, including traffic-calming road design, lateral separation, natural buffers, and setbacks to decrease excessive motor vehicle noise along major arterials.

*Consistency LU-7.1 through LU-7.5: Noise attenuation applies to any new development that includes residential or other noise sensitive uses. The City's General Plan Noise Element identifies two future noise contour levels on the Westminster Mall site, 70 dba CNEL (closest to the freeway) and 65 dba CNEL (transitioning midway in the property toward the single-family residential neighborhoods in Huntington Beach).*

*Applicants for new noise-sensitive development (e.g., residential, hospitals, etc.) must demonstrate to the Director that all habitable rooms would meet the 45 dBA CNEL interior noise standard required by the State Title 24 before the City issues building permits. This can be accomplished with enhanced construction design or materials, such as upgraded dual-glazed windows and/or upgraded exterior wall assemblies.*

*To ensure exterior noise compatibility, applicants proposing projects that fall within areas located within the 70 dbA CNEL contour lines must demonstrate that the noise levels for residential outdoor common areas and recreational areas are at or below 70 dbA CNEL to ensure compatibility with the ambient noise levels. Noise reduction measures could include increased setback from the freeway, shielding with noise barriers, or placing outdoor noise-sensitive areas behind buildings.*



*For noise attenuation purposes, outdoor common or recreation areas do not include parking and loading areas, ornamental landscaping, or walking/biking trails.*

## **B.2 ECONOMIC DEVELOPMENT ELEMENT**

### **GOAL ED-1: QUALITY OF LIFE**

Regionally competitive activity centers and destinations with a wide variety of shopping, dining, entertainment, and services that contribute to an excellent quality of life in Westminster.

**ED-1.1:** Unmet Need. In public decisions, discretionary land use and development approvals, and the investment of public resources, give priority to underrepresented commercial sectors, in shopping, dining, entertainment, and service businesses.

*Consistency: The WMSP includes retail, dining entertainment and hospitality uses and includes a diversity of housing options (market rate and affordable) to respond to the City's unmet needs.*

**ED-1.3:** Business Attraction. Facilitate and invest in programs and services that attract new businesses in underrepresented commercial sectors, including those that serve or attract higher income residents and visitors.

*Consistency ED-1.3: Property owners and businesses currently located within the Westminster Mall are working with the City to create a master planned approach for the project area and to develop a business model (and attraction/retention plan) that is able to evolve with changing market conditions.*

**ED-1.4:** Experience-oriented destinations. Consistent with the land use plan, support public and private investments and development projects that create or improve commercial districts to be amenity rich, pedestrian friendly, and experience oriented, and encourage these districts to be distinct from one another.

**ED-1.6:** Increased shopping, dining, and entertainment. Increase the number and variety of shopping, dining, and entertainment businesses and establish experience-oriented shopping areas and activity centers to expand Westminster's capture of regional retail spending.

*Consistency ED-1.4 and ED-1.6: The WMSP is envisioned to be the City's premiere destination and activity center, providing parks, open spaces, plazas and other features that create a special district in Westminster and north Orange County. It allows for 1.2 million square feet of non-residential uses in addition to up to 425 hotel rooms on site which will help generate Transient Occupancy Tax revenue for the city.*

**ED-1.7:** New development. Encourage commercial development that achieves these objectives:

- » Create an effective concentration of land use that will remain competitive with future surrounding developments
- » Provide high sales tax generating, quality retail and anchor tenants that will generate high sales tax by attracting customers from beyond Westminster and reducing the outflow of local consumer spending.
- » Avoid small convenience-oriented strip centers
- » Provide functional design and site configuration.

*Consistency: The WMSP allows for a mixture of land uses including residential, office, hospitality, retail, dining, and entertainment. One of the objectives of the project site is to maintain the City's sales tax base and to improve the destination experience to draw customers and*

visitors from outside the Westminster area. The clustered mixed-use nature of the project maximizes use of space and shifts away from the City's prior retail model of primarily strip center development to allow for internal circulation and an experience that prioritizes pedestrians over the automobile.

## GOAL ED-2: TAX BASE

A financially healthy city government with a balanced mix of land uses and special funding and financing districts that increase resources to invest in public facilities and services.

**ED-2.2:** Leveraging private investment. When deciding where to invest public resources and how much to invest, consider the degree to which the public investment will leverage private sector investment, whether through development and redevelopment or through establishment of financing mechanisms, such as assessment districts, business improvement districts, and infrastructure finance districts.

*Consistency: The WMSP identifies several financing mechanisms that can be used by the property owners and the City to finance improvements to the project area, including infrastructure, landscaping, and programming of the open space areas, including assessment districts, BIDs and Infrastructure financing districts. As projects are proposed in the project area, property owners will coordinate with the City to determine the best mechanisms to implement improvements (including the construction of two new parks and improvements to the Navy Trail Class I bicycle lane).*

**ED-2.7:** Development support for fiscal vitality. Secure developments that serve the economic needs of the community and effectively sustain or increase the City's ability to provide all public services and maintenance demands.

*Consistency: Tax revenue generated by the mix of uses proposed within the WMSP will aid the city's ability to provide public services and meet community needs.*

## GOAL ED-3: JOBS AND INCOME.

**ED-3.6:** A balanced business mix. Achieve a balanced mix of land use activities and a competitive level of development that optimize economic growth among all business sectors.

*Consistency: The WMSP provides a mix of land uses upon the site that will contribute to the area's overall levels of activity in the commercial, hospitality, and residential sectors. In addition to the creation of new construction and retail jobs, the project will provide new and attractive rental opportunities for professionals, which may incentivize additional employers to locate in the area.*

## B.3 COMMUNITY DESIGN ELEMENT

### GOAL CD-1: COMMUNITY IDENTITY

A unified and attractive community identity that creates a distinctive sense of place and distinguishes Westminster from other cities.

**CD-1.1:** Design features. Enhance the City's identity through the use of attractive high-quality gateways, city entry signs and design features, cohesive street signs, and other design features at public gathering spaces and other areas, where appropriate.

**CD-1.3:** Individual neighborhoods. Strengthen the identity of individual neighborhoods/communities with entry monuments; flags; street signs; and/or special tree streets, landscaping and lighting.

*Consistency CD-1.1 and CD-1.3: The WMSP will require landmark signage, artwork, or murals to create a visual marker from the I-405 freeway to serve as a City entry sign and will include monument signage at key gateways into the project from Bolsa Avenue and Edwards (in addition to the special parkway treatments that are required along the public street edges and along internal roadways). It is anticipated that other amenities and design features will be included in the internal open spaces and plaza areas.*

**CD-1.4:** Design quality. Support development of the built environment that enhances and improves community image through the use of high-quality architectural features, design elements and natural materials.

**CD-1.6:** Streetscapes. Promote drought tolerant landscaping, tree planting, and tree preservation along Westminster's streets as a means of improving aesthetics, making neighborhoods more pedestrian-friendly, providing environmental and economic benefits.

**CD-1.7:** Gathering spaces. Promote lot consolidation and assemblage for redevelopment of small, underutilized, and blighted properties in mixed-use areas to create lot sizes that would allow for incorporation of public plazas and other gathering spaces in project design.

*Consistency CD-1.4, CD-1.6 and CD-1.7: The WMSP includes provisions that require or encourage design quality, enhanced streetscapes and the inclusion of gathering spaces as fundamental components of the project area.*

## GOAL CD-2: DESIGN QUALITY

Projects, developments, and public spaces that visually enhance the character of the community, reinforce civic pride and public safety, discourage blight, and appropriate buffer dissimilar land uses so that the differences in type and intensity do not conflict.

**CD-2.1:** Special site features. Preserve positive characteristics and unique features of a site during the design and development of a new project; the relationship to scale and character of adjacent uses should be considered.

*Consistency: The topography of the site and the orientation adjacent to the freeway are encouraged to be used in future project design. Areas that are lower in elevation can accommodate taller buildings without creating significant visual impact on adjacent single-family neighborhoods.*

**CD-2.2:** Public and private facilities. Minimize visual impacts of public and private facilities and support structures through sensitive site design and construction. This includes but is not limited to: appropriate placement of facilities; undergrounding of utilities, where possible; and aesthetic design (cell tower stealthing).

*Consistency: The Specific Plan provides development standards that require loading, service, parking and similar activities be located away from public streets, fronting internal streets and other pedestrian areas. In addition, the specific plan requires screening of these areas as well as screening of trash enclosures, utilities (cell phone antennas and towers) and mechanical equipment to minimize their visual impact.*

**CD-2.4:** Parking in mixed-use areas. Design parking lots and structures in mixed-use areas to be functionally and visually integrated into and connected with new projects or adjacent buildings; off-street parking lots should not dominate the streetscene or should be screened by enhanced landscaping to minimize the view.

*Consistency: The WMSP encourages parking structures to be screened, enclosed in a "wrap" type development (internal to a residential or mixed use project) or placed behind areas or street scenes that are intended to function with a pedestrian focus. Limited areas of shorter term on street parking are envisioned for the internal Main Street areas and limited surface parking lots; however, it is envisioned that the*

majority of the parking proposed will be structured to maximize the space that can be used for building footprints, plazas, parks or other amenities and landscaping to create a desirable and active project.

**CD-2.5:** Gathering spaces in mixed-use areas. Integrate plaza or gathering spaces in new mixed-use developments, and explore ways to creatively integrate outdoor dining, seating, or other activity-generating features into project design.

**CD-2.6:** Public art. Consider including public art that reflects the diversity of the City at key gateways, major projects, and public gathering spaces.

*Consistency CD-2.5 and CD-2.6: The WMSP encourages the use of landmark signage, artwork, or murals to create a visual marker from the I-405 freeway to serve as a City entry sign and will include monument signage at key gateways into the project from Bolsa Avenue and Edwards (in addition to the special parkway treatments that are required along the public street edges and along internal roadways). Gathering spaces that are linked via pedestrian routes are essential features required as part of all development in the project area. It is anticipated that other artwork and design features will be included in the internal open spaces and plaza areas.*

**CD-2.7:** CPTED. Utilize Crime Prevention Through Environmental Design (CPTED) techniques and defensible space design concepts to enhance community safety.

*Consistency: The WMSP will include various techniques including lighting to enhance community safety along pedestrian walkways, common areas, entrances/exits, parking structures, and all public spaces. Further, landscaping shall be placed to maximize observation while providing the desired degree of aesthetics.*

**CD-2.8:** Vehicular access points. Minimize new driveways and consolidate access points to improve site design, traffic flow and safety of vehicles and pedestrians in new and existing developments.

*Consistency: The Mobility Chapter of the WMSP addresses project access and includes provisions that monitor vehicular capacity at primary entries along Bolsa Avenue and Edwards Street. Consolidation of driveways is encouraged to maintain consistency in the pedestrian and bicycle experience (and to minimize conflicts with cars leaving or entering the Mall area). Right turn only lanes may also be implemented to limit cross traffic turning.*

**CD-2.9:** Building Design and Materials. Employ design strategies and building materials that evoke a sense of quality and permanence.

**CD-2.10:** Building Massing. Provide special building-form elements, such as towers and archways, and other building features to help distinguish activity nodes and establish landmarks in the community.

**CD-2.11:** Architectural Styles. Use different but complementary forms of architectural styles and designs that incorporate representative characteristics of a given area. Old English architecture should be limited to the Civic Center area.

**CD-2.12:** Architectural Features. Use architectural design features (windows, columns, offset roof planes, etc.) to vertically and horizontally articulate elevations.

**CD-2.13:** Wall Treatments. Provide variations in color, texture, materials, articulation, and architectural treatments. Avoid long expanses of blank, monotonous walls or fences.

**CD-2.14:** Property and Landscape Maintenance. Require property owners to maintain structures and landscaping to high standards of design, health, and safety on all sides of buildings.

**CD-2.15:** Variations in Wall Plane. Avoid use of long, blank walls by breaking them up with vertical and horizontal façade articulation achieved through stamping, colors, materials, modulation, and landscaping.

**CD-2.16:** Landscaping. Encourage the use of creative landscape design to create visual interest and reduce conflicts between different land uses.

**CD-2.18:** Walls and Fences. Design walls and fences that are stylistically well integrated with adjacent structures and terrain, and use landscaping and vegetation to soften their appearance.

*Consistency CD-2.9 through CD-2.18: The provisions of the WMSP development standards and design standards and guidelines address landscaping, architecture, building materials, building articulation, design features, design massing, etc. for the project and require or strongly encourage implementation of the policies identified in the Community Design Element of the General Plan.*

### GOAL CD-3: ENHANCED CORRIDORS AND INTERSECTIONS

A visually attractive appearance of the City from freeways, interchanges, and enhanced corridors and intersections.

**CD-3.1:** Streetscape Design. Create unifying streetscape elements for enhanced landscape streets, including coordinated streetlights, landscaping, public signage, street furniture, and hardscaping.

*Consistency: The WMSP includes parkway planting standards for Bolsa Avenue and Edwards Street and includes a planting palette for internal roadways to create a consistent image throughout the site. A master sign plan for the entire WMSP site will be required to ensure there are consistent directional and entryway signs, and individual projects will also need to design their signage consistent with the development standards provided in the Specific Plan.*

**CD-3.2:** Streetscape Features. Design new and, when necessary, retrofit existing streets to improve walkability, bicycling, and transit integration; strengthen connectivity; and enhance community identity through improvements to the public right-of-way such as sidewalks, street trees, parkways, curbs, street lighting, and street furniture.

*Consistency: The WMSP will improve bicycle access to the area through the improvements to the Navy Trail, internal pedestrian and bicycle linkages, enhanced intersection treatments on Bolsa Avenue and Edwards Street, and enhanced parkways both internal and external to the project area.*

**CD-3.3:** Enhanced Intersections. Apply special treatments at major intersections and crosswalks along enhanced corridors to create a visual focal point, sense of place, and slower traffic.

*Consistency: Traffic calming improvements are required for both Bolsa Avenue and Edwards Street to provide traffic flow, minimize cut-through traffic in adjacent neighborhoods. The Specific Plan also includes parkway and street tree standards, and provisions to regulate the placement and design of lighting throughout the project area.*

**CD-3.4:** Undergrounding Utilities. Underground and/or screen existing, new, or relocated electric or communication distribution lines, which are visible from many of the City's enhanced corridors.

*Consistency: Utilities are already undergrounded on the Westminster side of Bolsa Avenue and Edwards Streets. Any new utilities proposed on the project site would be placed underground and would be part of the site's infrastructure plan.*

### GOAL CD-5 CULTURAL RESOURCES

An understanding and acceptance of the City's ethnic diversities and preservation of the rich, historical context of Westminster's past.



**CD-5.5:** Performance and Visual Arts. Promote and preserve the richly diverse cultural heritage of Westminster residents by providing and encouraging performance and visual arts opportunities for all segments of the community.

*Consistency: Programming is an integral component of the activation of the Westminster Mall. With two new parks planned for the project site, the property owners are encouraged to plan events for the community that make use of the new open spaces that will be created on site. The Specific Plan includes provisions for programming requirements at the Mall.*

**CD-5.7:** Paleontologic Resources. In order to protect paleontologic resources in the City, projects that would involve extensive grading or grading of large areas to depths of more than ten feet below grade, should be conditioned to immediately stop grading activities if paleontological or archaeological resources are encountered. At this point a qualified paleontologist approved by the City should be enlisted to investigate the resources and conduct a preliminary assessment to determine whether a paleontologic impact report will be required.

*Consistency: The WMSP EIR includes provisions and mitigation measures to monitor new construction if paleontological resources are found during grading.*

## **B.4 MOBILITY ELEMENT**

### **GOAL M-1: COMPLETE STREETS**

A safe, efficient, and accessible transportation system that serves the mobility needs of all users of all ages and abilities.

**M-1.1:** Priority Travel Modes. Develop a comprehensive network of complete streets throughout the City, to provide connectivity for priority modes of travel based on the prioritized modes identified on Figure 5-1.

*Consistency: The WMSP employs Complete Streets requirements as spelled out in the Mobility Chapter. Roadways are consistent with the General Plan roadway classifications and priority modes of travel specified for Bolsa Avenue and Edwards Streets.*

**M-1.2:** Agency Coordination. Coordinate with Caltrans, OCTA, and SCAG to implement complete streets and maintain consistency with the Orange County Master Plan of Arterial Highways (MPAH), the Congestion Management Program (CMP), and the Regional Transportation Plan/Sustainable Communities Strategy (RTP/SCS).

*Consistency: Bolsa Avenue is classified as an Arterial Highway. City will continue to coordinate with Caltrans, OCTA and SCAG to illustrate how complete streets practices are being implemented on the site and are contributing to the implementation of the MPAH, CMP and RTP/SCS*

**M-1.3:** Level of Service. Maintain level of service (LOS) D for vehicles at intersections and roadways when vehicles are considered a prioritized mode of travel (see Figure 5-1). When vehicles are not prioritized, LOS E for vehicles at intersections and roadways (operating at capacity) shall be acceptable.

*Consistency: As part of the Environmental document, impacts to the vehicle miles traveled will be studied and will be mitigated (a new environmental threshold adopted through SB 743 went into effect after the General Plan was adopted). Although LOS can no longer be used to evaluate environmental impacts under SB 743, the City will still evaluate LOS at key intersections and road segments as another measure and suggest improvements as applicable. Since one of the goals of the project is to ensure no additional vehicular lanes are added along Bolsa Avenue or Edwards Streets, the traffic impacts are anticipated to be manageable and mitigatable.*

**M-1.5:** Mobility Impacts. Evaluate potential mobility impacts associated with proposed new developments and require the implementation of appropriate mitigation measures.

*Consistency: A traffic analysis has been prepared as part of the WMSP EIR and appropriate mitigation measures have been identified. No additional vehicular lanes will be added on Bolsa Avenue or Edwards Street to accommodate the new development that results over time in the WMSP area.*

**M-1.7:** Future Improvements. Obtain and preserve adequate right-of-way to accommodate future mobility system improvements.

*Consistency: One of the objectives of the WMSP is to accommodate an intensification and new mix of uses within the existing area without the need to add new vehicular travel lanes on Bolsa Avenue and Edwards Street, so no additional right-of-way area is anticipated.*

**M-1.9:** Traffic Calming Tools. Use traffic-calming tools to assist in implementing complete street principles; possible tools include roundabouts, curb extensions, high visibility crosswalks, and separated bicycle infrastructure.

*Consistency: The WMSP proposes crosswalk and turning improvements along Bolsa Avenue and Edwards Street and includes improvements to the Navy Trail Class I Bicycle Lane on the north end of the project. Internal streets are also required to provide a shared circulation network for bicycles, vehicles, and pedestrians as well, minimizing areas of conflict between the three users.*

## GOAL M-2: ACTIVE TRANSPORTATION

A first-rate network of bicycle and pedestrian infrastructure that supports the City's complete street goals and provides safe, efficient, and accessible connectivity for activity modes of travel.

**M-2.3:** Development Projects. Require the provision of adequate bicycle and pedestrian access for new development projects through the site plan review process and update the development standards to include this provision.

*Consistency: The Specific Plan is designed to provide a unique, open, and walkable environment with amenities that enhances the pedestrian experience and prioritizes bicycle access by including conveniently located bike racks and storage locker facilities within the site design.*

**M-2.6:** Accessibility Standards. Design bicycle and pedestrian infrastructure in accordance with federal, state, and local design standards, including ADA accessibility standards. Ensure bicycle and pedestrian infrastructure is accessible for use by people of all abilities

*Consistency: The development will be designed to comply with the American Disabilities Act including bicycle and pedestrian infrastructure, which mandates accessibility to buildings and facilities by disabled individuals.*

**M-2.8:** Intersection and Signal Enhancements. Enhance pedestrian and bicycle crossing efficiency and safety, including timing of signals, crosswalks, and intersection design features. Provide signal timing that allows intersection crossing at a safe pace.

*Consistency: The WMSP identifies specific traffic calming, accessibility and safety improvements for two key intersections in the project area at Bolsa Avenue and Victoria Lane and Edwards Street and Royal Oak Drive.*

## GOAL M-3: TRANSIT

A comprehensive and well-connected transit system that expands transportation choices, reduces single-occupancy vehicle trips and related greenhouse gas emissions, and improves air quality.

**M-3.5:** Bus Stops. Partner with regional transit operators to provide attractive and convenient bus stops, including shade/weather protection, seats, transit information, and bus shelters as appropriate.

*Consistency: The Orange County Transit Authority (OCTA) currently operates Routes 64 and 64x, providing transit services from the WMSP site to the Tustin area along Bolsa Avenue (Bolsa-Victoria stop). Another bus stop is located on the Mall side of northbound Edwards Street (Edwards-Mar Vista stop). Any future redevelopment of the WMSP area must take into consideration the current location of the stops and relocate as appropriate if necessary.*

## GOAL M-4: PARKING

Adequate availability of parking that meets the needs of people who live, work, and visit Westminster.

**M-4.1:** Off-Street Parking. Require new developments to provide sufficient off-street parking to reduce on-street parking congestion and increase both auto and pedestrian safety, and encourage new development to provide electric vehicle charging stations and preferential parking for carpools, vanpools, and alternative fuel vehicles.

*Consistency: The WMSP requires that parking be provided consistent with the provisions of the Westminster Municipal Code (unless a Shared Parking Agreement or Parking Master Plan is approved), and also requires electric vehicle charging stations.*

**M-4.2:** Off-Street Parking Alternatives. Allow developers to meet their minimum parking requirements via shared parking techniques which can leverage unused parking with nearby parcels, in-lieu fees, or on-street parking.

*Consistency: Property owners may want to partner to build parking structures that could be used by multiple buildings. The WMSP includes provisions to allow property owners to submit a Shared Parking Agreement or Parking Master Plan that would allow for shared parking with adjacent parcels on the site or parking management (such as valet during peak demand times such as holidays or events).*

**M-4.3:** Managed Parking Supply. Manage parking supply through implementation of time limits, pay parking, or permits, while ensuring the preservation of economic development goals.

*Consistency: Projects will be required to demonstrate that onsite parking will be adequate to meet the expected residential, commercial, office (including medical office), hospitality and entertainment use demand generated by activity at Westminster Mall. Parking will be managed through a variety of parking management strategies to be identified by each property owner and may include shared parking, bundled parking, and the preparation of a Parking Management Plan, including time restrictions for parking on the internal streets.*

## GOAL M-5: TRANSPORTATION MANAGEMENT

An efficient and cost-effective transportation system that reduces use of single-occupant vehicles.

**M-5.1:** Transportation Demand Management. Utilize Transportation Demand Management (TDM) measures throughout the City, where appropriate, to discourage the single-occupant vehicle, particularly during the peak hours.

*Consistency: To reduce peak hour trips, the establishment and continuing maintenance of the Transportation Management Association (TMA) or a Transportation Management Organization (TMO) is a high implementation priority for the success of the WMSP. Projects that will generate more than 50 peak hour trips are required to join the TMA, while other property owners and tenants are encouraged to join and participate based on incentives and benefits that the TMA will offer. TDM strategies applicable to reduced parking requirements, include: car sharing, carpool/vanpools, unbundled parking (parking spaces are rented or sold separately, rather than automatically included with the*



rent or purchase price of a residential or commercial unit), joint use (shared parking), transit, bicycle, and pedestrian system improvements, or trip reduction incentives to employees, such as free transit passes.

## B.5 PARKS AND RECREATION ELEMENT

### GOAL PR-1: PARK FACILITIES AND OPEN SPACE

A diverse system of safe and accessible local public park and recreation facilities that meets the varied needs of the Westminster community.

**PR-1.3:** Service Area Radius. Focus new park facilities in areas that are outside ¼-mile walking radius from an existing or proposed park or bike trail, and enhance options for residents to access these facilities through safe walking, bicycling, and transit routes. Physical barriers such as I-405 and SR-22 should also be considered when evaluating service area and access.

**PR-1.4:** New Facilities. Pursue the development of new parks and recreational facilities at utility easements, flood control channels, railroad rights-of-way, vacant parcels, and underutilized facilities.

*Consistency PR-1.3 and PR-1.4: Figure 6-2 of the General Plan (Parks, Trails and Open Space Plan) shows that a portion of the WMSP area is not located within the ¼-mile service area (walking distance) from an open space amenity. It should be noted, that the amenity shown is the Navy Trail Easement, which would not be able to provide adequate open space resources for the number of potential new residents anticipated to live in the Specific Plan area (up to approximately 8,000 new residents) when built out. New recreational facilities must be designed into the WMSP area (or in-lieu fees paid for a portion of the requirement) to provide a quality living environment for the new residents. In addition, the Parks and Recreation Facilities Master*

*Plan has identified this area as deficient in park acreage. As such, the Specific Plan proposes the construction of two new parks within the project area, consistent with the recommendations of the PRFMP.*

**PR-1.5:** Open Space in Nonresidential Development. Encourage nonresidential development, including commercial centers, industrial uses, and public facilities, to provide on-site open space for employee use.

*Consistency: Open space uses will be integrated throughout the WMSP project area and are required to be distributed across the site (versus clustered in one location) and will be required to be connected by a series of pathways or trails. Non-residential uses within the project area will have equal access to open spaces for employee use.*

**PR-1.10:** Quality Parks. Design parks with attention to place-making elements that foster social interaction and community pride by incorporating design elements (art, landscape, monuments, murals, play equipment, benches) based on a unique theme within each park. Where possible, local and historical cultural elements should be reflected in the park's design.

*Consistency: The Specific Plan requires that the project's two new parks be designed to include the following (which is consistent with the Parks and Recreation facilities Master Plan):*

- » **Programming New Event Spaces:** Improve facilities and infrastructure at a new park in the Westminster Mall Redevelopment Area to accommodate events, activities, and festivals. Coordinate with the Redevelopment Agency to recruit, host, schedule and organize event and activities.

- » **Public/Private Partners and Developer Coordination:** Develop and maintain relationships with developers and businesses through public/private partnerships, especially in the Westminster Mall Redevelopment Area, and ensure that new development pays for park/recreation impacts.
- » **Navy Trail Improvements:** Acquisition, development, landscaping, irrigation, support amenities, outdoor fitness equipment and enhancement of a 1.1 mile, east-west, off-street trail extending from Hoover Street Trail/Mendez Memorial Bikeway, by the Westminster Mall redevelopment area. Includes pedestrian/bicycle traffic signal system at five street crossings
- » **Special Use Park and Promenade:** Acquisition, design, development and programming of an urban plaza and event space, with additional landscaped green space, promenade, and/or walkways connecting to nearby neighborhoods, businesses, and trails (6.5 acres).
- » **Mixed Use Neighborhood Park:** Acquisition, design and development an urban-oriented neighborhood park (minimum 2.5 acres) with space for play, social gatherings, and fitness/active recreation.

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## GOAL PR-2: PARKS AND RECREATION FACILITY MANAGEMENT

Park and recreational facilities that are well maintained and safe to meet the short- and long-term recreational needs of the City.

**PR-2.1:** Parks and Recreational Facilities Master Plan. Use the City's parks and recreational facilities master plan to guide future capital improvement projects, recreational programs, and maintenance needs.

*Consistency: The Specific Plan requires that the project's two new parks be designed to include the following (which is consistent with the Parks and Recreation facilities Master Plan):*

**PR-2.2:** Parkland Dedication. Require new developments to provide adequate, usable, and permanent open space on-site, off-site, or via in-lieu fees.

**PR-2.3:** Incentives. Incentivize developers of multifamily residential and mixed-use projects to provide on-site, publicly accessible open space and recreational facilities; incentives could include density bonuses, expedited development review, and the reduction of on-site parking.

**PR-2.4:** Partnerships. Establish creative partnerships with local and regional agencies, private developers, and institutions to develop new, nontraditional types of open space, such as plazas, pocket parks, parklets, rooftop gardens, and enhanced streetscapes.

*Consistency PR-2.2 through PR-2.4: The redevelopment of the Specific Plan area has the potential to increase demands on parks, facilities and trails by both new residents and more visitors to the City--potentially increasing use and maintenance needs.*

*The construction of the WMSP's two new on-site parks are contingent on developer funding and construction to meet needs for a special use park/promenade and a Neighborhood Park.*

*The Specific Plan allows for flexible design standards and parking reduction considerations to integrate the new open space uses and plazas, paseos. It is anticipated that the City may pursue partnerships with the property owners to help program the open spaces or to provide guidance on the types of activities that are in greatest need in the community for the property owners to consider implementing when programming the spaces.*

## **B.6 INFRASTRUCTURE AND NATURAL RESOURCES ELEMENT**

### **GOAL INR-1: WATER SYSTEM**

Reliable potable water supply and efficient distribution and Stormwater systems to meet the ongoing needs of Westminster residents.

**INR-1.4:** Water Infrastructure. Ensure that the City's water infrastructure is adequately sized for storage capacity and treatment to serve existing and future needs, and that adequate capital improvement funding is in place for the rehabilitation or replacement of critical infrastructure.

*Consistency: As part of the Specific Plan preparation process a water supply assessment was prepared by the City of Westminster confirming that adequate water supply exists to meet the project water demands generated by development of the Specific Plan Area.*

**INR-1.5:** Illegal Connections. Continue to prohibit illegal connections and discharges into the City's storm drain system.

*Consistency: A storm drain plan is required by the City to prevent illegal connections into the City's storm system.*

**INR-1.7:** Stormwater Runoff. Minimize the quantity of Stormwater directed to impermeable surfaces to allow more percolation of Stormwater into the ground.

*Consistency: To prevent stormwater runoff, the Santa Ana Regional Water Quality Control Board enforces the National Pollutant Discharge Elimination System (NPDES) Permit, which is issued by the City of Westminster.*

**INR-1.9:** Natural Water Bodies and Drainage Systems. Limit the disturbance of natural water bodies and drainage systems in Westminster by conserving natural areas, protecting slopes and channels, and minimizing the impacts from Stormwater and urban runoff.

*Consistency: No waterbodies exist upon the project site. Storm water will be treated and conveyed into the City's existing storm drain system. In accordance with the NPDES Permit, the City of Westminster requires submittal of a Water Quality Management Plan (WQMP) that describes the water treatment approach for Westminster Mall. The*

*WQMP must meet the requirements of the NPDES and the current Santa Ana Regional Water Quality Control Board technical guidance document. Existing catch basins and storm drainpipes are adequately sized to accept the anticipated flows.*

**INR-1.10:** Hydrology and Pollutant Loads. Encourage the use of structural and nonstructural best management practices to mitigate any projected increase in pollutant loads and hydrology flows, and ensure that post-development runoff rates and velocities from a site have no significant adverse impact on downstream erosion and stream habitat.

*Consistency: In accordance with the NPDES permit, the City of Westminster requires all new development to implement Storm Water Best Management Practices (BMP's), which will be described in the WQMP.*

## **GOAL INR-2: WASTEWATER SYSTEM**

A wastewater collection and treatment system adequate to serve the long-range needs of the community.

**INR-2.2:** Service Standards. Continue to coordinate land use development with the Midway City Sanitary District to provide adequate collection, supply, treatment, and disposal of wastewater and adherence to adopted service standards for sewer service systems.

*Consistency: Sanitary sewer service will be provided to the Specific Plan area by Orange County Sanitation District (OCSD) and Midway City Sanitary District (MCSD). Both OCSD treatment plants have adequate capacity to accept the increased sewage anticipated for development in the Specific Plan area.*

**INR-2.3:** New Development. Ensure that new development pays its fair share contribution to the wastewater treatment and collection system necessary to serve the demands created by the development.

*Consistency: The project would pay all applicable impact fees as required by the City of Westminster to ensure its fair share contribution to serve the demands created by the development.*

**INR-2.4:** Sewer Deposit Best Practices. Work with Midway City Sanitary District to identify and implement, as feasible, best practices and technologies for wastewater collection and treatment, including those that reduce the amount of wastewater requiring treatment, prevent contamination, avoid sewage spills, maintain the highest possible energy efficiency, and reduce costs and greenhouse gas (GHG) emissions

*Consistency: Midway City Sanitary District (MCSD) and the Orange County Sanitation District (OCSD) operate and maintain the wastewater system that serves the City of Westminster. An existing 15-inch sanitary sewer main, which is owned and operated by MCSD and located at the northwest corner of the area is adequately sized to accommodate the anticipated volumes generated by the development of the Specific Plan area. The project will comply with all requirements and best practices for wastewater.*

## **GOAL INR-4: COMMUNICATION SYSTEMS**

A modern and efficient telecommunications system that improves economic development, governmental efficiency, and equitable access.

**INR-4.1:** Adequate Facilities and Availability of Services. Work with telecommunications service providers operating in Westminster to ensure a wide range of modern telecommunication services that meet the advanced facility and service demands of existing and future development, and expand service to areas that are underserved or not served by telecommunication technologies.

*Consistency: A range of communication services are available to the site and will continue to be provided by national and local service providers.*

**INR-4.3:** Wireless Communication Facilities. Ensure that wireless communication facilities are designed and sited in a manner that minimizes visual impacts on and from the surrounding environment.

*Consistency: All wireless communication facilities shall be required to be screened from view.*

## **GOAL INR-5: NATURAL RESOURCES AND CONSERVATION**

An environment that conserves, protects, and enhances Westminster's biological, energy, and water resources.

**INR-5.1:** Biological Resource Protection. Conserve and protect wildlife ecosystems, riverine corridors, and sensitive habitat areas within the City, including existing trees that are suitable nesting and roosting habitat for resident and migratory bird species.

*Consistency: Biological resources shall be analyzed during the environmental review process and shall be mitigated where necessary.*

**INR-5.4:** Urban Forest. Build upon existing streetscapes and develop an urban forest along the City's commercial and mixed-use streets and in neighborhoods to provide avian habitat, sequester carbon monoxide emissions, foster pedestrian activity, and provide shade.

*Consistency: Project will implement street trees that will contribute to Westminster's urban forest. Specifically, there will be a mandatory parkway planting scheme for both Bolsa Avenue and Edwards Street.*

**INR-5.8:** Energy Efficient Building and Site Design. Continue to encourage new construction to incorporate innovative and energy efficient building and site design, including solar orientation, use of natural daylight, and passive ventilation.

*Consistency: The project will fully conform to the California Building Standards Code (California Code of Regulations, Title 24), Part 6, California Energy Code; and Part 11, California Green Building Standards Code (also referred to as CalGreen). Both include regulations for energy efficiency.*

**INR-5.12:** Reclaimed Water Systems. Continue to coordinate with the Municipal Water District of Orange County (MWDOC) and Orange County Sanitation District (OCSD) to pursue the use of reclaimed water systems for landscape irrigation and industrial uses, such as the Green Acres Project.

*Consistency: All future development within the Specific Plan Area will be required to comply with the City of Westminster 2015 Urban Water Management Plan and all other applicable water quality requirements of the Orange County Water Management District (OCWD), the Municipal Water Districts of Orange County (MWDOC), and the City of Westminster. A Water Quality Master Plan (WQMP) is also required as part of any future development plans.*

**INR-5.14:** Efficient Use. Coordinate with the MWDOC and OCSD to explore the development of programs that reduce public and private water use and water waste associated with landscape irrigation, including the installation of efficient irrigation systems, recycling runoff, and drought-tolerant landscaping.

*Consistency: "Landscaping for projects within the project area shall be consistent with the provisions of Chapter 17.310 Landscape Standards in the Zoning Code, as well as consistent with the efficiency standards in Title 21 of the California Building Code. The Specific Plan requires the use of a drought-tolerant plant palette, with a minimum of 75 percent of species required to be drought-tolerant or California native species appropriate for the climate zone region per Section A4.106.3 of CALGreen, 2013. Irrigation requirements in the Specific Plan include use of water conserving methods and technologies and irrigation systems*

*that reduce runoff. The Specific Plan encourages use of compost and mulch to retain soil moisture and reduce watering requirements. Additionally, the Specific Plan requires grading be designed to capture and slow water runoff and allows for green infrastructure technical such as bioretention areas and rain gardens.”*

**INR-5.15:** Landscaping. Encourage all public and private landscaping in new development and renovation projects to be designed to reduce water demand, prevent runoff, decrease flooding, and recharge groundwater through the installation of irrigation systems, the selection of appropriate plant materials, and proper soil preparation.

*Consistency: “Landscaping for projects within the project area shall be consistent with the provisions of Chapter 17.310 Landscape Standards in the Zoning Code, as well as consistent with the efficiency standards in Title 21 of the California Building Code. The Specific Plan requires the use of a drought-tolerant plant palette, with a minimum of 75 percent of species required to be drought-tolerant or California native species appropriate for the climate zone region per Section A4.106.3 of CALGreen, 2013. Irrigation requirements in the Specific Plan include use of water conserving methods and technologies and irrigation systems that reduce runoff. The Specific Plan requires use of compost and mulch to retain soil moisture and reduce watering requirements. Additionally, the Specific Plan encourages grading be designed to capture and slow water runoff and allows for green infrastructure technical such as bioretention areas and rain gardens.”*

## **B.7 PUBLIC HEALTH AND SAFETY ELEMENT**

### **GOAL PHS-1: COMMUNITY SAFETY**

Effective police services that provide protection and promote a safe and healthy community for all Westminster residents, businesses, and visitors.

**PHS-1.5:** Development Review. Ensure that all new projects are designed with public safety in mind to prevent crime and minimize loss through fire incidents.

*Consistency: Individual projects within the Specific Plan will undergo individual site plan review which will include review by the City’s Police and Fire Departments (OCFA) that may recommend conditions of approval that ensure that projects are designed to prevent crime and minimize loss through fire or other safety incidents.*

### **GOAL PHS-2: FIRE HAZARDS**

Persons and property protected from urban fire hazards.

**PHS-2.1:** California Fire Code. Require all development to comply with the provisions of the most recently adopted California Fire Code.

**PHS-2.2:** Development Review. Continue to coordinate all development proposals with the Police Department and the Orange County Fire Authority to ensure that proposed projects incorporate Crime Prevention Through Environmental Design principles, and to determine if sufficient resources exist to meet any anticipated project-specific demand.

*Consistency PHS-2.1 and PHS-2.2: The proposed development shall be required to comply with the provisions of the most recently adopted California Fire Code at time of building permit. All proposed development within the WMSP shall be subject to review by the Orange County Fire Authority and Police Department and shall comply with required project specific demands.*

**PHS-2.3:** Fire Station Facilities. Periodically consult with Orange County Fire Authority to ensure that their facilities are appropriately located throughout the community based on existing and proposed land uses.

*Consistency: Three OCFA fire stations provide protection to Westminster Fire Stations #64 and #65 are the nearest stations to the Specific Plan area, located less than one mile from the site, at 7351 Westminster Boulevard and 6061 Hefley Street in the City of Westminster, respectively. The other is OCFA Station #66, Westminster Station #2, located at 15061 Moran Street. OCFA will review and provide conditions of approval on new projects as they come in and will identify any additional service needs based upon the type of development proposed.*

### **GOAL PHS-3: GEOLOGIC AND SEISMIC HAZARDS**

Minimal risk of injury, loss of life, property damage, and social and economic impacts caused by geologic and seismic hazards.

**PHS-3.2:** Geotechnical Study. Require that any development proposed in areas with potential for geologic or seismic hazards prepare a geotechnical hazard study that addresses and offers mitigation for ground shaking, landslides, liquefaction, expansive soils, subsidence, and erosion.

*Consistency: The Environmental document as required by CEQA will evaluate geological and seismic hazards and apply the appropriate mitigation measures as needed.*

### **GOAL PHS-4: FLOODING AND INUNDATION**

Minimal injury, loss of life, property damage, and social and economic disruptions caused by flood and inundation hazards in Westminster.

**PHS-4.2:** Floodplain/Flood Hazard Zone Development. Prohibit development in the 100-year flood zone unless adequate mitigation is provided against flood hazards.

**PHS-4.6:** Best Management Practices. Prevent flooding by ensuring that the City's storm drainage culverts, channels, and facilities are adequately sized, maintained, and upgraded and that new developments integrate low-impact development best management practices (e.g., permeable pavements).

*Consistency PHS-4.2 and PHS-4.6: The WMSP area is split between two Flood Insurance Rate Maps (FIRMs). The northerly 8-acres is covered under FIRM map No. 06059C0119J and is designated as flood zone "X" (shaded). The balance of the WMSP area is covered under FIRM map No. 06059C0232J and is designated with two flood zones. The most southerly 270 feet, adjacent to the north side of Bolsa Avenue, is designated as flood zone "A" and is a special flood hazard area subject to inundation by the 1% annual chance flood. The portion north of the flood zone "A" is designated as flood zone "X" (shaded). Flood zone "X" is defined by FEMA as those areas subject to a 0.2% chance of flooding; areas of 1% annual chance of flooding with average depths of less than 1-foot or with drainage areas less than 1-square mile; and areas protected by levees from a 1% annual chance flood. Flood zone "A" is a special flood hazard area and has no base flood elevation determined.*

*An underground drainage culvert and drainage easement runs along Bolsa Avenue on the southerly border of the property. Buildings are required to setback at least 60 feet in this area to clear the drainage channel, and no structures can be constructed in the easement area. Parking, drive aisles and landscaping are permitted.*

### **GOAL PHS-7: AIR QUALITY IMPROVED AIR QUALITY THROUGH REDUCTIONS IN POLLUTION, PARTICULATE MATTER, AND GREENHOUSE GAS EMISSIONS.**

**PHS-7.5:** Sensitive Receptors. Buffer sensitive land uses, such as residences, schools, care facilities and recreation areas, from major air pollutant emission sources, including freeways, manufacturing, wastewater treatment facilities, and similar uses.

*Consistency: Open space areas and residential areas within the WMSP should be designed to be buffered from major air pollutants generated by the freeway to the extent feasible. Open spaces should be located internal to the site separated by buildings from the freeway, and placement of residential and open space uses in relation to the freeway will be evaluated as each project is submitted.*

**PHS-7.6:** Construction Activities. Meet state and federal clean air standards by minimizing particulate matter emissions from construction activities.

*Consistency: Future development within the project site could increase greenhouse gas emissions both during construction and operation. The Environmental Impact Report will analyze short-term construction activities and long-term operational emissions associated with traffic and building emissions as these activities pertain to greenhouse gas emissions.*

**PHS-7.8:** Energy Efficiency. Comply with the mandatory energy efficiency requirements of the California Green Building Standards Code (CAL Green) and Building and Energy Efficiency Standards.

*Consistency: The project will fully conform to the California Building Standards Code (California Code of Regulations, Title 24), Part 6, California Energy Code; and Part 11, California Green Building Standards Code (also referred to as CalGreen). Both include regulations for energy efficiency.*

## **B.8 HOUSING ELEMENT**

### **GOAL 1: IDENTIFY ADEQUATE SITES TO MEET WESTMINSTER'S CURRENT AND PROJECTED HOUSING NEEDS.**

**Policy 1.2:** Pursue opportunities to construct housing beyond the RHNA.

Ensure that adequate sites are identified and designated with appropriate Land Use and Zoning Designations so that Westminster will be able to accommodate its future need. The City shall implement actions necessary to ensure that properties identified in the vacant and underutilized land inventories are designated so adequate housing unit potentials can be achieved for all income groups.

*Consistency: The mixed-use zoning of the WMSP allows for a substantial increase in the number of new units that can be built in the City of Westminster. The WMSP project area is one of the City's largest opportunities for redevelopment, and the WMSP proposes intensification of uses in this location to help accommodate future housing needs (and to minimize impacts on existing single-family neighborhoods).*

### **GOAL 2: ASSIST IN THE DEVELOPMENT OF AFFORDABLE AND MARKET-RATE HOUSING.**

**Policy 2.1:** Increase housing opportunities and choices for lower and moderate-income Westminster households, as funding is available.

*Consistency: The WMSP buildout is based upon the assumption that up to 3,000 new residential units may be built on the project site (possibly more or less, depending on how many trips are generated for the entire site). The WMSP also includes a 10% affordability requirement: 10% of all new units shall be constructed as affordable units.*



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